

A DESIGN-BUILD PROJECT



I-581 / Valley View Interchange

Phase II From: ~0.240 Mi. S. Route 101 (Hershberger Road)
To: ~1.561 Mi. S. Route 101 (Hershberger Road)

City of Roanoke, Virginia



Qualifications prepared for the Virginia Department of Transportation
December 9, 2011

State Project No.: 0581-128-109, P101, RW201, C501, B627
Federal Project No.: NH-581-5(035) Contract ID Number: C00016595DB45



American Infrastructure™ in association with



ATTACHMENT 3.1.2

0581-128-109, P101, RW201, C501, B627

STATEMENT OF QUALIFICATIONS CHECKLIST AND CONTENTS

(Revised December 05, 2011)

Offerors shall furnish a copy of this Statement of Qualifications (SOQ) Checklist, with the page references added, with the Statement of Qualifications.

Statement of Qualifications Component	Form (if any)	RFQ Cross reference	Included within 20-page limit?	SOQ Page Reference
Statement of Qualifications Checklist and Contents	Attachment 3.1.2	Section 3.1.2	no	N/A – Opening Page
Acknowledgement of RFQ, Revision and/or Addenda	Attachment 2.10 (Form C-78-RFQ)	Section 2.10	no	Appendix 2.10
Letter of Submittal (on Offeror's letterhead)				1
Offeror's point of contact information	NA	Section 3.2.1	yes	4
Authorized Representative's signature	NA	Section 3.2.1	yes	6
Principal officer information	NA	Section 3.2.2	yes	4
Offeror's Corporate Structure	NA	Section 3.2.3	yes	4
Affiliated/subsidiary companies	NA	Section 3.2.4	yes	4
Debarment forms	Attachment 3.2.5(a) Attachment 3.2.5(b)	Section 3.2.5	no	Appendix 3.2.5
Offeror's VDOT prequalification evidence	NA	Section 3.2.6	no	Appendix 3.2.6
Evidence of obtaining bonding	NA	Section 3.2.7	yes	5

ATTACHMENT 3.1.2

0581-128-109, P101, RW201, C501, B627

STATEMENT OF QUALIFICATIONS CHECKLIST AND CONTENTS

(Revised December 05, 2011)

Statement of Qualifications Component	Form (if any)	RFQ Cross reference	Included within 20-page limit?	SOQ Page Reference
Professional Services Evidence				
Full size copies of SCC and DPOR registration documentation (appendix)	NA	Section 3.2.8	no	Appendix 3.2.8
SCC Registration	NA	Section 3.2.8.1	yes	5
DPOR Registration (Offices)	NA	Section 3.2.8.2	yes	5
DPOR Registration (Key Personnel)	NA	Section 3.2.8.3	yes	5
DPOR Registration (Non-APELSCIDLA)	NA	Section 3.2.8.4	yes	5
DBE statement within Letter of Submittal confirming Offeror is committed to achieving the required DBE goal				
	NA	Section 3.2.9	yes	6
Offeror's Team Structure				
Identity of and qualifications of Key Personnel	NA	Section 3.3.1	yes	9
Key Personnel Resume – DB Project Manager	Attachment 3.3.1	Section 3.3.1.1	no	Appendix 3.3.1
Key Personnel Resume – Quality Assurance Manager	Attachment 3.3.1	Section 3.3.1.2	no	
Key Personnel Resume – Design Manager	Attachment 3.3.1	Section 3.3.1.3	no	
Key Personnel Resume – Construction Manager	Attachment 3.3.1	Section 3.3.1.4	no	
Key Personnel Resume – Lead Structural Engineer	Attachment 3.3.1	Section 3.3.1.5	no	
Organizational chart	NA	Section 3.3.2	yes	11
Organizational chart narrative	NA	Section 3.3.2	yes	12

ATTACHMENT 3.1.2

0581-128-109, P101, RW201, C501, B627

STATEMENT OF QUALIFICATIONS CHECKLIST AND CONTENTS

(Revised December 05, 2011)

Statement of Qualifications Component	Form (if any)	RFQ Cross reference	Included within 20- page limit?	SOQ Page Reference
Experience of Offeror's Team	NA	Section 3.4	yes	16
Lead Contractor Work History Form	Attachment 3.4.1(a)	Section 3.4	no	Appendix 3.4.1
Lead Designer Work History Form	Attachment 3.4.1(b)	Section 3.4	no	
Project Risk				
Identify and discuss three critical risks for the Project	NA	Section 3.5.1	yes	18



301 Concourse Boulevard, Suite 300
Glen Allen, VA 23059
Phone: 804-290-8500 Fax: 804-418-7935
www.americaninfrastructure.com

December 9, 2011

Mr. Joseph A. Clarke, P.E.
Alternate Project Delivery Office
Virginia Department of Transportation
1221 East Broad Street
Main Building, 4th Floor
Richmond, VA 23219

**Letter of Submittal/Statement of Qualifications for:
I-581 / Valley View Interchange Phase II Project
Contract ID Number: C00016595DB45**

Dear Mr. Clarke:

American Infrastructure-VA, Inc. (AI-VA) is pleased to submit our qualifications for the Virginia Department of Transportation (VDOT) I-581/Valley View Interchange Phase II Project in Roanoke, Virginia. We are one of the largest and most respected contractors in the Mid-Atlantic Region and enjoy a reputation for safely delivering quality projects, on time and on budget. We trust you will find our qualifications exceptional and we believe our team presents the following strengths:

- * **AI-VA is an award winning Design Builder:** The Design Build Institute of America, Mid-Atlantic Region, recently presented the Merit Award-Transportation Category to AI-VA for the Airport Connector Road PPTA Project in Richmond.
- * **A Successful Team:** Our team is committed to providing VDOT with the experience and expertise to deliver a quality project with balance between schedule, costs and project controls. AI-VA, along with our partners, will be referred to as the AI Team throughout this proposal. AI-VA has proven experience with three recent Design Build projects: the recently completed Richmond Airport Connector Road Project for Transurban, the Middle Ground Boulevard Extension Project in Newport News, VA and the Tye River Bridge Project in Amherst/Nelson Counties, both for VDOT. We have assembled a team that understands design-build delivery. Our Team members, Rinker Design Associates, Clark Nexsen, NXL, Froehling & Robertson, ECS Mid-Atlantic and Pulsar all have extensive experience in Design Build projects. We have provided an overview of our team under “The AI Team” heading below.

AI Experience – Past 5 Years	
\$128M	Federal Projects
\$650M	Projects for DOTs
\$826M	Highway/Road Improvement Projects
\$240M	Design-Build Projects

I-581 / Valley View Interchange Phase II



State Project No. 0581-126-109, FID1, RW207, CS01, R627 Federal Project No. NH-581-510351 Contract ID Number: C000765950R645

- * **A History of Innovative Solutions:** American Infrastructure has returned significant cost savings to the Commonwealth on four large transportation projects through innovative solutions in design and construction. The Hampton Boulevard project in Norfolk (\$38.2M), the Airport Connector Road Design-Build project in Richmond (\$39.0M), the Route 29 Approaches and Bridge over the Tye River Design-Build project in Amherst/Nelson County (\$6.7M), and the previously mentioned Middle Ground Boulevard Extension project (\$32.5M). Each project has benefited from innovative solutions led by American Infrastructure and AI Teams. Another example of innovation can be found in an environmentally relevant, though non-highway project, involving the construction of 1,700 LF of 54" sanitary sewer force main under the Back River in Baltimore County, MD. We developed an alternative construction approach that decreased the environmental impact and wetland disturbance footprint, shortened construction time by allowing work to proceed from both shorelines simultaneously, and decreased project costs. Regarding the I-581/Valley View Interchange Phase II Project, we acknowledge this project has unique characteristics and our team is committed to delivering an innovative design incorporating sound utility coordination and well-managed construction to ensure a successful project for the Commonwealth, FHWA, the City of Roanoke, Valley View Crossing Mall, Roanoke Parks & Recreation (Lick Run Greenway) and all other stakeholders.
- * **An Understanding of Project Goals and Objectives:** We understand these to be:
 - Complete the current partial Valley View Boulevard interchange;
 - Improve safety and ease congestion along I-581 and Valley View corridor;
 - Maintain an uninterrupted flow of traffic (vehicular and pedestrian) along I-581, Valley View Boulevard, Hershberger Road and The Greenway;
 - Meet all environmental requirements in accordance with NEPA documentation;
 - Provide and manage right-of-way acquisition, utility relocation and upgrade services;
 - Maintain public access to the Valley View shopping area and adjacent businesses;
 - Deliver a quality project on time and on budget.
- * **A Commitment to Exceed VDOT Expectations:** We are committed to exceeding VDOT's expectations by focusing on the following initiatives:
 - Use industry-excellent QA and QC procedures for both design and construction;
 - Provide high-level professionals with specific experience in Design-Build delivery;
 - Develop the project to mitigate schedule slippage and provide opportunities for the advancement of completion dates;
 - Acquire right-of-way and easements in a professional and sensitive manner;
 - Maintain an injury free work environment for all stakeholders.
- * **A Proven Safety Record:** We have a strong culture with regard to safety. Our commitment to safety is evidenced through our policies and our excellent safety performance.
- * **Financial Strength:** Our construction operations are supported by our financial strength, bonding capacity, and ability to draw from the deep expertise and resources of our affiliated companies; Allan A. Myers, LP and American Infrastructure-MD, Inc. These resources allow AI-VA to provide competitive pricing and apply quality and innovative solutions to benefit the project and the Commonwealth.

The AI Team: AI-VA will lead the Design-Build team of experienced and qualified partners. Our partners have significant experience with VDOT policies, standards, and specifications at both District and Central Office levels. Rinker Design Associates, P.C. (RDA) will lead the design for our team. They are a

Virginia-Certified Small Business who has been providing professional services throughout the Commonwealth for over 28 years. Supporting RDA, Clark Nexsen will provide structural design; Froehling & Robertson, Inc. (F&R), a certified SWaM, will provide design geotechnical services; and Anderson & Associates, Inc. (AA), a certified SWaM, will provide surveying services.

Leading the Quality Assurance Program will be NXL Construction Services, Inc. (NXL) who will operate independently from the design, design Quality Control, and construction Quality Control portions of the team. Working with NXL will be ECS Consultants, Inc. (ECS), serving as the QA Materials Testing laboratory. AI-VA will also be supported by F&R, for construction Quality Control; and Pulsar Advertising, Inc. (a certified SWaM and DBE) for Public Outreach. We have selected these firms to build the strongest and most qualified Team. We will provide the Commonwealth of Virginia, the City of Roanoke and all project stakeholders with the best value and delivery schedule.

Safety Program: AI-VA has a strong culture with regard to safety and believes “Everyone Has a Voice”. We conduct two daily meetings with each crew - one before work starts and the other after work finishes. All team members are encouraged to openly address safety concerns during those meetings. Every team member and stakeholder within the project also have the right to stop work if there is a safety issue. A project is not considered successful unless the goal of zero incidents is achieved. It is our policy to perform work in the safest manner possible, consistent with good construction practices.

We will provide a full-time Safety Coordinator dedicated to the I-581/Valley View Interchange Phase II Design-Build Project. We require 100% compliance in wearing hardhats, eye protection, and foot protection. Everyone on the project site will be required to comply with the safety policies and procedures established by our team. As part of our Project Safety and Loss Control Policy, this project, like all of our projects, will have a crisis management plan in place prior to mobilizing.

AI-VA has extensive safe-work experience for bridge and roadway projects. We institute comprehensive fall protection programs for our projects and use a job-specific Crane Safety Program; mandatory on every project requiring cranes. All roadway crews and supervisors are appropriately certified through ATSSA.

The table below shows evidence of AI-VA’s industry-excellent commitment to safety and the success of our programs. Safety performance data includes Experience Modification Rate (EMR), number of OSHA Citations (OSHA), number of VOSH Citations (VOSH), Recordable Incident Rate (RIR), and Lost Time Incident Rate (LTIR). This data represents over 2,450,000 man-hours (MH) of performed construction work. These rates reflect an exceptional safety performance relative to the industry average of 1.0 for EMR and 2.3 for RIR.

Year	EMR	OSHA	VOSH	RIR	LTIR	MH
2010	0.80	0	0	0.99	0.00	394,045
2009	0.75	0	2	0.42	0.00	471,668
2008	0.68	0	0	2.74	0.48	538,208
2007	0.66	0	0	4.20	0.52	463,274
2006	0.78	0	0	6.71	0.44	592,424

Safety Training: We believe that providing industry-excellent safety training to our employees will help us achieve the goal of “HOME-SAFE-TONIGHT” for every person on our projects.

I-581 / Valley View Interchange Phase II



State Project No. 05B1-12B-109, FID1, RW2LS, C5S1, R627 Federal Project No. NH-581-50351 Contract ID Number C00015595DB45

Why the AI Team? Because we use a collaborative approach and have the experience and expertise that sets us apart.

- * We believe “value” is more than just price
- * We place safety and quality above all else
- * We believe challenges are opportunities.
- * We understand the unique elements of this project
- * We have over 425 years of combined experience among our eight partners

Together, we are a team that will seamlessly support and represent the Department in designing and constructing the I-581/Valley View Interchange Phase II Project. Equally important, we will partner with VDOT to develop innovative solutions that will make this Project successful and relieve congestion for the traveling public.

The AI Team offers the following information as requested in **Section 3.2** of the Request for Qualifications.

3.2.1 David Nardon is the official representative and point of contact for The AI Team relative to this SOQ. In his role as the Design-Build Project Manager, he will be responsible for the oversight of the entire AI Team. The required contact information is as follows:

J. David Nardon	804.290.8528 (Telephone)
Design-Build Project Manager	610.222.4351 (Fax)
301 Concourse Blvd, Suite 300	443.876.6367 (Cell)
Glen Allen, VA 23059	david.nardon@americaninfrastructure.com

3.2.2 The principal officer of American Infrastructure-VA with whom a Design-Build contract with VDOT would be written is:

Aaron T. Myers	804.290.8525 (Telephone)
Vice President/General Manager	610.222.3360 (Fax)
301 Concourse Blvd, Suite 300	aaron.myers@americaninfrastructure.com
Glen Allen, VA 23059	

3.2.3 American Infrastructure-VA, Inc. is a registered Corporation in the Commonwealth of Virginia and will take financial responsibility for the Project without limitation. AI-VA has a bonding capacity of \$600M.

3.2.4 The affiliated companies of American Infrastructure-VA, Inc. are:

American Infrastructure—MD, Inc.	Allan A. Myers, L.P.
2011 Belair Road	1805 Berks Road
Fallston, MD 21047-2721	Worcester, PA 19490

3.2.5 Certification Regarding Debarment: The executed Primary & Lower Tier Attachments 3.2.5(a) and (b) are included in Appendix 3.2.5

3.2.6 Evidence of VDOT Prequalification: AI-VA Vendor No. G303. Working Classes: Excavating, Major Structures, Drainage Structures, Underground Utilities and Asphalt Pavement. A copy of our prequalification certificate can be found in *Appendix 3.2.6*

I-581 / Valley View Interchange Phase II

State Project No. 0581-128-109, FID1, RW201, CS01, R627 Federal Project No. NH-581-510351 Contract ID Number: C0006595DB45

3.2.7 Evidence of Bonding and Surety: AI-VA has the capability to obtain a performance and payment bond on the estimated contract value of the project. Our letter from Liberty Mutual Surety (dated November 9, 2011) supporting this information is found on page 7 of our Statement of Qualifications.

3.2.8 Evidence of professional services: The matrix below provides a summary of professional licenses held by the members of our team who are offering professional services for the Project. Full size copies of individual licenses for AI Team business entities and Key Personnel have been provided in Appendix 3.2.8.

3.2.8.1	3.2.8.2	3.2.8.3	3.2.8.4
<p>American Infrastructure – VA, Inc. 0113780-1 Corporation <i>Status: Active</i></p>	<p>44209 Wade Drive Chantilly, VA 20152 <i>Class A Contractors</i> 2701 009872A <i>Exp.</i> 12.31.13</p>	<p>N/A</p>	<p>N/A</p>
<p>Rinker Design Associates PC 0227062-7 Corporation <i>Status: Active</i></p>	<p>301 Concourse Boulevard Suite 120 Richmond, VA 23059 <i>Eng</i> 0410 000220 <i>Exp.</i> 02.29.12</p> <p>9300 W. Courthouse Road Suite 300 Manassas, VA 20110 <i>Eng, LS</i> 0405 000502 <i>Exp.</i> 12.31.11</p>	<p>Design Manager Darell Lee Fischer 14101 Spring Gate Terr. Midlothian, VA 23112 <i>Professional Engineer</i> 0402 023296 <i>Exp.</i> 06.30.12 Richmond, VA Office</p>	<p>9300 W. Courthouse Road Suite 300 Manassas, VA 20110 <i>Real Estate Appraisal</i> 4008 001684 <i>Exp.</i> 02.28.13</p>
<p>NXL Construction Services, Inc. 0349742-7 Corporation <i>Status: Active</i></p>	<p>114 E. Cary Street, Suite 200 Richmond, VA 23219 <i>Eng, LS</i> 0407 003031 <i>Exp.</i> 12.31.11</p> <p>2870-C South Main Street Harrisonburg, VA 22801 <i>Eng, LS</i> 0411 000678 <i>Exp.</i> 02.29.12</p>	<p>Quality Assurance Manager Joseph Roy Hamed 110 Wenn Drive Christiansburg, VA 24073 <i>Professional Engineer</i> 0402039327 <i>Exp.</i> 02.29.2012</p>	<p>N/A</p>
<p>Clark, Nexsen, Owen, Barbieri & Gibson PC dba/ Clark Nexsen 0703837-5 Corporation <i>Status: Active</i></p>	<p>213 S Jefferson St Suite 1011 Roanoke, VA 24011 <i>Arc, Eng</i> 00410 000130 <i>Exp.</i> 12.29.12</p> <p>6160 Kempsville Circle STE 200A Norfolk, VA 23502 <i>Arc, Eng, LA</i> 0405 000238 <i>Exp.</i> 12.31.11</p>	<p>Lead Structural Engineer Achyut (AI) Patel 729 Queen Elizabeth Dr. Virginia Beach, VA 23452 <i>Professional Engineer</i> 0402 025919 <i>Exp.</i> 04.30.13 Norfolk, VA Office</p>	<p>N/A</p>

I-581 / Valley View Interchange Phase II



State Project No: 0581-78-109, POI, RW207, CS0, B627 Federal Project No: NH-581-S0351 Contract ID Number: C000165950645

Froehling & Robertson, Inc. 0027211-2 Stock Corporation <i>Status: Active</i>	1734 Seibel Drive N E Roanoke, VA 24012 <i>Eng</i> 0411 000053 Exp. 02.29.12	N/A	N/A
Anderson & Associates of Virginia, Inc. 0171262-9 Corporation <i>Status: Active</i>	100 Ardmore Street Blacksburg, VA 24060 <i>Eng, LS, LA</i> 0407 004805 Exp. 12.31.11	N/A	100 Ardmore Street Blacksburg, VA 24060 <i>Eng, LS, LA</i> 0407 004805 Exp. 12.31.11
ECS Mid-Atlantic LLC S120821-6 Limited Liability Corporation <i>Status: Active</i>	5320 Peters Creek Road Suite F Roanoke, VA 24019 <i>Eng</i> 0411 000381 Exp. 02.29.12	N/A	N/A
Pulsar Advertising, Inc. F160855-5 Corporation <i>Status: Active</i>	N/A	N/A	N/A

3.2.9 DBE Goal Statement: It is the policy of American Infrastructure - VA to support the establishment, preservation of, and contribute to, the viability of small businesses owned by women and minorities. AI-VA is committed to achieving a 16% DBE participation goal for the full contract value of the I-581/Valley View Interchange Phase II Project.

Dated: 12/9/11

By: 
 Aaron T. Myers, VP/GM
 American Infrastructure – VA, Inc.

We are committed to delivering a successful, quality project to VDOT on time and on budget. Our commitment to VDOT is focused and we look forward to partnering with you on this important project.

Respectfully,


 Aaron T. Myers, VP/GM
 American Infrastructure – VA, Inc.


 J. David Nardon, DBPM
 American Infrastructure – VA, Inc.

ROSENBERG & PARKER

S U R E T Y B O N D S

November 9, 2011

Mr. Joseph A. Clarke, P.E.
Virginia Department of Transportation
Innovative Project Delivery Division
1401 East Broad St.
Richmond, VA 23219

Re: American Infrastructure-VA, Inc.
Request for Qualifications – A Design-Build Project
State Project No.: 0581-128-109, P101, RW201, C501, B627;
Federal Project No. NH-581-5(035); Contract ID Number: C00016595DB45;
I-581/Valley View Interchange Phase II From: 0.240 Mi. S. Route 101 (Hershberger Road)
To: 1.561 Mi. S. Route 101 (Hershberger Road)

Dear Mr. Clarke:

American Infrastructure-VA, Inc., a subsidiary of American Infrastructure, is a highly regarded and valued client of Liberty Mutual Insurance Company and Arch Insurance Company. Liberty Mutual Insurance Company is rated A XV in the Best's Key Rating Guide, listed in the Department of the Treasury's listing of Approved Sureties (Department Circular 570) and licensed to transact business in the Commonwealth of Virginia. Arch Insurance Company is rated A XV in the Best's Key Rating Guide, listed in the Department of the Treasury's Listing of Approved Sureties (Department Circular 570) and licensed to transact business in the Commonwealth of Virginia. Liberty and Arch have expressed to them their willingness to provide bonding to support on individual projects in the amount of \$250,000,000.00 and aggregate of \$600,000,000.00. As surety for American Infrastructure-VA, Inc., Liberty and Arch, with A.M. Best Financial Ratings as stated above, is capable of obtaining a 100% Performance Bond and a 100% Labor and Materials Payment Bond in the amount of the anticipated cost of construction, and said bonds will cover the Project and any warranty periods on behalf of the Contractor, in the event that American Infrastructure-VA, Inc. be the successful bidder and enter into a contract for this project.

In accordance with the normal practice, the willingness of Liberty and Arch to extend suretyship will be based on their underwriting of the account at the time the bonds are requested. This letter shall be valid for a period of 180 days from the date of this letter.

In addition, we would expect that the execution of any final bonds would be subject to a review of the contract documents by American Infrastructure-VA, Inc., Liberty Mutual Insurance Company and Arch Insurance Company as well as satisfactory evidence of financing for the project.

If we can provide any further assistance, please do not hesitate to call upon us.

Sincerely,

Rosenberg & Parker, Inc.


Matthew J. Rosenberg
Principal

MJR/mgh

cc: Mr. Jack Butler, Liberty Mutual Insurance Company
Mr. Russ Wilson, Arch Insurance Company



455 SOUTH GULPH ROAD • SUITE 400 • KING OF PRUSSIA, PENNSYLVANIA 19406

p 610.668.9100 • p 800.394.9200 • f 610.667.5200

info@suretybond.com • suretybond.com

3.3

Team Structure



Offeror’s Team: AI-VA strategically developed this project team to provide VDOT a strong, economical and experienced Offeror. We, with lead designer, Rinker Design Associates, have successfully teamed on previous projects. Together, we have a synergy that will provide VDOT with a capable team, ready to work.

AI-VA has the ability to self-perform the majority of work on the I-581/Valley View Interchange Phase II Design-Build Project. Similarly, RDA has the ability to self-perform most of the design and engineering disciplines. However, given the complexity and size of the project, we will strengthen our team by adding subconsultant partners with strategic areas of expertise. All design subconsultants will contract with RDA while all subconsultants or subcontractors required for construction will contract with AI-VA. We have selected our team based on relevant experience, expertise and benefit to the project. We have selected each member to be a complementary fit to the team, expected to provide quality, cost effective and value-added services. Together, we have the depth and strength to deliver this project safely, efficiently and with considerable value to VDOT.

 **American Infrastructure (AI)** has provided quality construction services in the Mid-Atlantic region since 1939 and as American Infrastructure-VA, Inc. (AI-VA) in the Commonwealth of Virginia since 1970. A Virginia contractor with a regional workforce of more than 320 employees and 458 pieces of heavy equipment and rolling stock, AI-VA has the available skilled craft and equipment

American Infrastructure Rankings

Engineering News Record (ENR)

- #120 Top 400 Contractors
- #68 Top 200 Environmental Firms
- #30 Domestic Heavy Highway

McGraw-Hill

- #13 Mid-Atlantic Top Contractors

to respond immediately to the needs of this project. AI-VA is backed by the resources of its parent company, American Infrastructure, with a fleet of over 2,062 pieces of heavy equipment and rolling stock with a dedicated staff of more than 1,650 people. With annual revenues in excess of \$435M, AI has performed over \$2.3B of construction in the Mid-Atlantic area over the last five years with \$1.6B of this work specifically in the highway and bridge sectors. AI-VA is licensed to operate in all major municipalities in the region for bridges, highways, utilities, and wastewater construction. AI-VA is committed to using local labor and establishing relationships with local small businesses and minority firms.

 **Rinker Design Associates, P.C. (RDA)** will be the lead designer for this project and provide AI-VA with multi-disciplinary designs. RDA is a mid-sized firm with more than 85 employees located in Richmond, Manassas and Fredericksburg, Virginia. Providing professional services throughout the Commonwealth since 1982, RDA is a Virginia-Certified Small Business (DMBE Certification #652784), and a member of the United States Green Building Council. RDA is a leading provider of professional civil, transportation and environmental engineering, surveying, land planning, and permitting services to both the public and private sectors. RDA brings Design-Build/PPTA experience having completed two PPTA’s and two Design-Builds, with an additional Design-Build project currently underway with AI-VA.

 **NXL Construction Services, Inc. (NXL)** will provide independent Quality Assurance inspection and testing for this project. They are a Virginia certified DBE (#626437) founded in 1989 and based in Richmond, Virginia with offices throughout Virginia. NXL provides surveying, construction management/inspection services and project controls for transportation projects.

I-581 / Valley View Interchange Phase II



State Project No: 0581-128-109, P101, RW201, C501, B627 Federal Project No: NH-581-50351 Contract ID Number: C00016595D845

CLARK • NEXSEN

Architecture & Engineering

Clark Nexsen will provide structural engineering services and other design support services for this project. They are a full-service engineering, architecture, and planning firm headquartered in Norfolk, Virginia with nine offices in the mid-Atlantic Region including an office in Roanoke, VA. Founded in 1920, the firm has approximately 550 employees and is #147 of the top 500 design firms in the Engineering News-Record (ENR) rankings. Clark Nexsen's Virginia Transportation Practice includes over 40 technical personnel specializing in the design of bridges, roadways, hydraulics, landscaping, and other related disciplines.



FROEHLING & ROBERTSON, INC.

Froehling & Robertson (F&R)

will provide Quality Control field inspection and testing services for the team and serve as the independent QC materials testing lab for the project. F&R is a multi-disciplinary engineering firm providing a full range of services, including construction management, construction materials testing and environmental and geotechnical engineering. Based in Richmond with an office in Roanoke, F&R has the local resources to deliver quick, efficient and cost-effective services required for this project. F&R is a woman-owned business (SWaM Cert# 649650) in operation since 1881.



Anderson & Associates, Inc. (A&A)

will provide supplemental survey and utility design support services for this project. Founded in 1968, they have cultivated an operating philosophy committed to client satisfaction and dedicated to innovation.



Pulsar Advertising (Pulsar) will provide public relations and public outreach. They are a minority owned business (DBE Certification #005622)

established in 1992. Pulsar is one of the nation's premier advertising agencies specializing in transportation and transit marketing, branding,

advertising and public relations. They have successfully collaborated on numerous large highway projects in Virginia, including the Springfield Interchange and the Dulles Metrorail Extension. Pulsar can draw from these projects and a deep portfolio of other challenging projects to provide unique and innovative approaches to maintain stakeholder awareness.



ECS Mid-Atlantic, LLC (ECS) will provide independent Quality Assurance laboratory services.

Founded in 1988, ECS is a leader in environmental, geotechnical and cultural resources engineering as well as construction observation and testing services. Their 12 regional labs are WACEL, AASHTO, AMRL, CCRL and USACE certified.

3.3.1 Key Personnel: We have identified the five key personnel for this project.

3.3.1.1 Design-Build Project Manager (DBPM)

Mr. David Nardon (AI-VA) will be responsible for the overall project design, construction, quality management, and contract administration for the project. He will be the primary point of contact for VDOT and other agencies. Mr. Nardon has 35 years of experience as a Senior Manager for similar types of projects.

3.3.1.2 Quality Assurance Manager (QAM) -

Mr. Joseph Hamed, PE (NXL) will be responsible for QA inspection and compliance testing of materials and work performed, in conformance with the contract requirements and the "approved for construction" plans and specifications. This includes monitoring of the AI Team's QC program. Mr. Hamed is a licensed professional engineer in the Commonwealth of Virginia with over 22 years of experience. As part of NXL, a separate and independent operating entity, he will not be involved in construction operations. Mr. Hamed has worked extensively with VDOT and is knowledgeable regarding the Quality Assurance standards required by VDOT.

3.3.1.3 Design Manager (DM) - Mr. Darell Fischer, PE (RDA) will be responsible for leading and coordinating the individual design disciplines including roadway, bridge, drainage, utilities, right-of-way, maintenance of traffic (MOT), geotechnical and environmental. He will ensure the overall project design conforms to the contract documents, and will establish and oversee the Quality Assurance and Quality Control (QA & QC) Program for design. This includes reviewing designs, working plans, specifications, and constructability. Mr. Fischer, a licensed professional engineer in the Commonwealth of Virginia with over 25 years of design and management experience.

3.3.1.4 Construction Manager (CM) - Mr. Robert Ackley (AI-VA) will be responsible for managing the construction process, including QC activities. He will be on the project site for the duration of the construction operations and will ensure materials used and work performed meet contract requirements and “approved for construction” plans and specifications. Mr. Ackley brings 17 years of VDOT experience to the project as a VDOT employee and a contractor. He holds a VDOT Erosion and Sediment Control

Contractor Certification (“ESCCC”) and a Virginia Department of Conservation and Recreation (“DCR”) Responsible Land Disturber (“RLD”) Certification.

3.3.1.5 Lead Structural Engineer (LSE) - Mr. Al Patel, PE (Clark Nexsen) will be responsible for structural design of the bridge and retaining walls. He brings over 27 years of experience in structural bridge design and is a licensed professional engineer in the Commonwealth of Virginia. Mr. Patel has been involved in numerous bridge projects for VDOT throughout the Commonwealth including several Design-Build/PPTA projects. His Design-Build/PPTA experience includes the Route 288 PPTA project (interchange bridges) and more recently, the Clifton Forge (Route 60) Design-Build project.

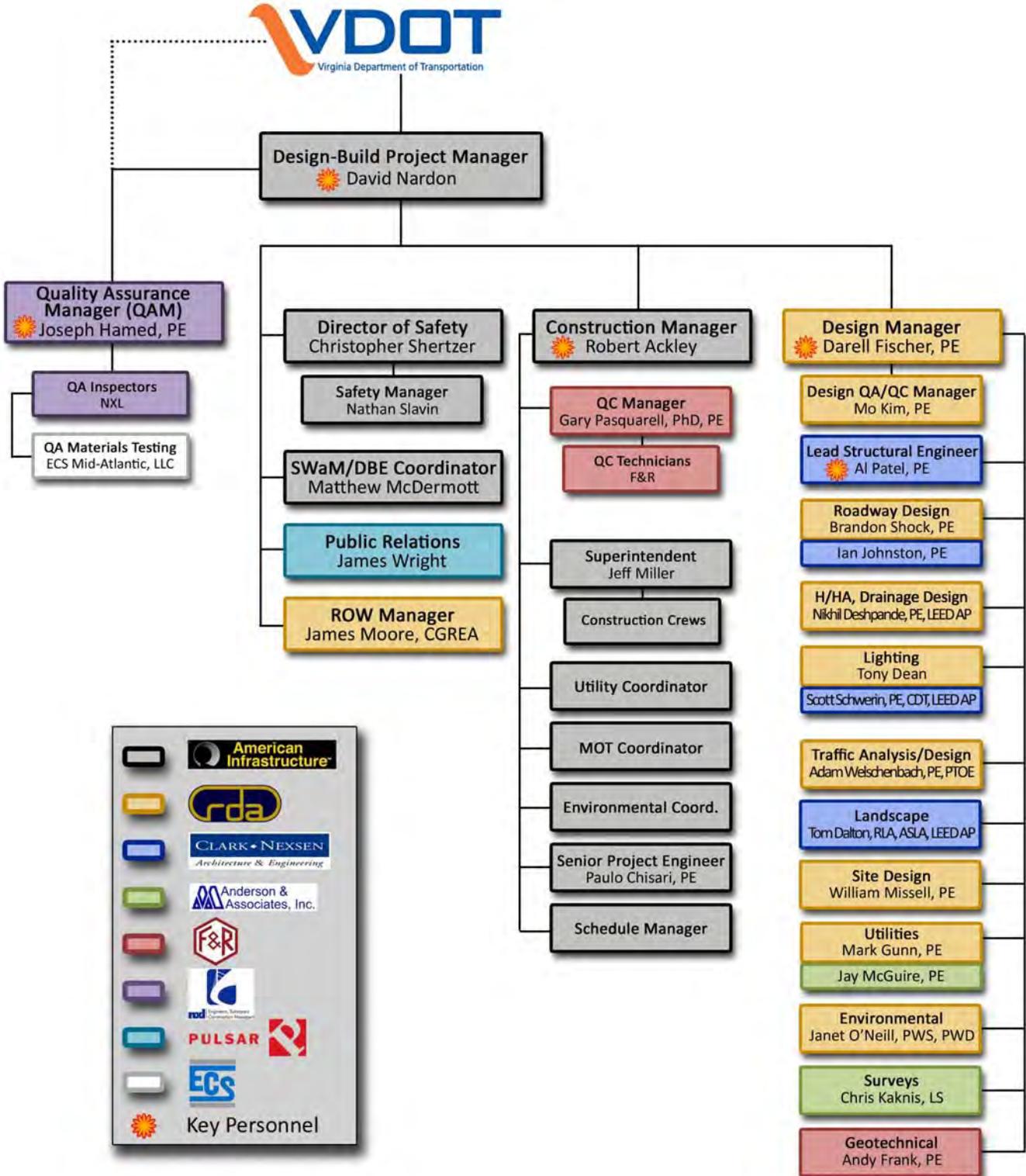
3.3.2 Organizational Chart: The organizational chart illustrates the “chain of command” and functional structure of our team. It shows a clear separation between Quality Assurance (QA) and construction. The organizational chart also shows a fully independent integration of Quality Control (QC) and QA operations. The following symbol identifies Key Personnel.



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	American Infrastructure
	rda
	CLARK + NEXSEN Architecture & Engineering
	Anderson & Associates, Inc.
	F&R
	rdi Transportation Solutions Construction Management
	PULSAR
	ECS
	Key Personnel

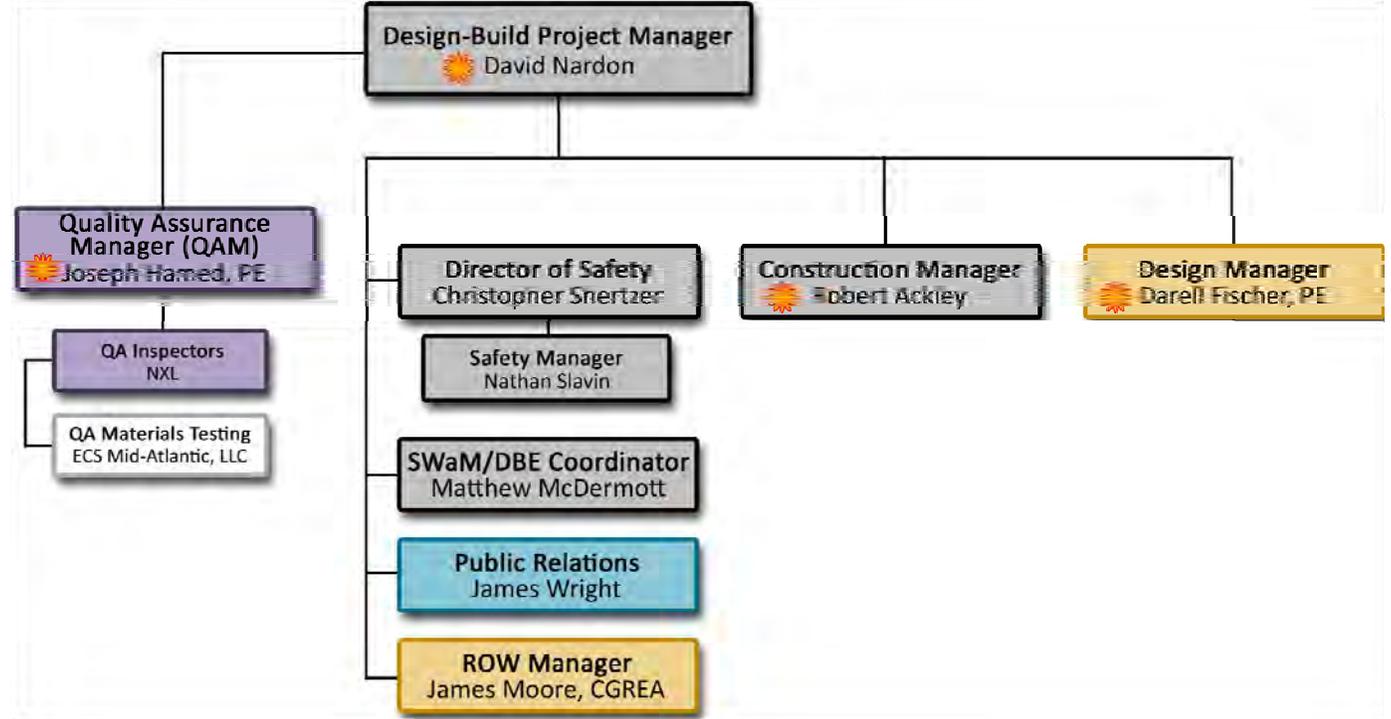


Functional Relationships: As DPBM, David Nardon is ultimately responsible for the successful completion of this project. He will be the primary point of contact for VDOT from the submission of the SOQ through project completion, and will work directly with the VDOT Project Manager. Mr. Nardon will have the authority to act for the AI Team on all matters related to the Project. Functional relationships shown graphically in the organizational chart are described below.

Primary Design-Build Management Team: Our management team includes the five designated Key Personnel with four additional positions. These are Director of Safety, **Christopher Shertzer**; SWaM/DBE Coordinator, **Matthew McDermott**; Public Relations Manager, **James Wright**; and ROW Acquisition services, **James Moore**. This team of professionals provides our DBPM with additional depth of expertise needed to deliver a safe and compliant project. Maintaining open and direct lines of communication with the public and all team members is critical to the success of the project.

Christopher Shertzer (AI-VA), Director of Safety, will be responsible for monitoring the safety on the Project. He will help to ensure construction activities meet or exceed AI corporate safety standards, and comply with site-specific safety policies. Mr. Shertzer brings a comprehensive safety and construction background to the I-581/Valley View Interchange Phase II Design-Build Project and provides safety management for all projects in Virginia. He has managed the safety program on several large highway projects. He recently managed the safety team for the Richmond Airport Connector Road Design/Build project, which was completed without incident or injury. Safety Coordinator Nathan Slavin (AI-VA) will support Mr. Shertzer. Mr. Slavin will be on site full time and is responsible for implementation and administration of AI-VA’s safety practices and policies. His 11 years of experience have proven an extremely valuable asset to the AI Team.

Matthew McDermott (AI-VA), SWaM/DBE Coordinator, will be responsible for the solicitation of SWaM/DBE firms during the RFP



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phase of the submittal, and for the management of SWaM/DBE firms during procurement after award. He will ensure we meet the Disadvantaged Business Enterprise goal for this project and act as a mentor to firms who are new to the process. Mr. McDermott will use public media advertisements and the VDOT prequalified SWaM/DBE list to achieve the project goal.

James Wright (Pulsar), Public Relations Manager, will work hand-in-hand with Mr. Ackley to keep the public “in the loop” regarding construction operations. Collaborating with stakeholders, Mr. Wright will develop a comprehensive public outreach strategy. As our liaison, he will conduct meetings with stakeholders, agencies, property owners, and the Design-Build Team. He will provide VDOT with project updates for posting to the VDOT website and content information for VDOT news releases. Mr. Wright has 34 years of experience in public relations, advertising and outreach, and has significant experience working with VDOT. His innovative ideas and experience in the region make Mr. Wright a strong addition to the AI Team.

James Moore (RDA), Right-Of-Way (ROW) Manager, will be responsible for administering the negotiations and the closing processes for land acquisitions. Mr. Moore has over 41 years of experience in real estate appraisal services and will oversee all work related to these transactions. Local appraisers and appraisal reviewers will support him. RDA’s right of way group is VDOT certified and staffed with negotiators and professionals who average over 35 years of experience. Mr. Moore and his team have completed successful ROW acquisition projects for both state and local municipalities.

Quality Assurance Team: Quality Assurance Management for construction will be performed by NXL and led by QAM, **Joseph Hamed, PE.**

He will responsible for independent QA oversight of all construction activities. He will report directly to David Nardon, DBPM, and indirectly to VDOT. Mr. Hamed will have the authority to stop any work not meeting contract requirements. In accordance with VDOT’s Minimum Quality Control and Quality Assurance Requirements for Design-Build and PPTA Projects, he will maintain independent oversight of the project. Mr. Hamed will be supported by several QA inspectors and by ECS for QA Materials Testing. As required by VDOT’s guidelines, our team’s Quality Assurance (QA) program will be completely independent from our Quality Control (QC) program.

Design Team: Darell Fischer will manage all design disciplines associated with this project and will report directly to Mr. Nardon. His primary functions will be to ensure design plans are coordinated with utilities, right-of-way, TMP, and environmental permitting. He will also coordinate geotechnical engineering to establish the design parameters. The following team members, each of whom will provide design services in their area of expertise, support Mr. Fischer:

Mo Kim, PE (RDA) is responsible for Design QA/QC. He will conduct independent design reviews and report directly to Mr. Fischer. Mr. Kim has over 17 years of experience in all capacities related to design, including Design Manager on the Route 15 PPTA/Design-Build project.

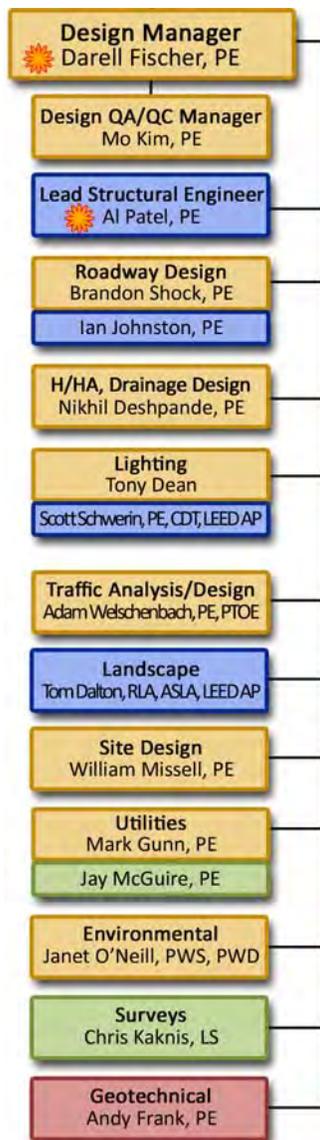
Al Patel, PE (Clark Nexsen), our LSE, will develop all structural designs (e.g. bridge widening, MSE walls, etc.). He will work closely with the roadway design group so that all structural features coordinate with roadway plans. This will ensure both design plans (roadway and structural) are complimentary and will correlate with each other.

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Brandon Shock, PE (RDA) will be responsible for horizontal and vertical geometrics, signing and marking, plan detailing, and overall development of the roadway plan. Mr. Shock has over 12 years of experience providing design services to VDOT and local jurisdictions. He recently fulfilled this same role on three (3) Design-Build projects: Middle Ground Boulevard Extension Project (VDOT), Route 36 Improvements Project (VDOT) and the Crosspointe Centre Roadway Improvements Project (Prince George County). Having recent Design-Build experience, he has a complete understanding of project requirements and applicable standards and specifications. Supporting Mr. Shock will be **Ian Johnston, PE** (Clark Nexsen) who has over 14 years of experience in roadway design. Until recently, Mr. Johnston spent 6 years working for VDOT as Project Manager



Nikhil Deshpande, PE (RDA) will perform the proposed stream relocation design and detailed H&HA study along with coordination with FEMA for map revision approval. Mr. Deshpande has 9 years of experience in roadway drainage design in Virginia, most recently as Senior H&HA/Drainage Design Engineer for the Route 15 PPTA Team. He completed three FEMA,

H&HA studies for two waterway bridges and a major box culvert.

Tony Dean (RDA), will analyze all lighting design to ensure luminance and illuminance requirements are met. He has 16 years in the industry. Assisting Mr. Dean will be **Scott Schwerin, PE, CDT, LEED AP** (Clark Nexsen). Mr. Schwerin will provide technical assistance and additional expertise as needed to complete the design.

Adam Welschenbach, PE (RDA) will be responsible for leading traffic analyses and engineering designs. Mr. Welschenbach has over 11 years of experience in traffic analysis, signal design and TMP design.

Tom Dalton, RLS, ASLA, LEED AP (Clark Nexsen) will provide landscape designs associated with the project. He has over 17 years of experience to draw upon.

William Missell, PE (RDA) will provide necessary site designs associated with impacts to parking lots and parking circulation. His 22 years of land development experience will be a valuable asset.

Mark Gunn, PE (RDA) will be responsible for utility coordination and design. He will preside over Utility Field Inspection meetings; obtain, review and approve Plans and Estimates, and assist in acquiring land rights for relocations. Mr. Gunn has over 14 years of experience. Supporting Mr. Gunn will be **Jay McGuire, PE** (AA) who will assist in the design of public utilities along the corridor.

Janet O'Neill, PWS, CWD (RDA) will lead all environmental efforts to obtain required permits and ensure compliance with permit conditions. Ms. O'Neill has over 35 years of experience identifying environmental resources and permitting projects throughout the Commonwealth. She oversees RDA's environmental staff and has extensive knowledge of compliance issues on state transportation

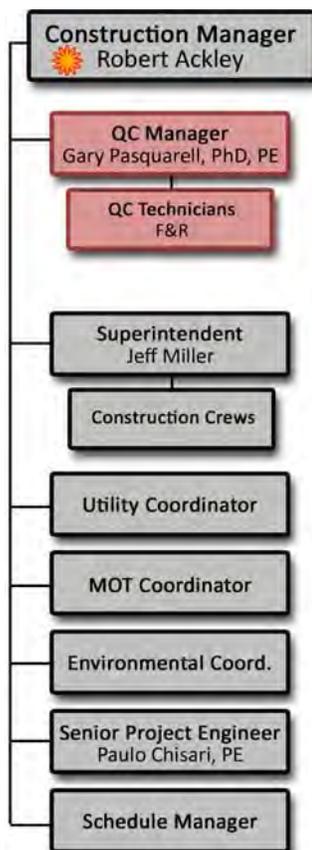


projects. Her strong working relationship with VDOT and other state and federal review agencies results in faster response times on agency approvals.

Chris Kaknis, LS (AA) will provide and coordinate field survey operations including topographic surveys, utility designating/locating, and wetland and boring stakeouts. Mr. Kaknis has over 25 years of experience in all aspects of surveying including participation in numerous road projects in the Commonwealth.

Andy Frank, PE (F&R) will be responsible for geotechnical investigations for the project. With more than 23 years of experience providing geotechnical services, he has been involved in numerous VDOT projects throughout the region.

Construction Team: The Construction Team is led by **Robert Ackley**, Construction Manager. In his current role as Project Manager on the \$38M VDOT Route 60 Midlothian Turnpike / German School Road project, he is leading all construction operations. He will have overall responsibility of day-to-day operations for I-581/Valley View Interchange Phase II project and will be located on-site full time. Mr. Ackley will report directly to Mr. Nardon, the DBPM.



The following team members will support Mr. Ackley:

Gary Pasquarell, PE (F&R), our Quality Control Manager, will oversee quality control inspection and testing for construction and prepare necessary reports. He will ensure construction conforms to the quality expectations of VDOT. Mr. **Pasquarell** has over 30 years of experience in construction materials testing and inspection services. This experience includes testing (laboratory and field), inspection of soils and foundations, reinforced concrete structures, stone base and asphalt pavement.

Jeff Miller (AI-VA), Project Superintendent will be responsible for all field operations. Field Superintendents, Foremen and subcontractors will report to Mr. Miller. He has 26 years of experience in complex highway and bridge projects.

Paulo Chisari (AI-VA) currently is our Senior Project Engineer for the \$39M VDOT Hampton Boulevard Grade Separation Project in Norfolk, VA. He is responsible for all field engineering for piling, structural concrete and structure excavation.

We have identified the additional roles we consider necessary to deliver this project successfully:

- Utility Coordinator
- MOT Coordinator
- Environmental Coordinator
- Schedule Manager

3.4 Experience of Offeror's Team

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AI-VA has experience working on projects similar to the I-581/Valley View Interchange Phase II Project both in scope and delivery method. **Appendix 3.4.1** (Lead Contractor – Work History Forms) contains three relevant projects demonstrating our qualifications to fill the role of Lead Contractor. Further, AI-VA and its affiliated companies have successfully completed many large highway projects in excess of \$200 million in contract value. Many of these projects involved complicated MOT, utility relocations and environmental sensitivities; all similar challenges to what will be encountered with the I-581/Valley View Interchange Phase II Project. AI recently completed a \$158M, 5.3 mile widening of the PA Turnpike that required extensive planning for MOT to accommodate 60,000 vehicles daily. The project included reconstructing five bridges and constructing over 250,000 SF of MSE retaining walls and noise barrier walls. The AI companies routinely pursue highway Design-Build projects that range in value up to \$250M and have successfully constructed Design-Build highway projects in Virginia, Pennsylvania and Maryland. **To date AI has been awarded over \$240M of Design-Build projects.**

We are currently working on several Design-Build highway projects in Virginia including the Middle Ground Boulevard Extension Project in Newport News (\$32.5M) and the Route 29 Bridge over Tye River in Amherst/Nelson Counties (\$6.7M) which will finish 11 months ahead of schedule. We recently completed the Richmond Airport Connector Road in Richmond (\$39M) finishing 3 months ahead of schedule. Progress reports received from owners on these projects have been very positive and recognize the success of AI's planning and scheduling processes.

As a lead designer, RDA has extensive experience on Design-Build/Public Private Partnership Projects (PPTA) projects. **Appendix 3.4.1** (Lead Designer – Work History Form) contains three relevant projects demonstrating their qualifications to fill the role of Lead Designer. These projects

include: Route 36 Improvements Design-Build in Prince George County/City of Hopewell, Route 15 PPTA in Prince William County, and Sudley Manor Drive PPTA in Prince William County. Additionally, RDA completed the Crosspointe Centre Roadway Improvements Design-Build project in Prince George County, a project requiring similar skills and expertise to what will be required to successfully complete this project. RDA is in the final design stages of the Route 645 (Stringfellow Road) widening and the I-81/Exit 310 VDOT project both of which demonstrate their qualifications and abilities with regard to this project.

In June of 2011, AI-VA with RDA kicked-off the Middle Ground Boulevard Extension Design-Build project in Newport News, VA. More recently, and as a team, we were shortlisted on another design-build pursuit, I-581/Elm Avenue Interchange Improvements project. Because it is in the same corridor and because there are common stakeholders, there may be inherent benefits to VDOT to engage the same team for both I-581 projects. Potential benefits would include:

- Common interface between the Design-Builder, VDOT and the City of Roanoke
- Cost savings with regard to mobilization of crews, equipment and subcontractors
- Efficiency in logistics because of proximity
- Common strategy for public outreach and relations

We believe a shared approach on the two I-581 design-build projects will provide significant cost savings and benefits to VDOT and consequently the public.

We share similar philosophies regarding quality and our approach to completing the work. And as importantly, we share similar business ethics and values. The synergy we have developed through these projects is an asset to our team, and an asset to VDOT. AI-VA and RDA have an established relationship as a team. We have developed

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integrated processes and procedures, with open and effective lines of communication.

AI-VA and RDA respectively, also have long-standing relationships with subconsultants and subcontractors and understand the value they bring to the team. Because AI-VA intends to self-perform the majority of construction to control schedule and cost, major subcontractors are not anticipated. However, we will be working with select and local subcontractors for specific project needs and to meet SWaM/DBE goals.

Successful Working Relationships of AI-VA and RDA with Subconsultants: AI-VA and F&R have successfully worked together on past design-build projects and have a strong understanding of each other's approach to providing solutions. Our firms enjoy a similar culture and work ethic. F&R will provide Quality Control and Testing for this project. On the recently completed \$39.0M Design-Build Richmond Airport Connector Road in Richmond, and currently the \$6.7M Design-Build reconstruction of Route 29 NBL Bridge over the Tye River, F&R provided Quality Control services for the AI Team.

Additionally, AI-VA, F&R, and Pulsar have teamed for other Design-Build pursuits, one being the Route 61 Narrows Bridge over the New River in Giles County Virginia, and the other, the Route 60 Bridge Replacement Project in Clifton Forge Virginia. Our team was shortlisted for both of these projects. As previously mentioned AI-VA, RDA, F&R, and Pulsar are teamed on the Middle Ground Boulevard Design-Build Project in Newport News and are short-listed to propose on the I-581/Elm Avenue Interchange Improvement Project in Roanoke. AI-VA and ECS are teamed on the Hampton Boulevard Grade Separation project in Norfolk.

RDA, the Lead Designer for the Project, has teamed with many of the identified subconsultants including *NXL and F&R*. Currently, RDA, NXL and F&R are teamed on the Route 36 Improvements Design-Build project in Prince George County and City of Hopewell, in identical roles to those planned for this project.

Strength of our Teaming Partners: Clark Nexsen has a substantial design-build practice. Clark Nexsen has completed over 100 design-build projects and four Public Private Partnership Projects over the past five years for state and federal agencies. These projects range in value up to \$200 million. This substantial background and their experience in successfully completing so many design-build projects provides the Clark Nexsen staff with the capabilities to address the needs and requirements of this project. Their recent success on the Route 60 Bridge Replacement over Smith Creek Design-Build project in Clifton Forge entailed the replacement of a 165' long bridge and is indicative of their ability to perform similar services in the Design-Build environment. Other signature projects include the Brambleton Avenue bridge design (700' long structure over tidal waters) and the Lessner Bridge Replacement project (twin 1,530 feet long structures over Lynnhaven inlet).

NXL is an experienced design-build partner. They have been involved in numerous design-build projects throughout the Commonwealth, primarily in the QA role. Their experience includes: Pacific Boulevard Widening (Northern Virginia District); Route 36 Improvements (Richmond District); I-81 Corridor Safety and Operational Improvements (Salem District); I-295/ Meadowville Road Interchange Project (Richmond District) and Route 60 – Main Street bridge replacement (Staunton District).

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The American Infrastructure Team has evaluated the I-581/Valley View Interchange Phase II project in great depth and believe there are several challenging, and in some ways unique, aspects to this project – if not individually then collectively. Items that garnered more serious consideration in determining the top three Project Risks include:

- Proximity of Roanoke Airport
- Design Waivers/Exceptions
- Right-of-Way Acquisitions / Relocations
- Transportation Management Plan (TMP)
- Public Relations / Stakeholder Coordination
- Valley View Crossing Mall
- Environmental Permitting & Compliance
- Geotechnical Considerations / Constraints

Of these eight risk elements, the three that resonate loudest are **Environmental Permitting & Compliance**, **Transportation Management Plan (TMP)** and **Geotechnical Considerations/Constraints**.

Environmental Permitting & Compliance:

Environmental considerations are numerous and varying and include:

- Section 4f (finding of De Minimis impact)
- Cultural Resources (no effect)
- Lick Run Relocation (~800 LF) / FEMA Floodplain
- Endangered Species (Logperch – no impact)
- Wetlands (none identified)
- Air Quality Analysis
- Noise Analysis and Sound Barriers

These risk factors represent the unknown environmental elements that may extend well beyond the “Scope Validation” period. As a result, should any of these elements grow in nature or reveal their presence, project cost, schedule and the NEPA process may be severely affected. For example, if the impacts to the Lick Run Greenway increase in order to construct the project while maintaining pedestrian traffic (a temporary trail may be required) then the project may exceed the “finding of De Minimis” threshold. Similarly, if

archeological remnants are encountered during construction the project schedule will be adversely impacted.

Mitigation Strategy: These risks may represent “changed conditions” which will be covered under the standard design-build contract with respect to compensation. However, schedule impact recovery will be difficult. Therefore, the best mitigation is avoidance. For this reason, we propose to provide a comprehensive overview of the environmental process during design. This would not typically be required as VDOT is providing the documentation upon which the permits will be acquired. However, this is a case where “an ounce of prevention is worth a pound of cure”. We will reinvestigate the potential for these risks during the “Scope Validation” period in hopes of proactively addressing their potential impact.

Transportation Management Plan (TMP):

TMP is always a challenging aspect of any project. Given its direct impact on motorists, pedestrians and cyclists, it is the one element about which the public is most vocal. When properly designed and implemented, it goes almost unnoticed and without comment. Conversely, when there is congestion attributed to construction or the controls break down, then the voice of public opinion is loud and unwavering.

The challenges with TMP as it relates to vehicular traffic are relatively minor on this project. The interstate component certainly puts a “spot-light” on the implementation but the design and modeling of phased traffic construction is relatively routine. The existing bridge has significant excess width to allow traffic adjustments and bridge widening to occur without lane closures or major disruption. This includes maintaining the active pedestrian trail access for the Lick Run Greenway trail bridge.

For this project, though, the TMP risk remains, and it is two-fold. The first aspect is establishing a schedule based on the TMP that will provide the least amount of disruption to the public during the

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holiday season. The Valley View Mall is the only major mall in the region and draws consumers from many surrounding counties. The traffic volume during the holiday season (November and December) is probably ten times that of any other time. Consequently, it creates a defined constraint in the schedule in the form of a TMP that needs to account for holiday traffic and leaves little room for slippage. The inherent risk lies with the schedule's Critical Path, which, if it pushes into the Holiday season, could result in a schedule slip of almost two months each season, that may not have a recovery path. The first step in mitigation strategy is to adhere to sound planning and scheduling techniques. To mitigate this risk, our team will hold design and construction meetings twice a month to monitor progress. At a minimum, these meetings will focus on "what we have done", "what we need to do", and "what impact it has on schedule" and "what can be done to accelerate the remaining work". Through continuous oversight and schedule updates, potential schedule impacts can be adequately avoided.

The other risk element associated with TMP involves the pedestrian trails and is a bit more challenging. The current location of the pedestrian trail connection to the Lick Run Greenway trail directly conflicts with the extension of Valley View Boulevard to the south. Furthermore, the relocated pedestrian trail connection also conflicts with the current trail location. This makes maintaining pedestrian access difficult without significant, temporary trail relocation. As a result, more land will be disturbed and the cost of a new, temporary trail is presently undetermined. Trail grade to meet ADA and avoiding disruption to the existing trail operation are priorities. Simply put, the consequent risk is the cost, the constructability and the geographic constraints surrounding the pedestrian trails.

Mitigation Strategy: Because of the visible nature of TMP (maintenance of traffic specifically), our team will take aggressive

measures to ensure the safety of the public (vehicular and pedestrian) and our team, as well as, to ensure that traffic flow operates at an acceptable level for this roadway. We will have alternative implementation strategies to employ should a breakdown occur in our TMP field operations. However, the best tool we have to mitigate risks is the synergy developed between AI-VA and RDA. This synergy extends to an integrated design and construction staff singularly focused from project kick-off to construction completion. AI-VA and RDA are currently applying this strength on our Middle Ground Boulevard Extension Design-Build project. Our staffs have integrated into a cohesive group to resolve design and construction issues logically and cost effectively. By having this unified approach, we have found opportunities to improve our strategies that will avoid and minimize the potential for problems in the field.

Geotechnical Considerations/Constraints:

Froehling & Robertson (F&R) has evaluated the Geotechnical Data Report provided on the Additional Information CD to establish the potential for risk regarding subsurface conditions. There are always numerous elements in this region of the Commonwealth that raise geotechnical concerns.

Large embankment loads over underlying compressible soils: Review of provided data show there are high compacted fill depths (embankment fills of 30 feet or greater) that will be required for the Valley View Boulevard extension and new ramp construction. The area load induced on the underlying subgrade by such deep fill depths often results in high magnitudes of settlement of the underlying soils and may take a reasonably long period of time for completion of the primary settlement. This presents a significant geotechnical risk that impacts the design of embankments and walls and the construction schedule.

Karst terrain/conditions: This is very common to the area. As a result, the design and construction

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team must be prepared to address the use of highly plastic soils (CH and MH), highly variable bedrock conditions and elevations, and potential for sinkholes to open up due to construction activities. Highly plastic soils are commonly used in construction in the area for structural fill. The exception is for use behind retaining walls; typically CH and MH soils are not recommended and a select material or a more granular soil with lower plasticity will be required. The anticipated variability of the bedrock will affect the selection, design and construction of the foundation support elements (drilled shafts, etc.).

Corrosivity for culverts and storm sewers:

Based on the available lab test data, it appears that a lot of the existing subsurface soils will exhibit resistivity values less than 2000 ohms/cm. This will affect the type of suitable materials used in culvert/underground utility products which may affect design approach, cost and schedule.

Residual soils in the area of the Valley View Mall:

Note that there are pockets of soils with the Valley and Ridge Province that have a mineral make up that causes standard nuclear gauge test results to be erroneous. One such documented site is the area around the Valley View Mall. In the area of the project site, it is common for the nuclear gauge to provide passing density values when visual evaluation of the materials performance under applied loads (construction equipment) indicates unsatisfactory conditions or vice versus. During construction and the related QC/QA field testing, it is important to at least recognize this fact ahead of time and develop a plan to address it should it present itself.

Mitigation Strategy: These collective geotechnical risks will affect design and construction alike, both in cost and schedule. We will invest in geotechnical analyses that exceed normal requirements in order to define where

issues may exist and to develop early strategies. We will also develop a detailed risk plan to help identify and address issues during the early stages of the project, before schedule or material cost impacts. The earlier we address these findings, the greater the potential to modify the design or develop innovative solutions.

VDOT/Agency Role in Mitigation Strategies:

Other than VDOT's typical role on a design-build project, we do not anticipate additional roles/measures from VDOT or other agencies to successfully mitigate these risks. However, we do believe expedited reviews and a collaborative, unified team approach are essential. The AI Team will partner with VDOT and stakeholders to work as a unified "team" with the singular goal to deliver this project timely, meeting the needs of the community and the goals of VDOT. One of the tools used to accomplish this will be quarterly formal partnering meetings to discuss "rocks in the road" and develop an action plan to mitigate risks.

AI-VA is experienced in partnering and strongly believes in the value this process imparts to the project and all participants. Our experience can be summarized as **Start Right - Stay Right, Manage Change, Manage Conflict**. Unlike contracts which establish "legal" relationships, partnering establishes "working" relationships. Participant requirements include commitment, trust, mutual goals, timely response and continuous evaluation of the project. When these requirements are understood and shared by partners, the results are substantial. Products of successful partnering include reduced project costs, mitigated impacts, shortened schedule and ensured compliance with project objectives. Because this process fosters greater levels of collaboration, projects tend to run more efficient and are more productive.

"In providing this forum [formal partnering], the day to day operations have run smoother as just a simple phone call can precipitate action needed by other entities to help resolve issues that arise frequently on these types of projects, and this seems to be providing for more timely action.

- Michael Johnson – VDOT Construction Manager; Hampton Boulevard Grade Separation

ATTACHMENT NO. 3.2.5(a)

**CERTIFICATION REGARDING DEBARMENT
PRIMARY COVERED TRANSACTIONS**

Project No.: 0581-128-109, P101, RW201, C501, B627

1) The prospective primary participant certifies to the best of its knowledge and belief, that it and its principals:

a) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any Federal department or agency.

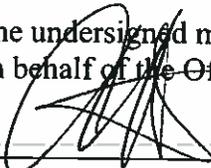
b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; and have not been convicted of any violations of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification, or destruction of records, making false statements, or receiving stolen property;

c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph 1) b) of this certification; and

d) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

2) Where the prospective primary participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

	11/28/2011	Vice President/General Manager
Signature	Date	Title
Aaron T. Myers		
American Infrastructure-VA, Inc.		

Name of Firm

ATTACHMENT NO. 3.2.5(a)

**CERTIFICATION REGARDING DEBARMENT
PRIMARY COVERED TRANSACTIONS**

Project No.: 0581-128-109, P101, RW201, C501, B627

1) The prospective primary participant certifies to the best of its knowledge and belief, that it and its principals:

a) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any Federal department or agency.

b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; and have not been convicted of any violations of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification, or destruction of records, making false statements, or receiving stolen property;

c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph 1) b) of this certification; and

d) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

2) Where the prospective primary participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

<u>Dale R. Wilson</u>	<u>11/28/2011</u>	<u>Dale R. Wilson, President</u>
Signature	Date	Title

Allan A. Myers, L.P.

Name of Firm

ATTACHMENT NO. 3.2.5(a)

**CERTIFICATION REGARDING DEBARMENT
PRIMARY COVERED TRANSACTIONS**

Project No.: 0581-128-109, P101, RW201, C501, B627

1) The prospective primary participant certifies to the best of its knowledge and belief, that it and its principals:

a) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any Federal department or agency.

b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; and have not been convicted of any violations of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification, or destruction of records, making false statements, or receiving stolen property;

c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph 1) b) of this certification; and

d) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

2) Where the prospective primary participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

Dale R. Wilson 11/28/11 President
Signature Dale R. Wilson Date Title

American Infrastructure-MD, Inc.
Name of Firm

ATTACHMENT NO. 3.2.5(b)

**CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS**

Project No.: 0581-128-109, P101, RW201, C501, B627

- 1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

- 2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.



_____ Signature	November 2, 2011 Date	_____ President Title
--------------------	--------------------------	-----------------------------

NXL Construction Services, Inc.

Name of Firm

ATTACHMENT NO. 3.2.5(b)

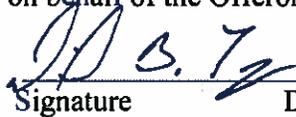
**CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS**

Project No.: 0581-128-109, P101, RW201, C501, B627

- 1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

- 2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

	12/9/11	Principal
Signature	Date	Title

Clark Nexsen, PC
Name of Firm

ATTACHMENT NO. 3.2.5(b)

**CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS**

Project No.: 0581-128-109, P101, RW201, C501, B627

1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

Donald J. Dupler 11/18/11 Regional Vice President
Signature Date Title

Froehling & Robertson, Inc.
Name of Firm

ATTACHMENT NO. 3.2.5(b)

**CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS**

Project No.: 0581-128-109, P101, RW201, C501, B627

1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

William Harris Eubank Nov. 6, 2011 Vice President -
Signature Date Title Transportation

Anderson & Associates of Virginia, Inc.
Name of Firm

ATTACHMENT NO. 3.2.5(b)

**CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS**

Project No.: 0581-128-109, P101, RW201, C501, B627

- 1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.
- 2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

James Rutledge 12/2/11
Signature Date

PRINCIPAL / REGIONAL DIRECTOR
Title

PULSAR ADVERTISING
Name of Firm

ATTACHMENT NO. 3.2.5(b)

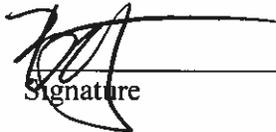
**CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS**

Project No.: 0581-128-109, P101, RW201, C501, B627

1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

	<u>11/30/11</u>	<u>VICE PRESIDENT</u>
Signature	Date	Title
<u>ECS MID-ATLANTIC, LLC</u>		
Name of Firm		



COMMONWEALTH OF VIRGINIA



CERTIFICATE OF QUALIFICATION

American Infrastructure-VA, Inc.

Vendor Number: **G303**

In accordance with the Regulations of the Virginia Department of Transportation, you are hereby notified that the following Rating and Classifications has been assigned to you by the Commissioner:

PREQUALIFIED

Work Classes: Grading, Major Structures, Minor Structures, Asphalt Pavement, Roadway Milling, Surface Treatment

Issue Date: March 22, 2011

This Rating and Classification will Expire: January 31, 2012

Suzanne FR Lucas

Suzanne FR Lucas Prequalification Officer

Don E. Silies For

Don E. Silies, State Construction Contract Officer



CISM0180

CORPORATE DATA INQUIRY

11/26/11

13:14:52

CORP ID: 0113780 - 1 STATUS: 00 ACTIVE STATUS DATE: 11/03/08
 CORP NAME: American Infastructure-VA, Inc.

DATE OF CERTIFICATE: 10/06/1967 PERIOD OF DURATION: INDUSTRY CODE: 00
 STATE OF INCORPORATION: VA VIRGINIA STOCK INDICATOR: S STOCK
 MERGER IND: CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: CT CORPORATION SYSTEM

STREET: 4701 COX RD STE 301 AR RTN MAIL:

CITY: GLEN ALLEN STATE : VA ZIP: 23060 6802
 R/A STATUS: 5 B.E. AUTH IN VI EFF. DATE: 01/05/04 LOC : 143
 ACCEPTED AR#: 211 16 3675 DATE: 09/20/11 HENRICO COUNTY
 CURRENT AR#: 211 16 3675 DATE: 09/20/11 STATUS: A ASSESSMENT INDICATOR: 0
 YEAR FEES PENALTY INTEREST TAXES BALANCE TOTAL SHARES
 11 670.00 100,000

(Screen Id:/Corp_Data_Inquiry)



CISM0180

CORPORATE DATA INQUIRY

11/26/11

13:14:17

CORP ID: 0227062 - 7 STATUS: 00 ACTIVE STATUS DATE: 04/22/91
 CORP NAME: Rinker Design Associates, P.C.

DATE OF CERTIFICATE: 02/24/1982 PERIOD OF DURATION: INDUSTRY CODE: 70
 STATE OF INCORPORATION: VA VIRGINIA STOCK INDICATOR: S STOCK
 MERGER IND: CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: JOHN S WISIACKAS
 ODIN FELDMAN & PITTLEMAN
 STREET: 9302 LEE HWY STE 1100 AR RTN MAIL:
 CITY: FAIRFAX STATE : VA ZIP: 22031 6054
 R/A STATUS: 4 ATTORNEY EFF. DATE: 08/28/03 LOC : 129
 ACCEPTED AR#: 211 01 5824 DATE: 12/22/10 FAIRFAX COUNTY
 CURRENT AR#: 211 01 5824 DATE: 12/22/10 STATUS: A ASSESSMENT INDICATOR: 0
 YEAR FEES PENALTY INTEREST TAXES BALANCE TOTAL SHARES
 11 190.00 20,000

(Screen Id:/Corp_Data_Inquiry)



Commonwealth of Virginia
State Corporation Commission

Virg

CISM0180

CORPORATE DATA INQUIRY

12/03/11

09:46:17

CORP ID: 0349742 - 7 STATUS: 00 ACTIVE STATUS DATE: 11/17/89
 CORP NAME: **NXL CONSTRUCTION CO., INC.**

DATE OF CERTIFICATE: 11/17/1989 PERIOD OF DURATION: INDUSTRY CODE: 00
 STATE OF INCORPORATION: VA VIRGINIA STOCK INDICATOR: S STOCK
 MERGER IND: CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: 50.00 MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: NICOMEDES L DE LEON

STREET: 9606 GEORGE'S BLUFF RD AR RTN MAIL:

CITY: RICHMOND STATE : VA ZIP: 23229
 R/A STATUS: 2 OFFICER EFF. DATE: 10/08/98 LOC : 143
 ACCEPTED AR#: 211 16 4444 DATE: 09/20/11 HENRICO COUNTY
 CURRENT AR#: 211 16 4444 DATE: 09/20/11 STATUS: A ASSESSMENT INDICATOR: 0
 YEAR FEES PENALTY INTEREST TAXES BALANCE TOTAL SHARES
 11 100.00 5,000

 (Screen Id:/Corp_Data_Inquiry)



CISM0180

CORPORATE DATA INQUIRY

11/26/11

13:10:29

CORP ID: 0190175 - 0 STATUS: 00 ACTIVE STATUS DATE: 05/16/89
 CORP NAME: CLARK, NEXSEN, OWEN, BARBIERI & GIBSON, P.C.

DATE OF CERTIFICATE: 11/27/1978 PERIOD OF DURATION: INDUSTRY CODE: 73
 STATE OF INCORPORATION: VA VIRGINIA STOCK INDICATOR: S STOCK
 MERGER IND: S SURVIVOR CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: 180.00 MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: CHRISTOPHER M STONE

STREET: 6160 KEMPSVILLE CIR STE 200A AR RTN MAIL:

CITY: NORFOLK STATE : VA ZIP: 23502
 R/A STATUS: 2 OFFICER EFF. DATE: 09/27/11 LOC : 212
 ACCEPTED AR#: 211 16 4604 DATE: 09/21/11 NORFOLK CITY
 CURRENT AR#: 211 16 4604 DATE: 09/21/11 STATUS: A ASSESSMENT INDICATOR: 0
 YEAR FEES PENALTY INTEREST TAXES BALANCE TOTAL SHARES
 11 670.00 100,000

(Screen Id:/Corp_Data_Inquiry)



CISM0180

CORPORATE DATA INQUIRY

12/03/11

09:48:13

CORP ID: 0027211 - 2 STATUS: 00 ACTIVE STATUS DATE: 11/13/09
 CORP NAME: FROEHLING & ROBERTSON, INCORPORATED

DATE OF CERTIFICATE: 10/11/1924 PERIOD OF DURATION: INDUSTRY CODE: 00
 STATE OF INCORPORATION: VA VIRGINIA STOCK INDICATOR: S STOCK
 MERGER IND: CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: 2480.00 MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: WILLIAM H HOOFNAGLE III

STREET: 1900 ONE JAMES CENTER AR RTN MAIL:
 901 E CARY ST

CITY: RICHMOND STATE : VA ZIP: 23219

R/A STATUS: 4 ATTORNEY EFF. DATE: 09/21/11 LOC : 216

ACCEPTED AR#: 211 16 6326 DATE: 09/23/11 RICHMOND CITY

CURRENT AR#: 211 16 6326 DATE: 09/23/11 STATUS: A ASSESSMENT INDICATOR: 0

YEAR	FEES	PENALTY	INTEREST	TAXES	BALANCE	TOTAL SHARES
11	1,700.00					1,100,000

(Screen Id:/Corp_Data_Inquiry)



CISM0180

CORPORATE DATA INQUIRY

11/26/11

13:12:49

CORP ID: 0171262 - 9 STATUS: 00 ACTIVE STATUS DATE: 12/06/76
 CORP NAME: ANDERSON & ASSOCIATES OF VIRGINIA, INC.

DATE OF CERTIFICATE: 12/06/1976 PERIOD OF DURATION: INDUSTRY CODE: 00
 STATE OF INCORPORATION: VA VIRGINIA STOCK INDICATOR: S STOCK
 MERGER IND: CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: S. K. ANDERSON

STREET: 100 ARDMORE STREET AR RTN MAIL:

CITY: BLACKSBURG STATE : VA ZIP: 24060
 R/A STATUS: 2 OFFICER EFF. DATE: 02/28/80 LOC : 160
 ACCEPTED AR#: 210 50 6806 DATE: 12/30/10 MONTGOMERY COUN
 CURRENT AR#: 210 50 6806 DATE: 12/30/10 STATUS: A ASSESSMENT INDICATOR: 0
 YEAR FEES PENALTY INTEREST TAXES BALANCE TOTAL SHARES
 11 370.00 370.00 50,000

(Screen Id:/Corp_Data_Inquiry)



Commonwealth of Virginia
State Corporation Commission

Virg

CISM0180

CORPORATE DATA INQUIRY

12/03/11

09:49:05

CORP ID: F160855 - 5 STATUS: 00 ACTIVE STATUS DATE: 11/22/04
 CORP NAME: PULSAR ADVERTISING, INC.

DATE OF CERTIFICATE: 11/22/2004 PERIOD OF DURATION: INDUSTRY CODE: 00
 STATE OF INCORPORATION: NY NEW YORK STOCK INDICATOR: S STOCK
 MERGER IND: CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: 50.00 MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: CT CORPORATION SYSTEM

STREET: 4701 COX RD STE 301 AR RTN MAIL:

CITY: GLEN ALLEN STATE : VA ZIP: 23060 6802
 R/A STATUS: 5 B.E. AUTH IN VI EFF. DATE: 11/22/04 LOC : 143
 ACCEPTED AR#: 211 19 6959 DATE: 11/29/11 HENRICO COUNTY
 CURRENT AR#: 211 19 6959 DATE: 11/29/11 STATUS: A ASSESSMENT INDICATOR: 0
 YEAR FEES PENALTY INTEREST TAXES BALANCE TOTAL SHARES
 11 100.00 200

 (Screen Id:/Corp_Data_Inquiry)

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA

EXPIRES ON

12-31-2012

NUMBER

2701 009872A

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

BOARD FOR CONTRACTORS
CLASS A CONTRACTORS LICENSE

AMERICAN INFRASTRUCTURE - VA INC

44209 WADE DRIVE

CHANTILLY VA 20152

CLASSIFICATIONS H/H

ALTERATION OF THIS DOCUMENT, USE AFTER EXPIRATION, OR USE BY PERSONS OR FIRMS OTHER THAN THOSE NAMED MAY RESULT IN CRIMINAL PROSECUTION UNDER THE CODE OF VIRGINIA.

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(POCKET CARD)

COMMONWEALTH OF VIRGINIA

BOARD FOR CONTRACTORS - CLASS A
CONTRACTOR LICENSE - CLASSIFICATIONS: H/H



NUMBER: 2701 009872A EXPIRES: 12-31-2012
AMERICAN INFRASTRUCTURE-VA INC

44209 WADE DRIVE



Gordon N. Dixon
Gordon N. Dixon, Director

(DETACH HERE)

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233

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**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

EXPIRES ON

02-29-2012

NUMBER

0410000220

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

**BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS**

PROFESSIONAL CORPORATION BRANCH OFFICE REGISTRATION

PROFESSIONS: ENG

**RINKER DESIGN ASSOCIATES PC
301 CONCOURSE BLVD, STE 120
GLEN ALLEN, VA 23059**



Gordon N. Dixon
Gordon N. Dixon, Director

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA

EXPIRES ON

12-31-2011

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER

0405000502

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
PROFESSIONAL CORPORATION REGISTRATION

PROFESSIONS: ENG, LS

RINKER DESIGN ASSOCIATES PC
9300 WEST COURTHOUSE RD
STE 300
MANASSAS, VA 22110

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(POCKET CARD)

COMMONWEALTH OF VIRGINIA

BOARD FOR APELSCIDLA
PROFESSIONAL CORPORATION REGISTRATION
NUMBER: 0405000502 EXPIRES: 12-31-2011
PROFESSIONS: ENG, LS



RINKER DESIGN ASSOCIATES PC
9300 WEST COURTHOUSE RD
STE 300
MANASSAS, VA 22110



Jay W. DeFever
Jay W. DeFever Director

(DETACH HERE)

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233

**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

EXPIRES ON
02-29-2012

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: 1 (804) 367-8500

NUMBER
0410000156

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
PROFESSIONAL CORPORATION BRANCH OFFICE REGISTRATION

PROFESSIONS: ENG, LS

RINKER DESIGN ASSOCIATES PC
927 MAPLE GROVE DR STE 105
FREDERICKSBURG, VA 22407



Jay W. DeLover
Jay W. DeLover, Director

ALTERATION OF THIS DOCUMENT, USE AFTER EXPIRATION, OR USE BY PERSONS OR FIRMS OTHER THAN THOSE NAMED MAY RESULT IN CRIMINAL PROSECUTION UNDER THE CODE OF VIRGINIA.

(SEE REVERSE SIDE FOR NAME AND/OR ADDRESS CHANGE)

(POCKET CARD)
COMMONWEALTH OF VIRGINIA

(DETACH HERE)

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233

BOARD FOR APELSCIDLA
PROFESSIONAL CORPORATION BRANCH OFFICE REGISTRATION
NUMBER: 0410000156 EXPIRES: 02-29-2012
PROFESSIONS: ENG, LS



RINKER DESIGN ASSOCIATES PC
927 MAPLE GROVE DR STE 105
FREDERICKSBURG, VA 22407

**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

EXPIRES ON
02-29-2012

NUMBER
0411000678

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY BRANCH OFFICE REGISTRATION

PROFESSIONS: ENG, LS

NXL CONSTRUCTION CO INC
NXL CONSTRUCTION SERVICES INC.
2870-C SOUTH MAIN ST.
HARRISONBURG, VA 22801



Jan W. DeBorja
Jan W. DeBorja, Director

ALTERATION OF THIS DOCUMENT, USE AFTER EXPIRATION, OR USE BY PERSONS OR FIRMS OTHER THAN THOSE NAMED MAY RESULT IN CRIMINAL PROSECUTION UNDER THE CODE OF VIRGINIA.

(SEE REVERSE SIDE FOR NAME AND/OR ADDRESS CHANGE)

(POCKET CARD)

COMMONWEALTH OF VIRGINIA

BOARD FOR APELSCIDLA
BUSINESS ENTITY BRANCH OFFICE REGISTRATION
NUMBER: 0411000678 EXPIRES: 02-29-2012
PROFESSIONS: ENG, LS



NXL CONSTRUCTION CO INC NXL CONSTRUCTION SERVICES INC.
2870-C SOUTH MAIN ST.
HARRISONBURG, VA 22801

(DETACH HERE)

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233

ALTERATION OF THIS DOCUMENT, USE AFTER EXPIRATION, OR USE BY PERSONS OR FIRMS OTHER THAN THOSE NAMED MAY RESULT IN CRIMINAL PROSECUTION UNDER THE CODE OF VIRGINIA.

**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: 1 (804) 367-8500

EXPIRES ON

12-31-2011

NUMBER

0407003031

**BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION**

PROFESSIONS: ENG, LS

**NXL CONSTRUCTION CO INC
NXL CONSTRUCTION SERVICES INC
114 E CARY ST STE 200
RICHMOND, VA 23219**



Jay W. DeBoer
Jay W. DeBoer Director

**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

EXPIRES ON

02-29-2012

NUMBER

0410000130

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: 1 (804) 367-8500

**BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS**

PROFESSIONAL CORPORATION BRANCH OFFICE REGISTRATION

PROFESSIONS: ARC, ENG

**CLARK NEXSEN OWEN BARBIERI & GIBSON PC
CLARK NEXSEN
213 S JEFFERSON ST STE 1011
ROANOKE, VA 24011**



Jay W. DeBoer
Jay W. DeBoer Director

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(SEE REVERSE SIDE FOR NAME AND/OR ADDRESS CHANGE)

**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

EXPIRES ON

12-31-2011

NUMBER

04050000238

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: 1 (804) 367-8500

**BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
PROFESSIONAL CORPORATION REGISTRATION**

PROFESSIONS: ARC, ENG, LA

**CLARK, NEXSEN, OWEN, BARBIERI & GIBSON PC
CLARK NEXSEN
6160 KEMPSVILLE CIRCLE
SUITE 200A**

NORFOLK, VA 23502

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Jay W. DeBoer
Jay W. DeBoer Director

(SEE REVERSE SIDE FOR NAME AND/OR ADDRESS CHANGE)

**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

**EXPIRES ON
02-29-2012**

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: 1 (804) 367-8500

**NUMBER
0411000053**

**BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY BRANCH OFFICE REGISTRATION**

PROFESSIONS: ENG

**FROEHLING ROBERTSON INC
1734 SEIBEL DR N E
ROANOKE, VA 24012**



Jay W. DeBart
JAY W. DEBART
DIRECTOR

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COMMONWEALTH OF VIRGINIA

**BOARD FOR APELSCIDLA
BUSINESS ENTITY BRANCH OFFICE REGISTRATION
NUMBER: 0411000053 EXPIRES: 02-29-2012
PROFESSIONS: ENG**



**FROEHLING ROBERTSON INC
1734 SEIBEL DR N E
ROANOKE, VA 24012**

(DETACH HERE)

**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233**

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**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

**EXPIRES ON
12-31-2011**

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: 1 (804) 367-8500

**NUMBER
0407004805**

**BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION**

PROFESSIONS: ENG, LS, LA

**ANDERSON & ASSOCIATES OF VIRGINIA INC
100 ARDMORE STREET
BLACKSBURG, VA 24060**



Jay W. DeBrec
Jay W. DeBrec, Director

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**BOARD FOR APELSCIDLA
BUSINESS ENTITY REGISTRATION
NUMBER: 0407004805 EXPIRES: 12-31-2011
PROFESSIONS: ENG, LS, LA
ANDERSON & ASSOCIATES OF VIRGINIA INC
100 ARDMORE STREET
BLACKSBURG, VA 24060**



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**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233**

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**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
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EXPIRES ON

02-29-2012

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0411000381

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

**BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY BRANCH OFFICE REGISTRATION**

PROFESSIONS: ENG

ECS-MID-ATLANTIC LLC
5320 PETERS CREEK RD STE F
ROANOKE, VA 24019



Jay W. DeBoer
Jay W. DeBoer, Director

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COMMONWEALTH OF VIRGINIA**

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

EXPIRES ON
06-30-2012

NUMBER

0402023296

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
PROFESSIONAL ENGINEER LICENSE

DARELL LEE FISCHER
14101 SPRING GATE TERRACE
MIDLOTHIAN, VA 23112



Gordon N. Dixon
Gordon N. Dixon, Director

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COMMONWEALTH OF VIRGINIA**

EXPIRES ON

02-29-2012

NUMBER

0402039327

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

**BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
PROFESSIONAL ENGINEER LICENSE**

**JOSEPH ROY HAMED
110 WENN DRIVE
CHRISTIANSBURG, VA 24073**



Joy W. DeRoos
Joy W. DeRoos, Director

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(POCKET CARD) COMMONWEALTH OF VIRGINIA

(DETACH HERE)

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233

BOARD FOR APELSCIDLA
PROFESSIONAL ENGINEER LICENSE
NUMBER: 0402039327 EXPIRES: 02-29-2012



JOSEPH ROY HAMED
110 WENN DRIVE
CHRISTIANSBURG, VA 24073

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**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

EXPIRES ON

04-30-2013

NUMBER

0402025919

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

**BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
PROFESSIONAL ENGINEER LICENSE**

**ACHYUT G PATEL
729 QUEEN ELIZABETH DR
VIRGINIA BEACH, VA 23452**



Gordon N. Dixon
Gordon N. Dixon, Director

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**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

EXPIRES ON

02-28-2013

NUMBER

4008 001684

**REAL ESTATE APPRAISER BOARD
BUSINESS REGISTRATION**

**RINKER DESIGN ASSOCIATES PC
9300 W COURTHOUSE RD STE 300
MANASSAS VA 20110**



Gordon N. Dixon

Gordon N. Dixon, Director

**ATTACHMENT 3.3.1****KEY PERSONNEL RESUME FORM****Brief Resume of Key Personnel anticipated for the Project.**

a. Name & Title:

J. David Nardon, Project Executive

b. Project Assignment:

Design Build Project Manager

c. Name of Firm with which you are now associated:

American Infrastructure-VA, Inc.d. Years experience: With this Firm 3 Years With Other Firms 35 Years

Please list chronologically (most recent experience first) your employment history, position and general experience or fields of practice for the last fifteen (15) years. (NOTE: If you have less than 15 years of experience, please list all of your experience for those years you have worked.):

American Infrastructure-VA, Inc., Project Executive; Jan. 2009- Present: Mr. Nardon is a Senior Manager with American Infrastructure responsible for Alternative Delivery Projects. His responsibilities include preconstruction services and estimating, value engineering and innovative project delivery solutions, procurement management, resource management, DBE participation and coordination, construction operations management, and business development. Mr. Nardon brings a wealth of knowledge from his previous experiences to this position. He has successfully overseen the delivery of construction aspects of the projects providing cost effective and creative concepts for design-build proposals for AI in Virginia. These Virginia Design-Build projects include, the **Middle Ground Boulevard Extension** project (\$32.5M) in Newport News, the recently completed **Richmond Airport Connector Road** (\$39.0M) in Richmond, the **Route 29 Bridge Over the Tye River** project (\$6.6M) located in Amherst, Virginia and the Design-Bid-Build **VDOT Hampton Boulevard Grade Separation** project (\$39.0M) in Norfolk

The Walsh Group, Program-Operations Manager for Heavy Civil Division; Jan. 2006 – Dec. 2008: Mr. Nardon managed large complex construction operations for The Walsh Group, a 3rd generation, family owned construction business that has a nation-wide presence in GC, CM, and Design-Build services. Mr. Nardon was directly responsible for all aspects of construction operations including the establishment of management systems, supervision of projects in process and project management staff, preconstruction services and estimating, value engineering, scheduling, and quality control programs. Mr. Nardon was Program Manager representing the Walsh Group on a Granite lead Joint Venture, for the 2-mile, \$267M Bay St. Louis Bridge MS, a 22 month fast-track, design build reconstruction of a Katrina devastated bridge on the coast of MS. Mr. Nardon also led two major Design-Build initiatives as Program Director, the \$340M fast-track reconstruction of the Biloxi Bay Bridge, MS project, and the \$150M Indian River Bridge Replacement in DE. Mr. Nardon's division was responsible for several large bridge and rail projects ranging from \$120M to \$267M in contract value.

Edward Kraemer & Sons, Inc., Vice President of Mid-Atlantic Operations; Mar. 2003 – Dec. 2005: As Vice President of Mid-Atlantic Operations for Edward Kraemer & Sons, a leader in bridge construction throughout the United States for nearly 100 years, Mr. Nardon was responsible for developing construction operations on the East Coast. Mr. Nardon oversaw operations for the self-performing, hands-on construction company while also acting as a project sponsor for a Joint Venture project on the Woodrow Wilson Bridge.

Cianbro Corporation, Vice President/General Manager; Oct. 1997 – Mar. 2003: Mr. Nardon started as a regional manager for The Southern New England and New York Region and was responsible for projects in the heavy civil and structural, marine operations, and industrial sectors. In 2000, Mr. Nardon was promoted to VP/GM of the Southern New England and New York Region. Cianbro Corporation, established in 1949, is a multi-disciplined heavy civil/marine contractor based in Maine that self performs 90+% of their own work. In the position of VP/GM, Mr. Nardon was responsible for long-term strategic planning and development for the Region, P&L responsibility, and the direct management and facilitation of regional operations within a six-state area. This included overseeing safety management, operation management, and resource organization for personnel and equipment.

Kiewit Construction Group, Area Manager – Project Sponsor; 1996 – Oct. 1997: As a Management Sponsor with Kiewit Construction Group beginning in 1993, Mr. Nardon established an Area Office in Richmond Virginia. His responsibilities included overseeing and developing safety initiatives, training and development, project procurement, estimate organization and management, full operation management, client negotiation, project closeout and development of the business plan. Mr. Nardon managed numerous heavy-highway, civil, bridge structure, marine and piling, tunnels, and deep sewer projects in Virginia, Pennsylvania, and Maryland.

Notable Experience:

Design Build Projects
 Complex Bridge Structures
 Large Complex Fast-Track Projects
 Concrete Paving

Multi-phased Construction

Major Earthwork, Drill and Shoot Rock Excavation, Pre- Split
 Complex Heavy Traffic Interstate Widening
 Power Plant Construction
 Deep Sewers

Memberships: American Society of Civil Engineers

VDOT Steering Committee with Jimmy Mills (ret) to develop VDOT Quality Initiative Program 1994

e. Active Registration: Year First Registered/ Discipline/VA Registration #: **n/a**

- f. Document the extent and depth of your experience and qualifications relevant to the Project.
1. *Note your specific responsibilities and authorities for each assignment, not those of the firm.*
 2. *Note whether experience is with current firm or with other firm.*
 3. *Provide beginning and end dates for each assignment.*

(List at least (3), but no more than (5) relevant projects for which you have performed a similar function.)

895/Richmond Airport Connector Road, Richmond, VA, a Design Build Project

1. Mr. Nardon is the Design Build Project Manager providing overall management and senior oversight for design and construction of the \$39M connector road project. This 1.6 mile, four-lane highway project includes 4 bridges, 111,000 SF of MSE walls, grading and drainage, asphalt, and lighting and signage. Additional scope items include 422,737 CY of import, 4 box culvert extensions, 133,507 TN of stone base and paving. He is responsible for design and construction operations, QA & QC, and, customer coordination and customer satisfaction.
2. *American Infrastructure; Senior Project Executive* 3. 2009 – Present

Middle Ground Boulevard Extension Design-Build

1. Mr. Nardon is responsible for the overall contract administration, construction quality management and design of this \$32.5M design build project that involves widening of two highly congested primary roadways (Route 60 Warwick Avenue and Route 143 Jefferson Avenue) in the City of Newport News. Connecting these two major intersections includes installation of a bridge over CSX rail. Project scope includes, but is not limited to, the design and construction of: mainline four-lane divided highway, bridge construction, storm drainage, storm water management basin(s), right-of-way acquisition and relocations, utility relocations, reconstruction of connections along mainline, mainline shared use path, traffic signal installation, sanitary sewer pump station and landscaping.
Owner Contact: Mr. Thomas Druhot, Virginia DOT (757) 253-5367
2. *American Infrastructure; Senior Project Executive* 3. 2011 - Present

Hampton Boulevard Grade Separation Project

1. Mr. Nardon provides overall management and senior oversight for construction of the \$38M road and bridge project. This 7/10th mile improvement will provide six lanes for through traffic and left turn lanes as needed. The existing at-grade conflict with the Norfolk Southern/Norfolk Portsmouth Beltline railroad will be removed. The project consists of depressing existing Hampton Boulevard 35 feet below existing grade; constructing a new 350 m long underpass & retaining walls; two new at-grade bridges, a double-span steel bridge providing the railroad crossing, and a single span concrete bridge providing vehicle access into Norfolk International Terminal and Naval Air Station Gate 6. A six-lane detour roadway will provide access into both facilities during construction, while maintaining vehicle capacity. All existing utilities within the proposed roadway will be relocated. Mr. Nardon is responsible for construction operations, QA/QC, and customer coordination and customer satisfaction.
2. *American Infrastructure; Senior Project Executive* 3. 2009 – Present

Woodrow Wilson Bridge, Bascule Span, Maryland and Virginia

1. Mr. Nardon was the Project Sponsor for the \$195M expansion of the Woodrow Wilson Bridge. He provided executive overview of self-performed construction operations for the 4-leaf bascule, CIP-Post-tensioned “V” pier substructure, placement of 40,000 cy of concrete to build the largest bascule in North America. Mr. Nardon managed the project team and construction crews, ensured safety on job site, provided mentorship on items such as project controls and schedule management.
2. *Edward Kraemer & Sons, Vice President of Mid-Atlantic Operations* 3. 2003 – 2005

Woodrow Wilson Bridge, Maryland Approach Spans, Maryland

1. Mr. Nardon was the Project Sponsor for the \$215M expansion of the Woodrow Wilson Bridge Approach Spans. He also was a member of the Joint Venture Executive Committee for the project. Spanning the Potomac River, the 3000’ Wilson Maryland Approach, Mr. Nardon oversaw self-performed construction operations on this fast-tracked project consisting of ten, highly complex pre-cast, post-tensioned concrete “V” shaped piers of which 616 segments (50,000 cy’s) was cast on site. The superstructure consisted of 40 million lbs of structural steel, and 30,000 CY of concrete; and piling ranged from 54” steel up to 160’ long, to 36” up to 165’ long.
2. *Edward Kraemer & Sons, Inc, Vice President of Mid-Atlantic Operations* 3. 2003 – 2005

I-581 / Valley View Interchange Phase II



State Project No: 0581-128-109, P101, RW201, C501, B627 Federal Project No: NH-581-510351 Contract ID Number: C00016595D845

ATTACHMENT 3.3.1
KEY PERSONNEL RESUME FORM

Brief Resume of Key Personnel anticipated for the Project.

a. Name & Title:

Joseph R. Hamed, PE, LS, PMP, Project Manager/Quality Assurance Manager

b. Project Assignment:

Quality Assurance Manager

c. Name of Firm with which you are now associated:

NXL Construction Company, Inc. (d/b/a: NXL Construction Services, Inc.)

d. Years experience: With this Firm **1** Year. With Other Firms **21** Years.

Please list chronologically (most recent experience first) your employment history, position and general experience or fields of practice for the last fifteen (15) years. (NOTE: If you have less than 15 years of experience, please list all of your experience for those years you have worked.):

NXL Construction Services, Inc., Project Manager/Quality Assurance Manager, May 2011 - Present

Independent Quality Control Manager for joint Design-Build projects ensuring contract requirements and specifications are appropriately administered and applied, required quality control testing and independent quality assurance is carried out, contractor invoices are correct – all in accordance with the applicable VDOT standards/documents addressing construction, QC, QA, and IA. Ensures that the QA staff adequately trained and equipped for their scope of work. Documents non-conforming work and follows up with the Owner and Contactor to ensure that corrective action is proposed by the contractor, approved by the owner, and completed with quality.

Virginia Department of Transportation, Area Construction Engineer, Salem District, Jan. 2011 – May 2011

In this role, Mr. Hamed managed the delivery of the Salem District Southern Construction Area's Construction Program. Mr. Hamed managed projects that include drainage, grading, placing, bridges, bridge superstructure/substructure restorations, signal, guardrail, pavement markings, utilities, etc. Key aspects of Mr. Hamed's responsibilities included: identifying and communicating with stakeholders and encouraging team member to communicate, identifying the need for extra work, reviewing and negotiating work order prices, and providing Responsible Charge oversight to ensure that each project was construction in conformance with the plans, specifications and standards. He also provided VDOT oversight of a locally administered urban project.

Virginia Department of Transportation Southwest Regional Operations, Program Delivery Manager, Oct. 2006 – Jan. 2011

In this role, Mr. Hamed provided oversight of all SW Regional Operations project delivery in all project phases, including planning, programming, project development and construction. He provided support for traffic signal projects, traditional traffic engineering projects, 2 mountain tunnel facilities (including fire, life and safety systems), and also provided support for technology projects including cameras, message boards, and FHWA Rule 940 Compliance. Mr. Hamed coordinated with SW Regional staff to identify and prioritize potential projects. In conjunction with Central Office and Regional staff from three Districts (Bristol, Lynchburg and Salem), Mr. Hamed identified funding sources for chosen projects, requested funding transfers, and initiated projects for within the system. He also provided oversight of the PE process to ensure that projects were developed in accordance with VDOT processes. He coordinated with other sections including Environmental, Right of Way, Location and Design, Scheduling and Contracts, Federal Highway Administration and Construction. Mr. Hamed provided responsible charge oversight for the Regional Signal Contract – Design and Construction - including approval of estimates, negotiating work orders, coordinating inspection, and claims avoidance. Provided claims support to the Salem District construction staff to prepare for and defend a claim against an earlier project; provided testimony at the Commissioner's hearing.

Virginia Department of Transportation, Area Construction Engineer, May 2005 – Oct. 2006

In this role, Mr. Hamed managed the delivery of the Salem District Northern Construction Area's construction program. His team managed construction and maintenance projects with a goal to deliver each project on time and on budget, with quality in the constructed project. In this regard, the team exceeded the expectation and percentages established by the Department's executive leadership. During his tenure in this position, Mr. Hamed managed projects that include drainage, grading, bridge construction, bridge superstructure/substructure restorations, signal, guardrail, pavement markings, utilities, etc. He managed the Preliminary Engineering of a Roadway Lighting System, now under construction. Key aspects of Mr. Hamed's responsibilities included: identifying and communicating with stakeholders and encouraging team member to communicate, identifying the need for extra work, reviewing and negotiating work order prices, and providing Responsible Charge oversight to ensure that each project was constructed in conformance with the plans, specifications and standards. Mr. Hamed provided input to the Preliminary Engineering process by attending various meetings to address unique project specific problems. Responsible for the project budget during the

Construction Phase, he monitored expenditures and compared them to the budgeted amounts on a monthly basis. Mr. Hamed provided Leadership that set the tone for a positive and productive work environment, with a staff consisting of a Staff Assistant, Construction Project Managers, VDOT Inspectors, and Consultant Inspectors.

Virginia Department of Transportation, Salem District, Project Manager, Aug. 2004 – May 2005

In this role, Mr. Hamed provided constructability, E&S and safety reviews for several projects in various phases including design and construction. He also provided project management and engineering analysis on a variety of projects, including the consultant design of the I-81 Lighting Project, Montgomery County, Virginia and the design and construction of the Stony Creek Emergency Contract, Giles, County, Virginia.

HNTB Corporation, Resident Engineer/PPTA Project, Mar. 2004 – July 2004

In this role, Mr. Hamed's duties included: documenting progress, providing reports to various design/build stakeholders, including VDOT, the prime contractor (Branch Highways), and the design office of HNTB. Mr. Hamed performed E&S inspections, recommended E&S preventive measures, coordinated problems and permits with DEQ, Corps of Engineers, and VDOT, and also collected and forwarded data required by DEQ and Corps of Engineers, including pH, temperature, dissolved oxygen, etc. He documented that the work met the proper VDOT specifications and standards, and documented, logged, tracked and forwarded any nonconforming items. Mr. Hamed provided interpretation and clarification of plans and specifications by coordinating closely with the design engineer. He also provided engineering support and analysis for a wide range of problems with varying degrees of complexity, including undercut depths, drilled shaft modifications, and additional under drain requirements.

Louis Berger Group, Inc., Project Manager/Project Engineer; Apr. 1999-Jan.2004

In this role, Mr. Hamed's primary duty was to lead a team of consultant inspectors during the construction of the a \$40 million bypass highway with approximately 12 lane miles of new alignment and seven new bridges. He and his team documented that the project was constructed in accordance with the plans, specifications and the contract. He monitored the contractor's work with respect to schedule, cost and quality. Mr. Hamed was responsible for recommending solutions to problems, corrections for deficiencies encountered, acceptance or rejection of work, changes and extras. He was also responsible for preparation of monthly project progress reports for the Owner. Other project responsibilities included directing the daily activities of 10 project inspectors, all project administration, maintaining daily log and reports, reviewing and assessing alternate work methods, preparation of Contractor's monthly pay estimate, and monitoring workmanship, quality and safety. He also provided claims review, Notice of Intent analysis, cost analysis of work orders, and review of safety plans.

Vecellio and Grogan, Inc., Beckley, West Virginia, Project Engineer; 1996 – Apr. 1999

In this role, Mr. Hamed supported field and office operations in a variety of assignments including: performed takeoffs, negotiated waste/borrow agreements, exploratory excavation and drilling, and other duties associated with estimating support. In the field he developed and managed an MS Access data base to record, track and report on production data. He also reviewed monthly estimates, ordered materials/parts, supported day-to-day blasting and hauling operations, monitored and repaired traffic controls, interfaced with project inspectors, submitted shop drawings, and prepared/updated CPM schedules.

e. Education: Name & Location of Institution(s)/Degree(s)/Year/Specialization:
University of Idaho/BS/1990/ Civil Engineering

f. Active Registration: Year First Registered/ Discipline/VA Registration #:

2004/VA Professional Engineer/039327

WV Professional Engineer/012756 ~ WV Professional Land Surveyor/1574

g. Document the extent and depth of your experience and qualifications relevant to the Project.

1. *Note your specific responsibilities and authorities for each assignment, not those of the firm.*
2. *Note whether experience is with current firm or with other firm.*
3. *Provide beginning and end dates for each assignment.*

(List at least three (3), but no more than five (5) relevant projects for which you have performed a similar function.)

I-81 Safety Improvement Project (Truck Climbing Lanes), Montgomery County, Virginia

I This \$75 million project consists of adding an additional lane onto I-81 through five miles of rugged terrain. The project scope includes drainage, grading, paving, demolition/reconstruction of two overpass bridges, phased demolition/construction of one mainline bridge, retaining walls, guardrails, and related appurtenances. As the Quality Assurance Manager, Mr. Hamed provides QA Inspection and Testing for the CH2M Hill Construction team. Services include performance of QA testing and inspection in accordance with VDOT's August 2008 Design Build Guidelines and the project's approved Quality Assurance and Quality Control Plan, the preparation, maintenance and submission of associated project documentation including but not limited to diaries, EEO, materials notebook/documentation, as-built sketches, monthly pay documents including verifying and approving monthly pay packages, and preparation and submission of final records. Monthly progress meetings, documentation and assures

that non-conforming work is addressed through approved methods of correction. Coordinates with the QC Manager, Construction and Project Managers (CH2M Hill and VDOT) on a variety of issues related to quality, schedule, and payment.

Client/Owner Contact: Richard Caywood, PE, VDOT Salem District Administrator (540) 387-5320

- 2 *NXL Construction Services, Inc., Quality Assurance Manager* 3 2011 – Present

Route 60/Main Street Bridge Replacement, Clifton Forge, Virginia.

- 1 A unique bridge replacement project in that the bridge superstructure abuts commercial buildings on both sides requiring careful consideration of demolition, construction and maintaining public access to buildings. The scope of the project includes complete demolition of existing bridge, reconstruction of the bridge and approaches, a signal light, protection of Smith Creek, and associated traffic management plan. As the Quality Assurance Manager, Mr. Hamed provides QA Inspection and Testing for the Orders Construction team. Services include performance of QA testing and inspection in accordance with VDOT's August 2008 Design Build Guidelines and the project's approved Quality Assurance and Quality Control Plan, the preparation, maintenance and submission of associated project documentation including but not limited to diaries, EEO, materials notebook/documentation, as-built sketches, monthly pay documents including verifying and approving monthly pay packages, and preparation and submission of final records. Attends monthly progress meetings, documents non-conforming work, assures and documents that non-conforming work is addressed through approved methods of correction. Coordinates with the QC Manager, Construction Manager, Project Managers (Orders and VDOT) on a variety of issues related to quality, schedule, and payment.

Client/Owner Contact: Randy Kiser, PE, VDOT Staunton District Administrator (540) 332-9092

- 2 *NXL Construction Services, Inc., Quality Assurance Manager* 3 2011 – Present

Region 3 Multiple Bridge Rehabilitation Project; Staunton District, Culpeper District, and NOVA District, VA

- 1 Regional VDOT/ARRA Design/Build Bridge Rehabilitation project comprised of 23 bridges located in three VDOT Districts with duration of approximately 22 calendar months of construction-related activities requiring QA Inspection and Testing for the Infrastructure Corporation of America, Inc. (ICA) Team. Mr. Hamed serves as the project's Quality Assurance Manager. Services include performance of QA testing and inspection in accordance with VDOT's August 2008 Design Build Guidelines and the project's approved Quality Assurance and Quality Control Plan, the preparation, maintenance and submission of associated project documentation including but not limited to diaries, EEO, ARRA, materials notebook/documentation, as-built sketches, monthly pay documents including verifying and approving monthly pay packages, and preparation and submission of final records. Attends monthly progress meetings, documents non-conforming work, assures and documents that non-conforming work is addressed through approved methods of correction. Coordinate with the QC Manager, Construction Manager, Project Managers (ICA and VDOT), and Design Project Manager on a variety of issues related to quality, schedule, and payment.

Client/Owner Contact: David A. Steele, PE, VDOT Innovative Project Delivery Division (804) 371-6787

- 2 *NXL Construction Services, Inc., Quality Assurance Manager* 3 2011 – Present

US Route 460 Christiansburg Bypass, Montgomery County, Virginia

- 1 This \$40 million project provided a 4-lane bypass with 12 lane-miles on new alignment, two new major interchanges connecting to urban streets, and renovation of the urban streets. The scope of the project included grading, drainage, seven new bridges, paving, sound walls, traffic signals, retaining walls, guardrail, utility relocation, railroad coordination, and appurtenances. Mr. Hamed managed a team of approximately 10 consultant inspectors to document that the project was constructed in accordance with the plans, specifications, and applicable standards. He monitored the contractor's work with respect to schedule, cost and quality. Mr. Hamed was responsible for recommending solutions to problems, corrections for deficiencies encountered, acceptance or rejection of work, changes and extras. He was also responsible for preparation of monthly project progress reports for the Owner. Other project responsibilities included directing the daily activities of the project inspectors, project administration, maintaining daily log and reports, reviewing and assessing alternate work methods, preparation of Contractor's monthly pay estimate, and monitoring workmanship, quality and safety. He also provided claims review, Notice of Intent analysis, cost analysis of work orders, and review of safety plans. Mr. Hamed coordinated with a host of stakeholders including the VDOT Christiansburg Residency, the Contractor Project Manager, Salem District Bridge Office, Salem District Traffic Engineering, Town of Christiansburg, Montgomery County, and utility owners.

Client/Owner Contact: Richard Caywood, PE, VDOT Salem District Administrator (540) 387-5320

- 2 *The Louis Berger Group, Consultant Project Manager* 3 May 1999 – Jan. 2004



ATTACHMENT 3.3.1

KEY PERSONNEL RESUME FORM

Brief Resume of Key Personnel anticipated for the Project.	
a. Name & Title:	Darell L. Fischer, P.E., Principal/General Manager (Richmond Office)
b. Project Assignment:	Design Manager
c. Name of Firm with which you are now associated:	Rinker Design Associates, P.C.
d. Years experience: With this Firm <u>4</u> Years With Other Firms <u>21</u> Years Please list chronologically (most recent experience first) your employment history, position and general experience or fields of practice for the last fifteen (15) years. (NOTE: If you have less than 15 years of experience, please list all of your experience for those years you have worked.):	
	<i>General Manager / Principal, Rinker Design Associates, P.C. 2011-Present</i> Responsible for allocating, overseeing and managing all designs performed in the Richmond Office (i.e. roadway design, hydrology/hydraulic analysis, traffic analysis and design, and construction plan preparation, R/W acquisition, utility coordination/design, etc.). Duties include QA/QC for services provided by the Richmond Office, oversight of all subconsultant work and coordination with clients to ensure client satisfaction and product quality.
	<i>Director of Transportation, Rinker Design Associates, P.C. 2007-2010</i> Responsible for overseeing and managing all design elements associated with roadway design, hydrology/hydraulic analysis, traffic analysis and design, and construction plan preparation. Duties include Quality Assurance and Quality Control (QA/QC) for services provided out of the Fredericksburg Office, oversight of all subconsultant work and coordination with clients to ensure client satisfaction and product quality.
	<i>Vice President/Branch Manager, Johnson, Mirmiran & Thompson, Inc. 2000-2007</i> Responsible for obtaining the work, executing the work and ensuring the quality of all work produced by the Richmond Office of JMT, oversight of all disciplines of work to include: roadway, drainage, structures, survey, construction inspection and environmental. Additionally, responsible for contractual obligations with clients and subconsultants as well as project management on many key projects. Responsible for the daily office operations to include: hiring, firing, raises, evaluations, dispute resolution, resource allocation, manpower projections and marketing.
	<i>Senior Project Manager, Carter & Burgess, Inc. 1998-2000</i> Responsible for the design and management of projects associated with roadway and H&HA designs. Duties included daily coordination with design staff, coordination with subconsultants and coordination with clients. Duties also included providing design changes during construction due to changed field conditions.
	<i>Senior Associate/Project Manager, Johnson, Mirmiran & Thompson, Inc. 1996-1998</i> Responsible for overseeing the design of roadway and drainage projects in Virginia. Provided mentoring, professional guidance and problem solving for all of JMT's Virginia staff. Developed QA/QC procedures for internal work as well as reviewing subconsultant work. Provided recommendations for teaming opportunities. Helped to develop marketing strategies and assisted in the hiring of new employees.
e. Education: Name & Location of Institution(s)/Degree(s)/Year/Specialization:	Virginia Polytechnic Institute and State University (Blacksburg, VA) / BS / 1986 / Civil Engineering
f. Active Registration: Year First Registered/ Discipline/VA Registration #:	1992 / Professional Engineer / #23296

- g. Document the extent and depth of your experience and qualifications relevant to the Project.
1. *Note your specific responsibilities and authorities for each assignment, not those of the firm.*
 2. *Note whether experience is with current firm or with other firm.*
 3. *Provide beginning and end dates for each assignment.*

(List at least three (3), but no more than five (5) relevant projects for which you have performed a similar function.)

Middle Ground Boulevard Extension (Design-Build, VDOT), City of Newport News, VA

1. Design Manager responsible for the design, management and design QA/QC for complete construction plans. Duties and responsibilities include the development of roadway design on new alignment, widening of highly congested, urban roadways, utility coordination, utility designs, closed storm drainage design in extremely flat terrain, SWM/BMP, TMP, signal designs and E&S. The plans are developed in work packages so that American Infrastructure-VA can initiate construction prior to final approval.
2. *Rinker Design Associates, P.C.*
3. *June 2011–Present*

Route 36 Improvements (Design-Build, VDOT), City of Hopewell and Prince George County, VA

1. Design Manager responsible for the design, management and QA/QC for complete construction plans. Duties and responsibilities include the development of roadway widenings and new alignments. Project responsibilities also include the design of open and closed storm drain systems, SWM, TMP, Signals and utility coordination/design. As design manager, it is my responsibility to oversee and provide design guidance on all design elements for Rinker Design and for sub-consultants. Also responsible for coordinating with Abernathy Construction, VDOT and each of the utility companies to ensure that the design requirements of the contract are being followed and the ensure that the design and associated services are expedited to provide Abernathy Construction the largest available time to construct the project – efficiently and under budget.
2. *Rinker Design Associates, P.C.*
3. *November 2008–2010*

Crosspointe Centre Roadway Improvements Design-Build (Rolls Royce), Prince George County, VA

1. Project Manager responsible for the design, management and QA/QC for complete construction plans. Duties include roadway design, traffic engineering, TMP/MOT, H&HA analysis, drainage design and phased erosion and sediment control. Also responsible for QC inspection during construction and coordination of geotechnical subcontractors for design and construction. The project involves 2.2 miles of new roadway construction and 1.5 miles of roadway widening. This Design-Build project began in November of 2008; design is complete (construction engineering continues) and construction is anticipated to be completed by November of 2010. The Virginia Economic Development Partnership (VEDP) through Transportation Partnership Opportunity Funds (TPOF) funds this project.
2. *Rinker Design Associates, P.C.*
3. *November 2008–2010*

James Madison Highway (Route 15) PPTA Design-Build, Prince William County (Haymarket), VA

1. Quality Control Reviewer responsible for independent reviews of the plans and computations at each milestone for all phases of work. QC reviews included plan quality, plan content and plan constructability. The project involves approximately 5 miles of roadway widening and roads on new locations amounting to \$54 million. Although this project is a Prince William County-administered project, responsibilities included close coordination with VDOT to address and integrate their comments and suggestions for plan acceptance.
2. *Rinker Design Associates, P.C.*
3. *February 2007–2009*

Russell Branch Parkway, Loudoun County, VA

1. Project Manager responsible for the design and management of the roadway design features, coordination with the County through the CPAP process, coordination with VDOT through the Land Development Review process, coordination with utility owners and coordination with adjacent developers for access and correlation of adjacent projects. Project features include a detailed H&HA study, multi-cell box culvert, sidewalk, multi-use trail and extensive coordination with an adjacent/impacted church. The roadway is being designed on new location with two lanes in each direction, curb and gutter and offsite Storm Water Management.
2. *Rinker Design Associates, P.C.*
3. *October 2008– Present*

**ATTACHMENT 3.3.1****KEY PERSONNEL RESUME FORM****Brief Resume of Key Personnel anticipated for the Project.**

a. Name & Title:
Robert Ackley, Senior Project Manager

b. Project Assignment:
Construction Manager

c. Name of Firm with which you are now associated:
American Infrastructure-VA, Inc.

d. Years experience: With this Firm 8 Years With Other Firms 17 Years
Please list chronologically your employment history, position and general experience or fields of practice for the last fifteen (15) years.

American Infrastructure, Project Executive; 2003- Present: Mr. Ackley is a Senior Project Manager for American Infrastructure, a heavy civil and site construction company in the Mid-Atlantic region. His responsibilities include estimating, proposal preparation, quality control and management of all aspects of construction projects. He oversees the construction activities in the field to ensure project delivery that meets or exceeds all expectations of quality, timeliness and budget.

New Construction, Inc; 2000 - 2003: Mr. Ackley was a Project Manager for New Construction Inc., a civil contractor in Northern Virginia. His responsibilities included managing all aspects of VDOT construction projects, estimating and proposal preparation.

Virginia Department of Transportation; 1996 - 2000: Mr. Ackley began his career at Virginia DOT, Northern Virginia District as an Engineering Technician in 1994. He performed Geological Surveys for roadway projects. He was also responsible for maintaining records of boring logs, soil classifications, and rock quality indexes. He oversaw subcontractors and authorized invoices. In 1996, Mr. Ackley was promoted into the role of Transportation Inspector for the Northern Virginia District where he supervised construction of roadways, drainage, box culverts, and bridges. Mr. Ackley prepared monthly estimates, approved change orders, and plan changes. In 1998, Mr. Ackley became the Engineering Technician Supervisor for the Northern Virginia District. In this role, his responsibilities increased to include management of QC testing of construction materials and methods for the Federal Highway Administration, evaluation soils for stability and recommendation of remedies for correction of unsuitable subgrade materials. He also administered the Radiation Safety Program and maintained records of transportation of Nuclear Density gauges, leak wipe testing and calibration. Mr. Ackley performed foundation inspections to ensure required bearing was achieved and he provided technical advice to field staff with overall function of nuclear testing.

e. Education: Degree(s)/Year/Specialization:
King George High School 1989 Construction Management Certificate - Drexel University 2012

f. Active Registration: Year First Registered/ Discipline/VA Registration #:
2008/Erosion and Sediment Control Contractor Certification/ #5141C
2008/Virginia DCR Responsible Land Disturber Certification/#36835

g. Document the extent and depth of your experience and qualifications relevant to the Project.
1. *Note your specific responsibilities and authorities for each assignment, not those of the firm.*
2. *Note whether experience is with current firm or with other firm.*
3. *Provide beginning and end dates for each assignment.*

(List at least (3), but no more than (5) relevant projects for which you have performed a similar function.)

VDOT Route 60 German School Rd

1. Responsible for overall construction activities, construction quality management and contract administration required for the construction of the \$38M highway project. Specific construction activities included construction of 2.5 miles of six-lane roadway, installation of gas, water, sanitary, storm sewer, irrigation, landscaping and lighting. Managed the planning and coordination of crews, subcontractors and suppliers. Mr. Ackley managed the coordination of Verizon, Dominion Power and the City of Richmond utility relocations. Planned work schedules for crews, subcontractors, and suppliers as they worked around the clock. Was responsible for shifts going on 24 hours a day, with the majority of the work occurring during off-peak hours. Coordinated lane closures and correspondingly communicated traffic pattern information with commuters using VDOT smart traffic system. He used innovative planning techniques to coordinate activities.

Owner Contact: Mr. Steven Sears, Engineer; Virginia DOT (804) 239-7650

2. *American Infrastructure; Project Manager*

3. 2010 – Present

Watkins Center Parkway at West Chester Commons

1. Mr. Ackley was responsible for overall construction activities, construction quality management, and contract administration required for the completion of the \$49M commercial and highway project. The 140-acre site now holds a large retail center and nearly three miles of new roadway. The roadway improvements included widening of Route 60 from two lanes to six lanes and intersection and ramp improvements at Route 288 and Watkins Center Parkway. Mr. Ackley oversaw construction activities including 3 million CY of excavation, 90,000 tons of stone sub-base and 90,000 tons of asphalt paving. . Because of the size and scope of this development, Mr. Ackley was responsible for a great deal of utility coordination in his role as Construction Manager. Both Verizon and Dominion Power were working directly for the owner however, AI was charged with the task of scheduling their installation and providing the infrastructure they needed. From the start of the project, Mr. Ackley organized and led weekly planning meetings with the Construction Managers from all utility companies to focus on coordination. These weekly meetings were held routinely, throughout the duration of the project. The collaboration between AI and the utility companies allowed for the successful installation of over 30,000 lf of conduit for electric, gas, and telephone. Mr. Ackley oversaw the installation of the utility infrastructure that AI provided including duct banks, conduit installation, junction boxes, vaults, and transformer pads.

Mr. Ackley also coordinated the MOT plan for this project. The roadway improvements included two major interchanges with Route 288 (a limited access highway). He planned for and managed numerous traffic shifts, daily lane closures and the coordination with VDOT smart traffic communication system. He also coordinated media notifications of the traffic shifts and other pertinent information for commuters.

Owner Contact: Mr. Allan Bellis; Zaremba Metropolitan, LLC, (216) 221-6600

2. *American Infrastructure; Construction Manager*

3. 2007 – 2009

Springfield Interchange/Loisdale Road

1. **Responsibilities:** Responsible for overall construction activities, construction quality management, and contract administration required for the completion of the \$14M project. Specific construction activities included constructing new exit lanes, soundwalls, drainage and paving. Additionally, he managed the planning and coordination of the crews, subcontractors and suppliers. Mr. Ackley managed the complicated MOT plan for the work on I-95 as New Construction build this new off ramp. Planned work schedules for crews, subcontractors, and suppliers as they worked around the clock. He was responsible for shifts going on 24 hours a day, with the majority of the work occurring during off-peak hours. Coordinated lane closures and correspondingly communicated traffic pattern information with commuters using VDOT smart traffic system. Mr. Ackley used innovative planning techniques to coordinate activities without disrupting motorist activities.

Owner Contact: Mr. Rao Igalá; Virginia Department of Transportation, (703) 361-1900

2. *New Construction, Inc; Project Manager*

3. 2000 - 2001

**ATTACHMENT 3.3.1****KEY PERSONNEL RESUME FORM****Brief Resume of Key Personnel anticipated for the Project.**

a. Name & Title:

Achyut (AD) Patel, P.E., Senior Structural Engineer

b. Project Assignment:

Lead Structural Engineer

c. Name of Firm with which you are now associated:

Clark Nexsen, PCd. Years experience: With this Firm 7 Years With Other Firms 20 Years

Please list chronologically (most recent experience first) your employment history, position and general experience or fields of practice for the last fifteen (15) years. (NOTE: If you have less than 15 years of experience, please list all of your experience for those years you have worked.):

Clark Nexsen, Senior Structural Engineer; January 2005 to Present:

Lead Structural Engineer responsible for the design and management of all types of bridge design projects using AASHTO LRFD and ASD design specifications. These included design of straight and curved structures over creeks and rivers, highways and railroads, as well as bridge widenings and rehabilitation of existing structures. Duties included preparation of preliminary and final bridge design and plans, environmental permit sketches, cost estimates, load ratings, daily coordination with design staff, and review of design and CADD work to ensure that the production of contract documents were performed in accordance with VDOT standards and specifications. Responsible for coordination with owner's project managers, other disciplines, subconsultants, and supervision of staff engineers and CADD technicians. Provided mentoring, professional guidance, and problem solving to design staff. Reviewed shop drawings and attended field coordination meetings during construction phase and provided design changes due to unforeseen field conditions.

Reid Structure & Bridge, Inc, Senior Structural Engineer; April 1996 to December 2005:

Since joining Reid Structure & Bridge, Inc in 1995, Mr. Patel was responsible for the planning, design and coordination of simple and continuous, steel and prestressed concrete bridges using AASHTO ASD and LFD design specifications. Managed and designed bridge projects from preliminary to final design submittals and also worked closely with VDOT project managers to fulfill their requirements and complete projects. Supervise and guide staff engineers and CADD technicians in daily design process and production of plans. Duties included design of prestressed and steel superstructures on a straight and curved alignment, integral and MSE wall abutments, multi column and tall hammerhead piers using design software such as SIMON, CONSPAN, DESCUS, STAAD and RC-Pier, preparation of environmental permit sketches, cost estimates and load rating. Also responsible for coordination with other disciplines and subconsultants. Performed inspections for highway bridges, navy dry docks and piers, and prepared rehabilitation reports and repair plans. Reviewed shop drawings and provided design changes during construction due to changed field conditions. Participated and prepared successful proposal schemes during bridge interview process for municipalities and VDOT.

e. Education: Name & Location of Institution(s)/Degree(s)/Year/Specialization:

University of Virginia / M.E. / 1994/ Civil Engineering (Structures)**New Jersey Institute of Technology / B.S. /1990/ Civil Engineering**

f. Active Registration: Year First Registered/ Discipline/VA Registration #:

Professional Engineer / Civil / 1994 / Virginia #025919

g. Document the extent and depth of your experience and qualifications relevant to the Project.

1. *Note your specific responsibilities and authorities for each assignment, not those of the firm.*
2. *Note whether experience is with current firm or with other firm.*
3. *Provide beginning and end dates for each assignment.*

(List at least (3), but no more than (5) relevant projects for which you have performed a similar function.)

Rte. 60 Bridge Replacement over Smith Creek, Clifton Forge, VA (Design Build)

1. **Design Manager** in charge of complete design for the complex replacement of a 164 foot long concrete T-beam bridge that is bordered along each side with historic buildings located less than 1 inch away for the length of the bridge. The new structure is a 3-span (48'-56'-43' with end cantilevers) prestressed concrete box beam superstructure supported on concrete frame bent piers founded on rock. Also coordinated all support services including approach roadway design, transportation maintenance plans, hydraulics and scour analysis, geotechnical analysis, surveying and utility designs. Worked closely with contractor, hydraulic engineer and geotechnical engineer to determine the most cost effective foundation system.
2. *Clark Nexsen; Design Build Project Manager*
3. 2010 – Present

Brambleton Avenue Bridge Widening and Deck Replacement, Norfolk, VA.

1. **Lead Structural Engineer** responsible for design and overall project management for the 13 span, 700 ft. long bridge crossing over waterway. Stage construction was employed to maintain 3 lanes of traffic during construction. Designed prestressed concrete superstructure and substructure widening, and complete deck replacement to accommodate 10-foot path. The new deck slab was detailed as 2- span continuous units to reduce number of deck joints. Prepared cost estimates, environmental permit sketches and coordinated design with other disciplines and utility companies. Provided construction period services to include shop drawings review periodic meetings with contractor and design changes to accommodate unforeseen conditions.
2. *Clark Nexsen; Lead Structural Engineer*
3. 2006 - 2006

Rte. 288 Interchange Bridges, Goochland Co., VA. (PPTA Project)

1. **Lead Structural Engineer** in-charge of preliminary and final plans and cost estimates for 4 new bridges and one widening including curved flyover ramp structure. Coordinated all design requirements with PPTA contractor and roadway consultant. Also provided post design services during construction. Structures included:
Ridgefield Parkway over Route 288: 72 meter long, 2 span continuous steel bridge
Ramp A Flyover Route 288: 101meter long, 2 span continuous curved steel bridge
Route 288 NBL over Tuckahoe Creek: 160 meter long, 7 span continuous prestressed concrete bridge
Route 288 SBL Widening over Tuckahoe Creek: 160 meter long, 7 span continuous prestressed concrete bridge
2. *Reid Structure & Bridge; Lead Structural Engineer*
3. 2001 - 2003

Rte. 604 (Genito Rd.) Widening over Rte. 288, Chesterfield County, VA

1. **Lead Structural Engineer** in-charge of preliminary and final plans for the widening of 2-span (106'-106') grade separation bridge over mainline Route 288. Stage construction was employed to maintain traffic during construction. Structure was widened to accommodate entire 2-lane wide EBL with sidewalk and median. Piers were designed for seismic category B.
2. *Reid Structure & Bridge; Lead Structural Engineer*
3. 2001 - 2003

I-95/Route 627 Interchange Bridges, Stafford County, VA

1. **Lead Structural Engineer** responsible for the preliminary and final design and plans for three grade separation bridges for Stafford County Airport Interchange project. Coordinated all design with roadway and geotechnical consultants. Structures included:
Rte. 627 Bridge Replacement over I-95: 430 ft. long, curved 5 span continuous prestressed concrete bridge
WB Connector CD Lane over I-95: 600 ft. long, 5 span continuous steel plate girder bridge
WB Connector CD Lane over Rte. 1: 282 ft. long, 4 span continuous steel wide flange beam bridge
2. *Reid Structure & Bridge; Lead Structural Engineer*
3. 2001 – 2003

I-581 / Valley View Interchange Phase II

State Project No.: 0581-128-109, P101, RW201, C501, B627 Federal Project No.: NH-581-510351 Contract ID Number: C00016595DB45

ATTACHMENT NO. 3.4.1(a)

LEAD CONTRACTOR – WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

Work by Lead Contractor-three (3) projects which best illustrates current qualifications relevant to this Project.

a. Project Name & Location	b. Narrative describing nature of Firm's Responsibilities	c. Client/Owner/Project Manager who can verify Firm's responsibilities. Include address and current phone number.	d. Contract Completion Date (Original)	e. Contract Completion Date (Actual or Estimated)	f. Estimated Value (in Thousands)			
					Original Contract Value	Final or Estimated Contract Value	Dollar Value of Work for Which Firm Was/Is Responsible	
RICHMOND AIRPORT CONNECTOR RD / VA 895 POCAHONTAS PKWY <i>Richmond/ Henrico County, VA</i>	<i>Please see detailed Narrative below.</i>	Transurban 501 Pocahontas Parkway Route 895 Richmond, VA 23231	Rich Prezioso Phone: 804.822.3460	May 2011	March 2011	\$38,523	\$39,446 The change in contract amount was a result of the scope validation after all engineering investigation was completed.	\$39,446

b. Narrative describing nature of Firm's Responsibilities

PROJECT DELIVERY METHOD: Design-Build: Lead Designer—Dewberry

AWARDS/RECOGNITION:

- Design-Build Institute of America/Mid-Atlantic Region (DBIA-MAR) 2011 Transportation Design-Build Merit Award.
- Worked a total of 152,546 man-hours on this project and achieved our project goal of completing the project with zero lost time incidents.
- American Infrastructure and Dewberry's proposal brought a 10% cost savings to the Owner.

PROJECT DESCRIPTION: Richmond Airport Connector Road (ACR) is a lump sum, design-build project for which the AI Team is fully responsible for design and construction in collaboration with AI-VA's lead design partner, Dewberry. The project consists of approximately 1.6 miles of four-lane roadway that will provide motorists with direct access to the Richmond International Airport from Route 895. Major quantities include: 422,737 CY of import, 16,541 LF of storm drainage, 111,511 SF of MSE walls, 3 new bridges (one crossing over existing 895 toll road), one bridge widening, 4 box culvert extensions, and 133,507 TN of stone base and paving.

This project required considerable advanced planning and design creativity from the AI Team to ensure success. The team realized early in the project that settlement periods would affect the critical path, and adjusted the work schedule accordingly. Extensive planning was required to reduce impact to the environmentally sensitive site and surrounding wetlands. AI worked together with key stakeholders to provide innovative Value-Engineering solutions including the adjustment of a roadway alignment to reduce excavations, altering the storm-water management design for ease of constructability and shortening the length of the bridges to reduce future maintenance costs. The AI Team also used innovative solutions for ground improvements and soils management including lime stabilization and geotextile fabrics. The AI Team is responsible for managing the QA and QC aspects of this project and is familiar with the

QA & QC procedures VDOT requires. *The DBPM for this project was Mr. Nardon who is our DBPM for the I-581/Valley View Interchange Phase II project.*

LESSONS LEARNED / KEYS TO SUCCESS

- **COMMUNICATION** - Open Communication between AI, Dewberry, VDOT and Transurban reduced the need for rework on design changes, and allowed the project team to know understand the Owner's goals before starting the work.
- **PARTNERING** - Formal Partnering between Contractor and Owner which included a set schedule, set project goals, and a dispute resolution process, all facilitated by a third party; FMI proved very successful. *"American Infrastructure... FHWA, and VDOT worked together almost seamlessly to prepare the plans for final construction approval and solve any and all issues that arose during the design of the project. This effort exemplified the goal that VDOT sets for partnering on each of our design-build projects"* Ian Millikan, P.E. – VDOT Design Project Manager; Richmond Airport Connector
- **PREPLANNING** - Early coordination and approvals from third parties such as CSX, Henrico County, Dominion Power, and the Richmond Airport proved extremely valuable in maintaining the schedule.



Construction of 800' long, 35' tall 4-sided MSE wall between two bridges



Aerial View of the entire project



Approach to Richmond Airport

ATTACHMENT NO. 3.4.1(a)

LEAD CONTRACTOR – WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

Work by Lead Contractor—three (3) projects which best illustrates current qualifications relevant to this Project.

a. Project Name & Location	b. Narrative describing nature of Firm's Responsibilities	c. Client/Owner/Project Manager who can verify Firm's responsibilities. Include address and current phone number.	d. Contract Completion Date (Original)	e. Contract Completion Date (Actual or Estimated)	f. Estimated Value (in Thousands)			
					Original Contract Value	Final or Estimated Contract Value	Dollar Value of Work for Which Firm Was/Is Responsible	
S.R. 202 SECTION 403, 404, 405 <i>Montgomery County, PA</i>	<i>Please see detailed Narrative below.</i>	Pennsylvania Department of Transportation 2140 Herr Street Harrisburg, PA 17103	Mark Compton Deputy Secretary of Transportation Phone: 717.787.5628	Dec 2003	Oct 2003	\$181,442	\$211,476	\$211,476

b. Narrative describing nature of Firm's Responsibilities

PROJECT DELIVERY METHOD: Design-Bid-Build - URS was the engineer

ACHIEVEMENTS: *Project Recognition Award: 2003 Project of the Year – Pennsylvania Partnership for Highway Quality; Excellence in Concrete Pavement, Traffic Management – American Concrete Pavement Association; Maintenance and Protection of Traffic – Pennsylvania Partnership for Highway Quality; Structure Award, 150 Ft. or Greater (Norfolk Southern RR Bridge) – Pennsylvania Partnership for Highway Quality; Governor's Letter of Commendation – Pennsylvania Governor Edward G. Rendell; Merit Construction Award of Excellence – Associated Builders and Contractors, Southeast Pennsylvania Chapter; Arthur L. Powell Award of Excellence in Restoration and Property Management – Greater Valley Forge Chamber of Commerce*

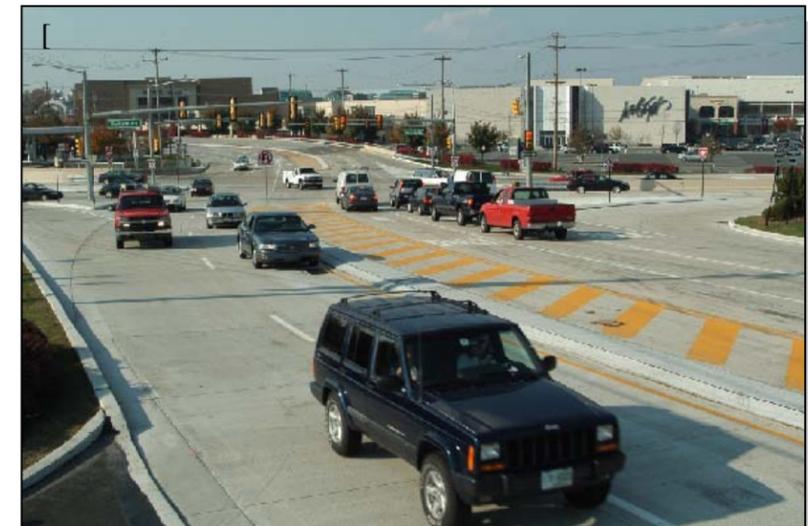
PROJECT DESCRIPTION: American Infrastructure completed this three and one half year, \$211.5 million, and limited access highway reconstruction project ahead of schedule. At that time, it was the largest highway construction project awarded by the Pennsylvania Department of Transportation. This phased project, on SR 202 project involved the reconstructing a complex, multiple interstate, highway interchange and a new, mile-long, multi-lane ramp linking SR 202 with Pennsylvania Turnpike I-76. Construction took place in front of the second largest mall on the East Coast, King of Prussia Mall, in the busy northwestern suburbs of Philadelphia, PA. Construction spanned a Christmas holiday with no disturbance to the Christmas shopping traffic. The overall project consisted of three, major, individual contracts; respectively referenced as Sections 403, 404 and 405. Collectively, the project scope required constructing 19 bridges (including major, complex, curved and continuous structures), 12 retaining walls, 350,000 SY of concrete paving, and 50,000 LF of drainage and storm water piping. Also included was an ITS system of CCTV cameras, incident detectors and electronic message boards accompanied by more than 32 miles of fiber-optic cable within the project limits, and an additional 10 miles of cable beyond these limits. The project took place within an area of roughly one square mile, with construction centered near King of Prussia, PA; an important commercial hub with the busiest intersection in the state. Consequently, AI employed extraordinary measures to safely and efficiently accommodate the traffic volume and maintain positive public relations for the owner. Sub-surface conditions were also challenging. After excavation started, an expanding network of sinkholes was discovered, eventually requiring 1.4 million cu. ft. of pressure-injected grout. AI's project team successfully managed all challenges on this complex project, completing the work ahead of schedule.

LESSONS LEARNED / KEYS TO SUCCESS

- **SUCCESSFULLY WORKING IN A CONGESTED AREA** - The busy nature and restricted footprint of this site required very detailed advance planning and collaboration with stakeholders to succeed. In particular, the intersection of Gulph Road and SR 202, located in front of the popular King of Prussia Mall and considered the busiest intersection in Pennsylvania, needed to be completely demolished and rebuilt. This required an extraordinary level of planning. The AI team successfully completed this work ahead of schedule and earned two milestone bonuses on this section of the contract.
- **INNOVATION** - Innovative approaches can make huge impacts. This is evident in the approach taken with the railway bridge over I-76. AI completely pre-constructed this twin-span rail bridge in an area adjacent to where the bridge would span the interstate. In a five-hour window ending 5:00 a.m. on a Sunday morning, this pre-constructed bridge was "launched" into position over I-76 with minimal, resulting traffic disruption.
- **PLANNING FOR TRAFFIC** – It is important to fully understand the traffic; and design for it, prepare for it and be diligent in managing it. I-76 is a key route for commuters from the northern and western suburbs of Philadelphia. With over 100,000 daily weekday commuters, safe and effective maintenance of traffic was critical to the completion of this section of the project a full month ahead of schedule.

(Left) Aerial view of project

(Right) King of Prussia mall area, post construction.



I-581 / Valley View Interchange Phase II



State Project No.: 0581-128-109, P101, RW201, C501, B627 Federal Project No.: NH-581-510351 Contract ID Number: C00016595DB45

ATTACHMENT NO. 3.4.1(a)

LEAD CONTRACTOR – WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

Work by Lead Contractor—three (3) projects which best illustrates current qualifications relevant to this Project.

a. Project Name & Location	b. Narrative describing nature of Firm's Responsibilities	c. Client/Owner/Project Manager who can verify Firm's responsibilities. Include address and current phone number.	d. Contract Completion Date (Original)	e. Contract Completion Date (Actual or Estimated)	f. Estimated Value (in Thousands)			
					Original Contract Value	Final or Estimated Contract Value	Dollar Value of Work for Which Firm Was/Is Responsible	
MARYLAND ROUTE 43 <i>White Marsh, MD</i>	<i>Please see detailed Narrative below.</i>	Maryland State Highway Administration 8019 Corporate Drive Baltimore, MD 21236	Gradon Tobery Phone: (410) 931-0808	Nov. 2006	Nov. 2006	\$46,700	\$49,356	\$49,356

b. Narrative describing nature of Firm's Responsibilities

PROJECT DELIVERY METHOD: Design-Bid-Build: Lead Designer—KCI

AWARDS/RECOGNITION:

- Associated Builders and Contractors, Inc. (ABC) Excellence in Construction Award in 2006.
- American Infrastructure received recognition from the owner, Maryland State Highway Administration, and ABC for precautions taken in the environmentally sensitive White Marsh Wetlands/Watershed.

PROJECT DESCRIPTION: This three-year project required constructing 3.8 miles of new, four-lane, divided highway through environmentally sensitive wetlands and watershed adjacent to the Chesapeake Bay. Included in this project were the construction of five new bridges (two spanning railways), the rehabilitation of four existing bridges and installation of three, parallel 180' linear feet runs of 84" diameter, reinforced concrete pipe (RCP). This pipe allowed wetlands water to flow naturally from one side of the highway to the other. Site preparation required over 90 acres of clearing and grubbing, 600,000 cubic yards of excavation and 1.2 million cubic yards of on-site borrow. The extensive earthwork required 13 on-site SWM ponds, off-site wetland mitigation and continual E&S management provided by a full-time, dedicated staff. Extensive MOT was required for the bridges spanning MD Route 40. The two spans over active railway lines created schedule constraints, and required coordination with both **Amtrak** and **MARC railways**. The AI team completed this project on time and within budget. AI self-performed 80% of the work on this 327,000 man-hour project.



(Above) Parallel runs of 84" dia. RCP under new roadway allowed flow of wetlands water from one side to other



(Above) Work zones near residential areas and through wetlands required extensive erosion, sediment and storm water management controls.

LESSONS LEARNED /KEYS TO SUCCESS

- **CORPORATE CITIZENSHIP** – This project reinforced known lessons regarding the importance of conducting our work in an environmentally responsible manner, and the considerable levels of planning and preparation required to do this properly. This project required constructing work in environmentally sensitive wetland areas. AI and KCI along with MD SHA needed to work collaboratively and well in advance of the work to ensure designs and ensuing construction did not adversely impact the designated and sensitive wetland areas.
- **RESPOND AND AFFECT CHANGE** – During the construction phase of the project, AI, KCI and MD SHA were requested to re-design the utilities to accommodate future utility expansions. The request was based on expected utility demands from new, large development project(s) planned in the area of the MD Route 43 project. Affecting this change required rapidly facilitating a collaborative planning environment with all of the identified stakeholders including local and state governments, private utility owners, utility operators and the community. Timely, collaborative responses to change ensure goals are met with improved designs and controlled construction costs.



(Above) Completed interchange at MD 43 and MD 40, with 125' dual spans over MD 40 and dual spans over **Amtrak** and **MARC** rail lines.

I-581 / Valley View Interchange Phase II



State Project No: 0581-128-109, P101, RW201, C501, B627 Federal Project No: NH-581-510351 Contract ID Number: C00016595DB45

ATTACHMENT NO. 3.4.1(b) LEAD DESIGNER – WORK HISTORY FORM (LIMIT 1 PAGE PER PROJECT)

Work by Lead Designer—three (3) projects which best illustrates current qualifications relevant to this Project.

a. Project Name & Location	b. Narrative describing nature of Firm's Responsibilities	c. Client/Owner/Project Manager who can verify Firm's responsibilities. Include address and current phone number.	d. Contract Completion Date (Original)	e. Contract Completion Date (Actual or Estimated)	f. Estimated Value (in Thousands)			
					Original Contract Value	Final or Estimated Contract Value	Dollar Value of Work for Which Firm Was/Is Responsible	
Route 36 Improvements Design-Build <i>City of Hopewell & Prince George County, VA</i>	<i>Please see detailed Narrative below.</i>	VDOT 4608 Boydton Plank Road Petersburg, VA 23803	Mr. Michael Saunders, Area Construction Engineer Phone: 804.862.6450	December 2012	December 2012 Est.	\$8,225	\$8,225	\$1,469

b. Narrative describing nature of Firm's Responsibilities

PROJECT DELIVERY METHOD: Design-Build: Lead Contractor—Abernathy Construction Corporation (Abernathy)

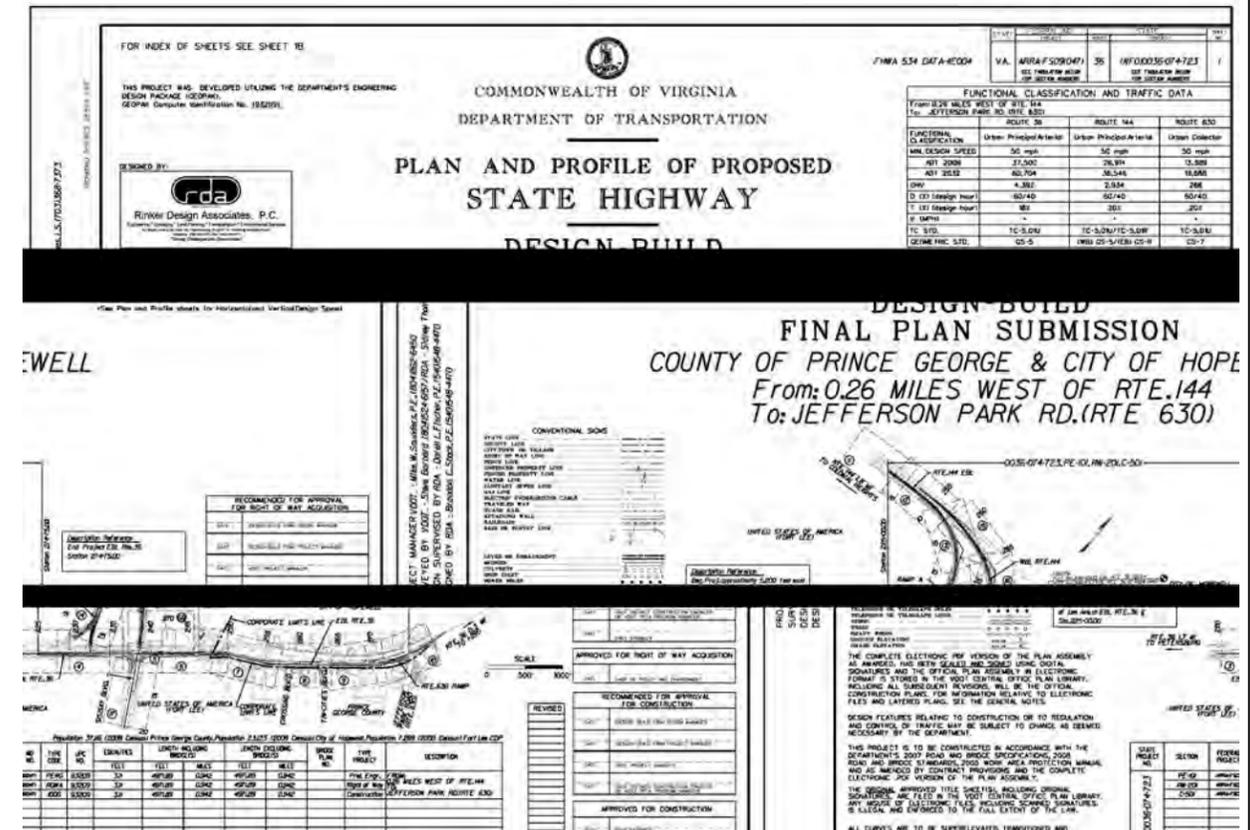
SPECIFICATIONS: Widen Route 36 (provide an additional lane in the eastbound direction), Urban Typical Section with Shoulders; 2,000 LF Urban Principal Arterial
Widen Route 36 (provide an additional lane in the eastbound direction), Urban Typical Section with Curb & Gutter; 2,500 LF Urban Principal Arterial
Widen Route 144 (provide an additional lane in the eastbound direction), Interchange Ramp Typical Section; 2,400 LF Urban Principal Arterial utilizing the Interchange Ramp GS Standard
Design a Spur Ramp from Route 144 eastbound to Route 36 westbound, Interchange Ramp Typical Section; 650 LF Interchange Ramp GS Standard
Realign/Reconstruct Route 144 westbound, Urban Typical Section with Shoulders; 2,200 LF Urban Principal Arterial

PROJECT DESCRIPTION: RDA is the Lead Designer providing engineering design services, right-of-way acquisition services, environmental permitting, and construction engineering/ inspection services for the Route 36 Improvements Design-Build project in Prince George County and the City of Hopewell. The project consists of roadway widening construction for approximately 1 mile of Route 36, 0.5 miles of Route 144, 0.2 miles of new roadway connection from Route 144 to Route 36 and 0.2 miles of widening to Sisisky Boulevard (Fort Lee entrance). Project limits are from 0.25 miles west of Sisisky Boulevard to 0.3 miles west of I-295 along Route 36 and from Route 36 to 0.5 miles west on Route 144. Access onto and off of Route 144 was designed to interchange ramp standards to facilitate higher speed access. The project is being performed as a Design-Build project utilizing ARRA funds.

As the Lead Designer for the Route 36 Improvements, Rinker Design was responsible for the following critical project elements:

- **Coordination**—project coordination with VDOT, City of Hopewell, Prince George County, and US Army / Fort Lee Military Base
- **Roadway Design**—included typical section development, horizontal and vertical geometry, traffic management plans, signage and marking, signalization plans and lighting plans
- **Drainage Design**—included roadway drainage, cross drainage (culvert design), erosion/sediment control plans, and storm water management (quantitative and qualitative)
- **Environmental Permitting**—permit drawings for all wetland (permanent and temporary) impacts and stream impacts
- **Right of Way Acquisition**—responsible for right of way and easement acquisition from 12 affected parcels
- **Utility Relocation Coordination**—responsible for holding UFI meeting, developing easement requirements, evaluating UT-9 forms to determine cost responsibility, reviewing utility plan and estimates, and monitoring the relocation of affected utilities
- **Subconsultant Management**—activities performed by subconsultants reporting to Rinker Design included geotechnical, traffic signal design and underground utility designation and location.

The design of the Route 36 Improvements, awarded in May 2010, is complete and construction is on schedule for completion by the end of 2012.



ATTACHMENT NO. 3.4.1(b)
LEAD DESIGNER – WORK HISTORY FORM
(LIMIT 1 PAGE PER PROJECT)

Work by Lead Designer—three (3) projects which best illustrates current qualifications relevant to this Project.



a. Project Name & Location	b. Narrative describing nature of Firm's Responsibilities	c. Client/Owner/Project Manager who can verify Firm's responsibilities. Include address and current phone number.	d. Contract Completion Date (Original)	e. Contract Completion Date (Actual or Estimated)	f. Estimated Value (in Thousands)			
					Original Contract Value	Final or Estimated Contract Value	Dollar Value of Work for Which Firm Was/Is Responsible	
James Madison Highway (Route 15) PPTA Design-Build <i>Prince William County, VA</i>	<i>Please see detailed Narrative below.</i>	Prince William County Department of Public Works 5 County Complex Court Prince William, VA 22192	Mr. Tom Blaser, Director of Transportation Phone: 703.792.6825	December 2009	December 2009	\$56,430	\$56,430	\$4,119

b. Narrative describing nature of Firm's Responsibilities

PROJECT DELIVERY METHOD: PPTA/Design-Build: Lead Contractor—Branch Highways, Inc.

SPECIFICATIONS: Design and construct Route 15 from an existing two-lane roadway to an urban principal arterial, VDOT Std. GS-5 with 60mph design speed from Dominion Valley Drive to Route 234 (approximately 2.2 miles). Construct Route 15 widening from two to four lanes from I-66 to Utterback Lane (approximately 1.2 miles). Design and construct realigned Waterfall Road from its existing location to the intersection of Route 15/Sudley Road (approximately 0.3 miles). Realigned Waterfall Road is a Major Collector, VDOT Std. GS-7, with 45mph design speed. Design and construct Heathcote Blvd. Extended, from the intersection of Route 15 and Heathcote Boulevard to the intersection of Old Carolina Road and Heathcote Boulevard (approximately 0.3 miles). Heathcote Boulevard is a four-lane divided Urban Minor Arterial, VDOT Std. GS-6, with design speed of 45mph. Design and construct Old Carolina Road from existing two-lane roadway to the ultimate four-lane divided section—Major Collector, VDOT Std. GS-7, with 45mph design speed from the intersection of Old Carolina Road and Heathcote Boulevard to a point approximately 0.7 miles north. (Total Length of Work: 3.5 miles of Design and 4.7 miles of CEI).

PROJECT DESCRIPTION: Rinker Design is the Lead Designer providing engineering design services, right-of-way acquisition services, environmental permitting and construction engineering/inspection services for the Route 15 PPTA project in Haymarket (Prince William County), Virginia. The project consists of complete roadway and bridge construction for 2.2 miles of US Route 15, 0.3 miles of Waterfall Road, 0.7 miles of Old Carolina Road and 0.3 miles of Heathcote Boulevard.

The project also includes construction of an additional 1.2 miles of Route 15 widening design by others. Project limits are from the I-66/Route 15 interchange on the south to the Route 15/Route 234 intersection on the north, including construction of bridge structures over Little Bull Run Creek and Catharpin Creek and a major box culvert at the Tributary to Catharpin Creek. Rinker Design is serving as the Prime Engineering Consultant to Branch Highways, Inc., the Lead Contractor/Project Constructor responsible for development and construction. The project is being performed as a D-B venture under the Virginia PPTA Act of 1995.

Rinker Design's commitment to quality was demonstrated in their willingness to provide innovative solutions throughout the Design Build process. Working closely with VDOT, Prince William County, Branch Highways and other stakeholders, Rinker Design facilitated conflict resolution by providing numerous engineered solutions that were acceptable to all parties involved. These solutions enabled the project to maintain momentum, without compromising VDOT standard and requirements, and at the same time, met the Team's budgetary constraints.



ATTACHMENT NO. 3.4.1(b)
LEAD DESIGNER – WORK HISTORY FORM
(LIMIT 1 PAGE PER PROJECT)

Work by Lead Designer—three (3) projects which best illustrates current qualifications relevant to this Project.



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					Original Contract Value	Final or Estimated Contract Value	Dollar Value of Work for Which Firm Was/Is Responsible	
Sudley Manor Drive PPTA <i>Prince William County, VA</i>	<i>Please see detailed Narrative below.</i>	Prince William County Department of Public Works 5 County Complex Court Prince William, VA 22192	Mr. Tom Blaser, Director of Transportation Phone: 703.792.6825	February 2007	January 2007	\$24,139	\$37,408	\$1,200

b. Narrative describing nature of Firm's Responsibilities

PROJECT DELIVERY METHOD: PPTA/Design-Build: Offeror—CH2M Hill, Inc., Lead Contractor—The Lane Construction Corporation

SPECIFICATIONS: Four-Lane Divided Highway, Urban Typical Section with Curb and Gutter, and Raised Median; 10,000 LF Urban Minor Arterial

PROJECT DESCRIPTION: Sudley Manor Drive was prepared for Prince William County on an accelerated schedule in accordance with the Public-Private Transportation Act of 1995 (PPTA). The project provides a direct connection from Linton Hall Road to the Prince William Parkway and Sudley Road area as called for in the Prince William County Comprehensive Plan. In addition to the 10,000-foot extension of Sudley Manor Drive (a four-lane urban minor arterial designed to accommodate future expansion to six lanes), the project included Linton Hall Road Improvements from Devlin Road to Broad Run. The project required close coordination with the Virginia Department of Transportation to meet the accelerated schedule for plan design, utility relocation, right-of-way acquisition and construction. This project has been constructed and placed under traffic.



The project's typical section consisted of a four-lane roadway built on six-lane right of way with curb and gutter, raised median, sidewalk and a 10-foot wide shared use path to accommodate both pedestrians and bicyclists in the corridor. The design adhered to VDOT standards and policies throughout, incorporating standard pavement, incidentals, drainage and stormwater management design.

The Sudley Manor Drive project provided many challenges for the project team. The accelerated schedule required Rinker Design to assemble construction plans within seven months of project kickoff while incorporating directives from the Contractor, VDOT and Prince William County into the design. Design issues that needed special consideration included: a bridge with MSE walls over a railroad; coordination of the project with several large fuel pipelines, the construction and access requirements of a new firehouse and several site developments; floodplain analysis and environmental considerations related to major stream crossings; and a traffic analysis and signal design. The project team also worked closely with VDOT to ensure a seamless transition between this PPTA project and the adjoining VDOT administered construction project on Linton Hall Road (also successfully designed by Rinker Design). As demonstrated on this project, Rinker Design's ability to deliver high quality design plans on an accelerated schedule, as well as their experience working in partnership with VDOT, localities, contractors and utility companies on D-B projects to ensure that all stakeholders' objectives are adequately addressed will be of particular benefit on the I-581/Valley View Interchange Phase II project.

As part of this contract, Rinker Design also provided survey services including complete boundary and topographic surveys, in addition to plat preparation for more than 50 properties.

