



RFQ Information Meeting

**I-64 Southside Widening & High Rise Bridge
Phase 1 – Widen From 4 Lanes to 6 Lanes**

August 25, 2016

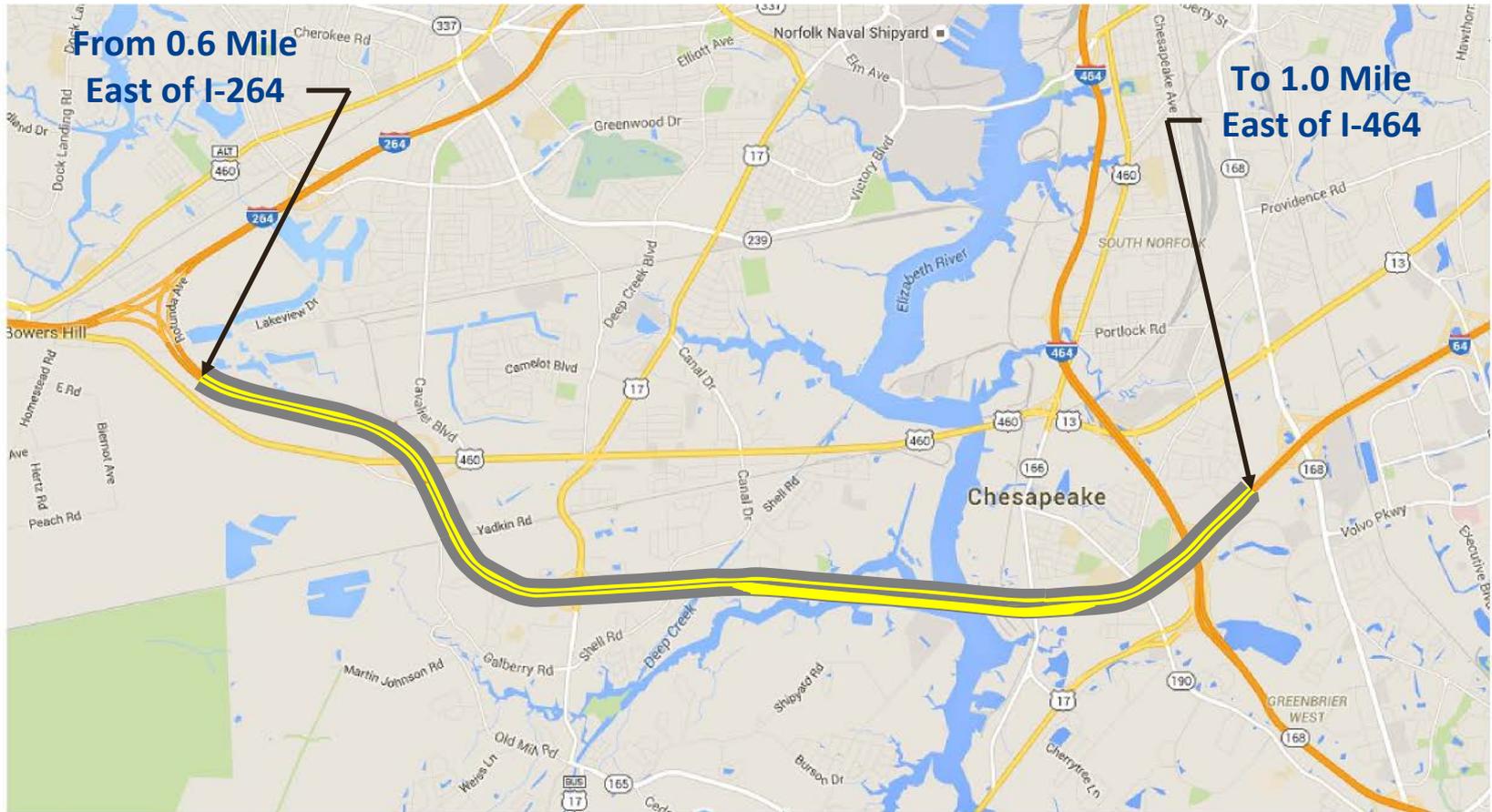
Rick Correa, PE – Project Manager

Jeff Roby, PE – APD Project Manager and POC

Agenda

- **Project Location**
- **Project History**
- **Project Overview**
- **Advanced Coordination by VDOT**
- **RFQ/SOQ Overview**
- **Questions**

Project Location



Total Project Length = 8.75 Miles

Project History

Purpose & Need

- **March 2013:** The Hampton Roads 2034 Long-Range Transportation Plan was amended to include an environmental study for the improvement of the I-64 corridor from I-464 to I-664/264, including the High Rise Bridge.
- The **Purpose** of the environmental study was to develop alternative solutions to address insufficient transportation capacity and correct roadway and bridge deficiencies throughout the corridor.
- The following **Needs** were considered:
 - Improve capacity
 - Enhance safety
 - Improve ability for emergency evacuation route
 - Address High Rise Bridge improvements

Project History

Draft Environmental Document (EA)

- **October 2014:** Draft EA was approved by FHWA.
- **November 2014:** Location Public Hearing was held.
- **March 2015:** CTB approved location of project – Alternative 2.
- The approved **Alternative 2** consists of:
 - Widen I-64 from 4 to 8 lanes
 - Add 2 managed lanes in each direction
 - Replace High Rise Bridge with high-level, fixed-span bridge over Elizabeth River
 - Managed lane option to be determined after more detailed study and analysis



Project History

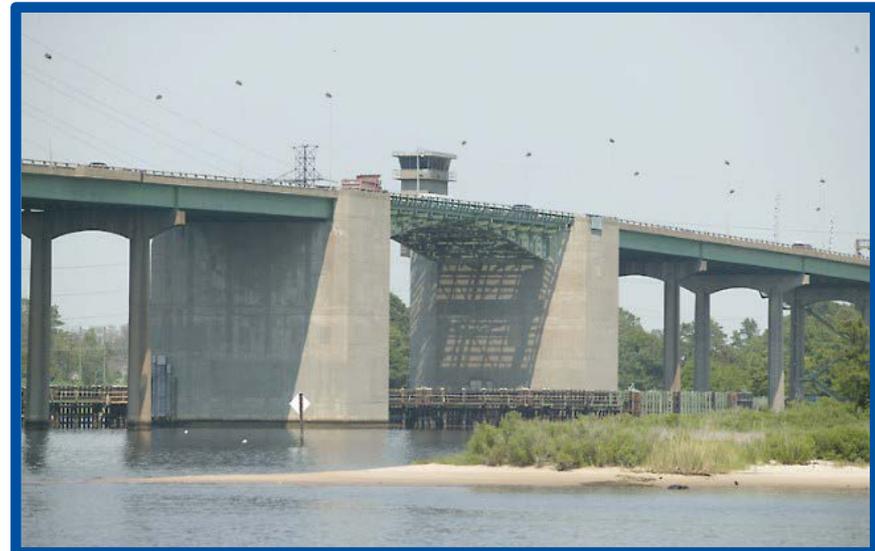
Project Phasing

- **July 2015:** Phasing Study began to determine how best to deliver the project in operationally independent phases.
- **January 2016:** Phasing study was presented to the Hampton Roads TPO; decision was made to **build project in 2 phases.**
- **February 2016:** Started preliminary design and development of the design-build contract for the **Phase 1 project.**
- The approved **I-64 Widening & High Rise Bridge Phase 1** project generally consists of:
 - Widen I-64 from 4 lanes to 6 lanes
 - Add 1 managed lane in each direction in median
 - Asphalt overlay on existing pavements
 - Build new High-Rise Bridge south of existing bridge

Project Overview

I-64 Widening & High Rise Bridge Phase 1

- Phase 1 project is the subject of the RFQ (**the Project**).
- Phase 2 project will be developed and constructed **in the future** and will consist basically of widening I-64 from 6 lanes to 8 lanes including the replacement of the existing bascule bridge.
- The final EA was submitted to FHWA in July 2016. The Finding of No Significant Impact (FONSI) was received on Monday. The EA and the 2040 Long-Range Range Plan support building both Phases 1 and 2 projects.
- Some preliminary design details were developed considering the future Phase 2 project.
- Design and construction cannot preclude constructing Phase 2.

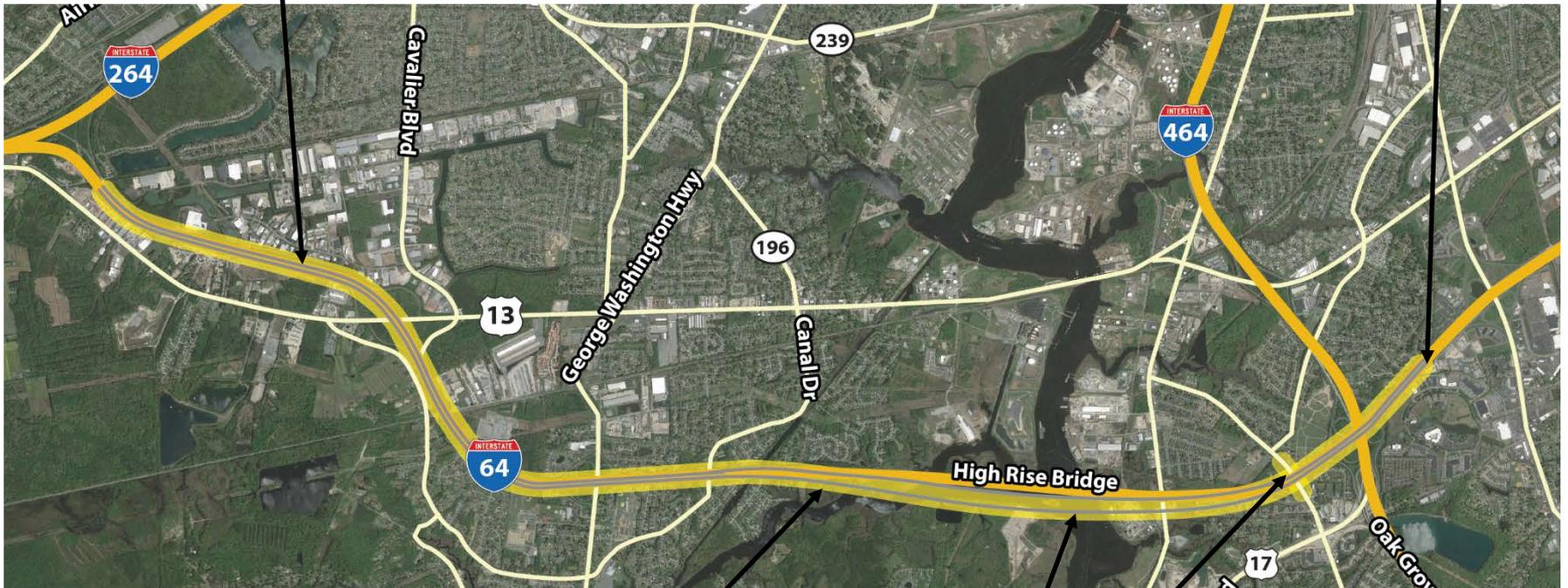


Project Overview

Add Managed Lanes in the median

Provide overlay of existing pavement

Add Managed Lanes aligned with HOV

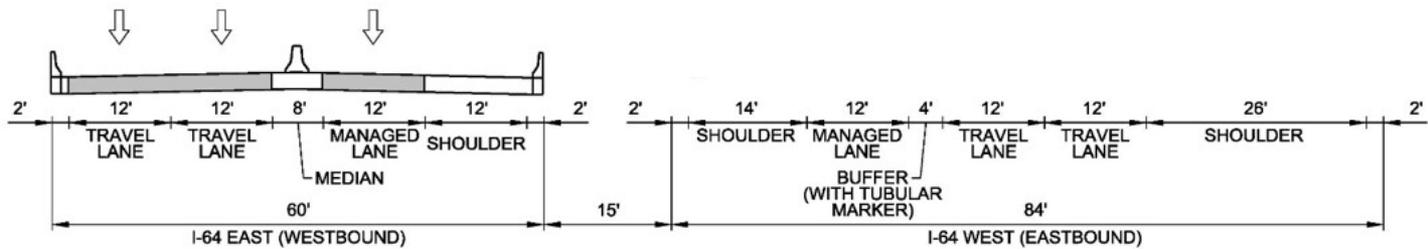
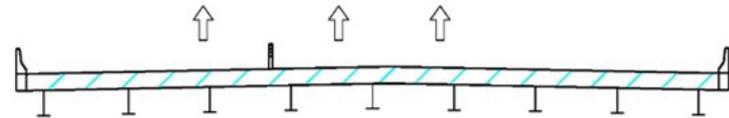
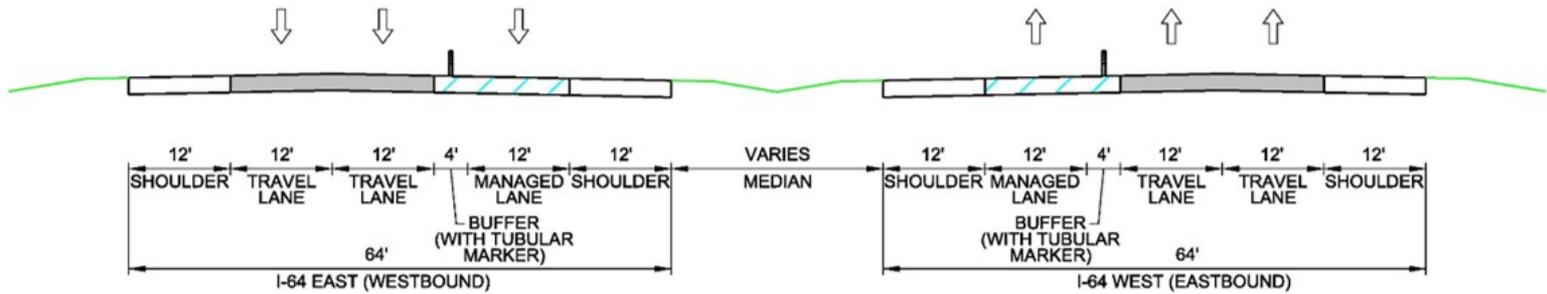


Align I-64 WB with new bridge

Build new bridge south of existing

Replace Great Bridge Boulevard bridge

Project Overview



3 LANES ON EXISTING BRIDGE, 3 ON NEW BRIDGE

LEGEND

- EXISTING LANES
- PROPOSED LANES

Project Overview

Roadway Widening

- I-64 is urban principal arterial (GS-5); Design speed = 70 mph
- Widening to median except at new HRB approaches – avoids impacts to interchanges, limits ROW and environmental impacts
- Add 12ft managed lane, 12ft shoulder, 4ft buffer in each direction
- Overlay of existing pavements, except at overpasses where pavement will be reconstructed
- Widening west of HRB to be asphalt or concrete pavement
- Widening east of HRB to be asphalt pavement
- Extension of 2 box culverts and 14 pipe culverts



Project Overview

Roadway Widening

- Civil infrastructure for managed lanes; no integration system
- Drainage design and stormwater management facilities
- Tide gate at Gilmerton Canal culvert
- MOT – maintain 2 lanes of traffic in each direction at all times
- Environmental permits
- Cannot impact wetland bank property along Libertyville Road
- Reconfigure gates and pavement markings on existing HRB
- Noise analysis; noise barrier design and construction
- Intelligent Transportation System for traffic monitoring
- Right of way acquisition
- Utility relocations
- Public involvement/relations

Project Overview

Roadway Widening

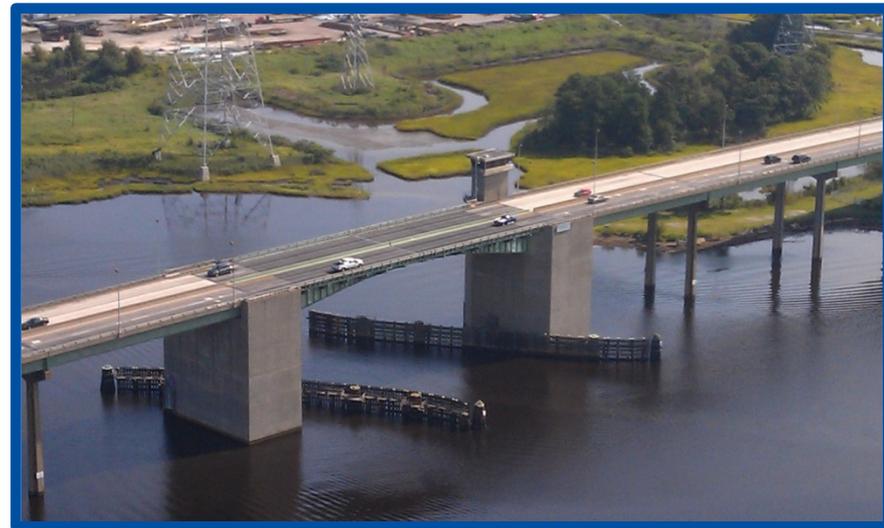
- Example of tide gate (for Gilmerton Canal culvert)



Project Overview

New High Rise Bridge

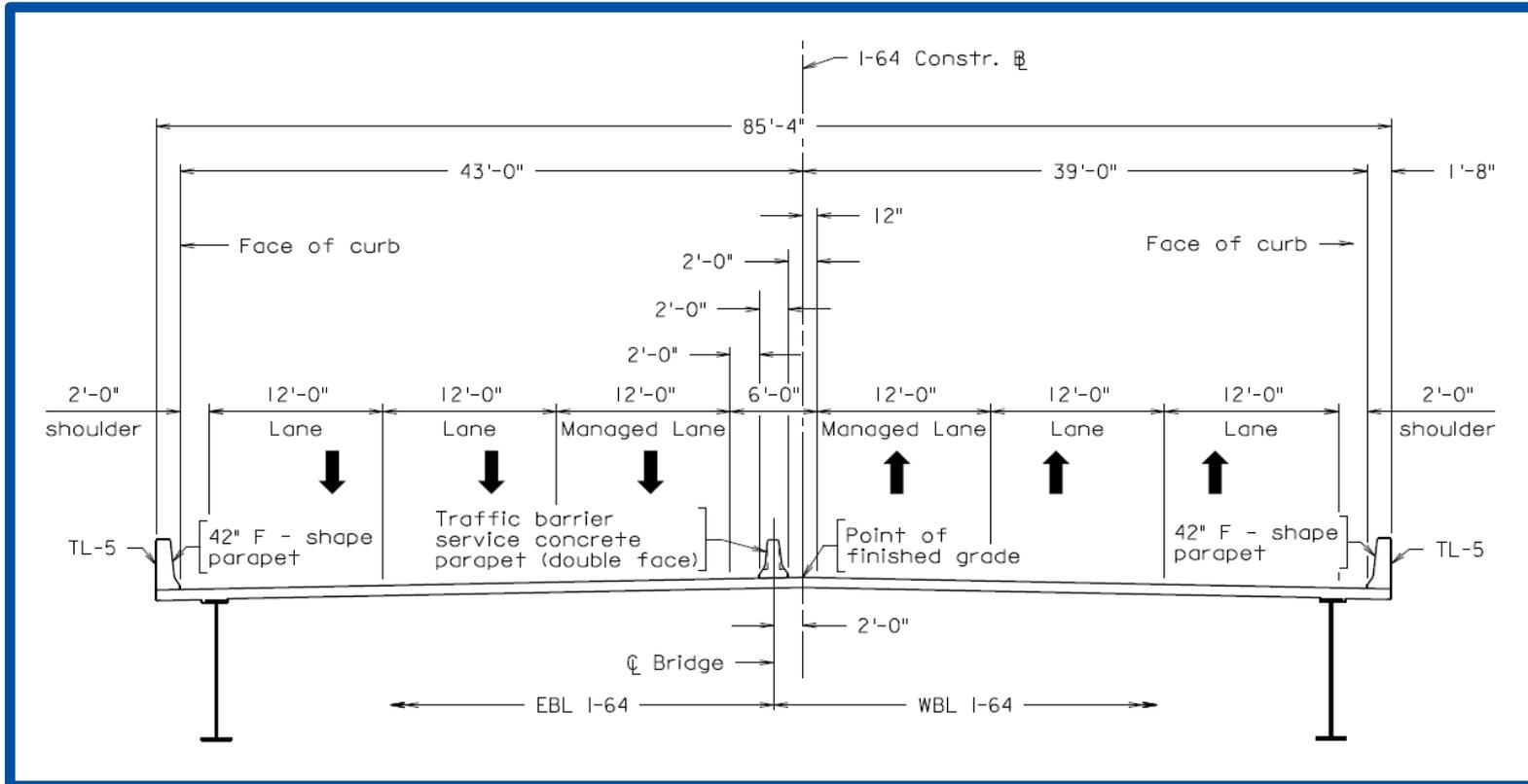
- Build wide enough to accommodate 4th lane in ultimate roadway and 6 lanes during the bridge replacement in Phase 2 project
- Navigation clearance required – 100ft vertical, 125ft horizontal (per USCG Preliminary Navigation Clearance Determination)
- Maintain required Norfolk & Portsmouth Belt Line RR clearances
- Post-tensioned superstructures will not be prohibited
- New fender system (for existing and new bridge)
- Special design required at end of bridge because bridge overlaps Libertyville Road
- USCG coordination and permit
- USACE Section 408 coordination



Project Overview

New High Rise Bridge

- Six-lane configuration during future Phase 2 project construction



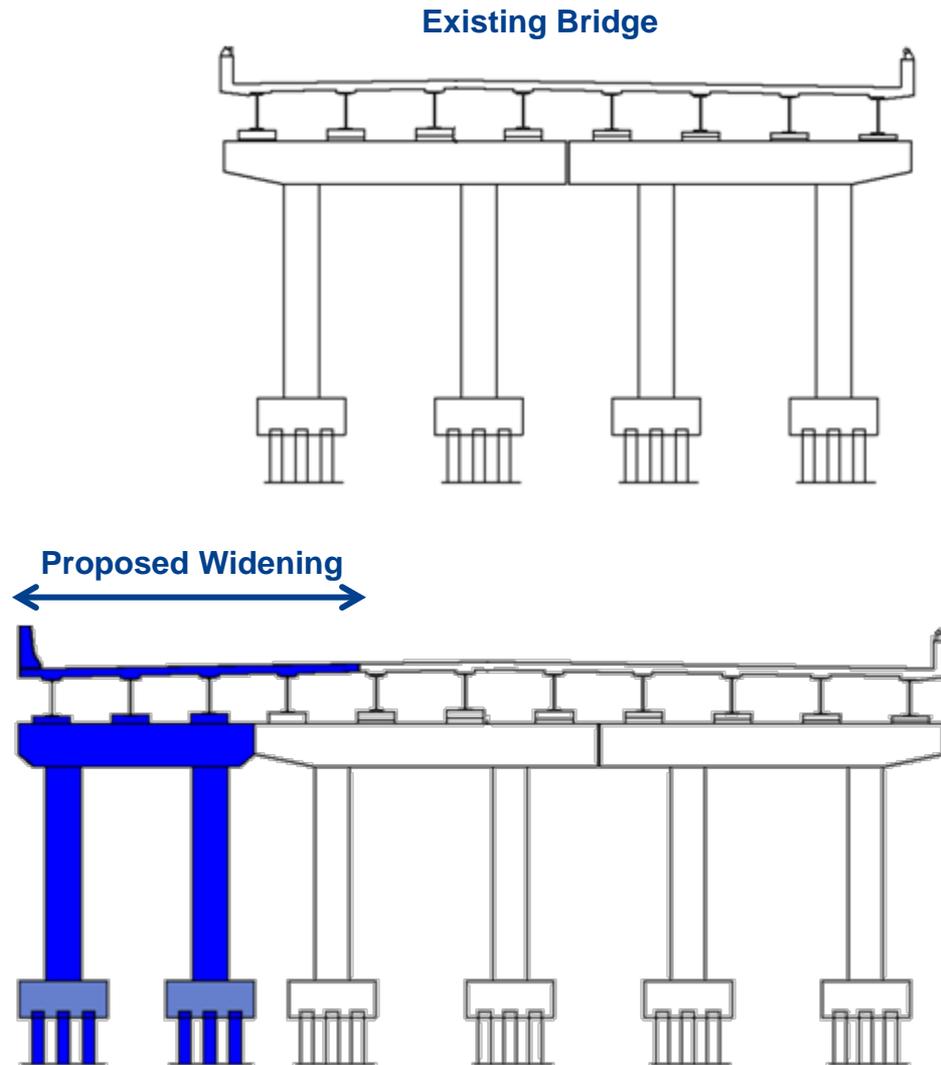
Project Overview

Bridge Widening

- Six bridge widenings:
 - I-64EB & I-64WB over Military Highway
 - I-64EB & I-64WB over Yadkin Road and Norfolk Southern Railroad
 - I-64EB & I-64WB over Shell Road
- Existing structures to remain; some rehabilitation work to be done in advance of design-build contract
- Pier protection system required at all 6 bridges
- Convert piers adjacent to Norfolk Southern Railway to crash wall piers (for railroad protection)
- Existing vertical clearances cannot be reduced

Project Overview

Bridge Widening



Project Overview

Great Bridge Boulevard Bridge Replacement

- Great Bridge Boulevard to be realigned to the west
- Maintain 2 lanes of Great Bridge Blvd traffic through construction
- Bridge layout must not preclude ultimate I-64 lane configuration to be built in the future Phase 2 project
- Bridge typical section has been approved by City of Chesapeake



Project Overview

Overpass Bridges

- Three bridge locations:
 - Route 17 over I-64
 - I-464EB & I-464SB over I-64
- Bridges to remain with no improvements
- Pier protection system required at all 3 bridges

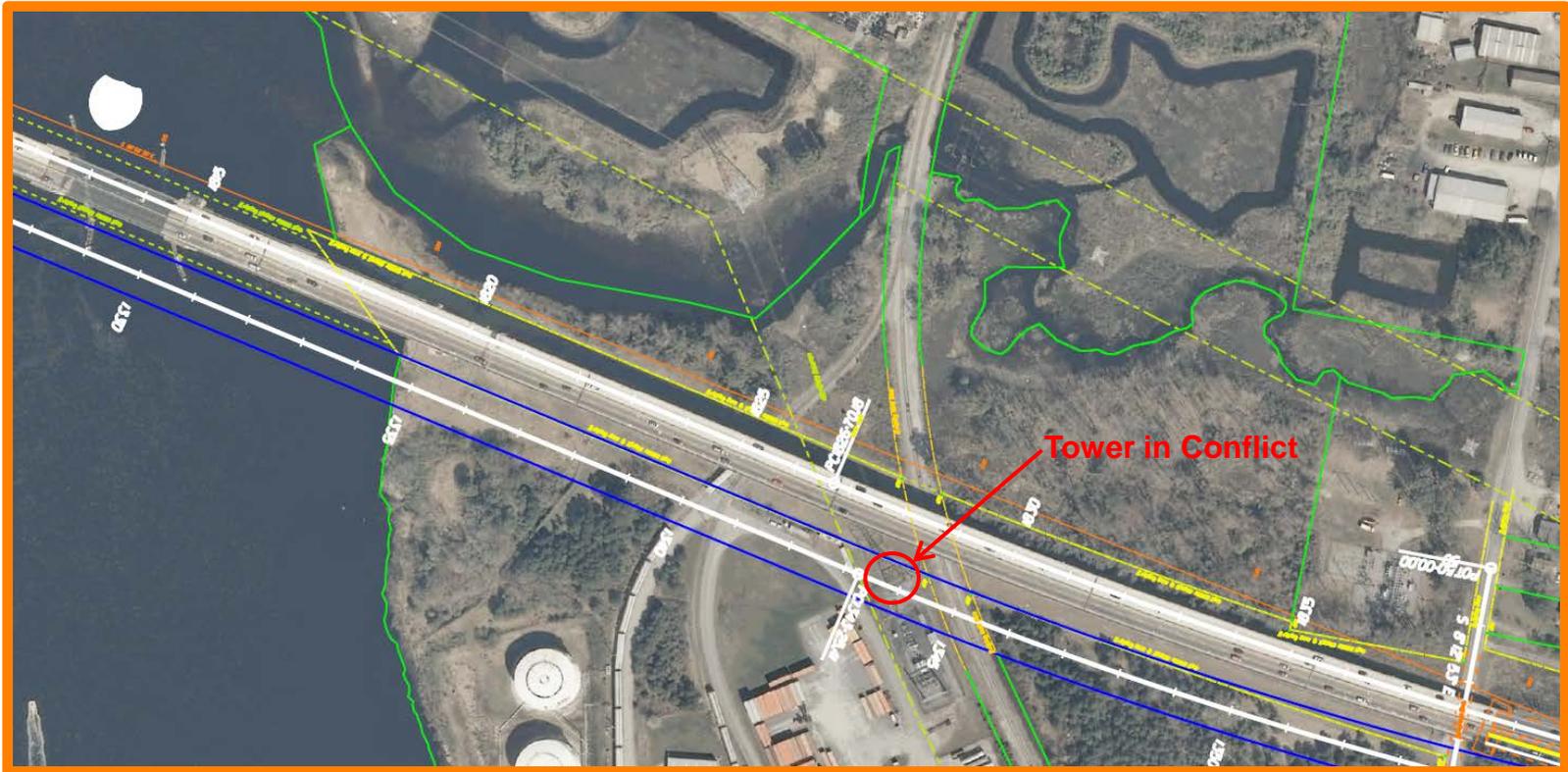


Advanced Coordination by VDOT

- USCG Preliminary Navigation Clearance Determination
- Wetland delineations and Preliminary Jurisdictional Determination
- USACE Section 408 review of preliminary bridge and fender plans
- Geotechnical data report
- Pavement assessment report
- Archeological (cultural resources) survey report
- Corridor study for hazardous materials report
- River sediment analysis report
- Railroad agreements (NSRW and N&PBL)
- Traffic safety assessments
- Design exceptions and design waivers
- **Dominion VA Power transmission tower relocation**

Advanced Coordination by VDOT

- Dominion VA Power transmission tower relocation



RFQ/SOQ Overview

RFQ/SOQ Overview

- **VDOT POC**

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- **Two-Phase Best Value Procurement**
- **Short-List 3 Highest Ranked Teams**
- **Estimated contract value is approximately \$480M**
- **DBE Goal of 8%**

Alternative Technical Concepts (ATC)

- Code of Virginia (§ 33.2-209(B)) has been amended to allow consideration of ATCs
- ATCs will be permitted and considered during the RFP phase of the Project.
- Specific procedures will be included in the RFP
- VDOT's draft ATC Process and Procedures can be found at VDOT's Design-Build website:

<http://www.virginia.org/business/design-build.asp>

RFQ Schedule

(Section 2.6)

RFQ Questions to VDOT	09/07/16
VDOT Response to Questions	09/15/16
Statement of Qualifications Due Date	10/06/16
Notification of Shortlist	11/30/16
RFP Release Date	12/14/16
Technical Proposals Due	06/14/17
Price Proposals Due	07/20/17
Anticipated Award Date	09/19/17
NTP Date	10/19/17

Final Completion February 2021

VDOT is anticipating providing early completion incentives for interim project milestones as well as the final completion of the Project.

FORM C-78 (ATTACHMENT 2.11)

Form C-78-RFQ

ATTACHMENT 2.11

COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION

PROJECT: I-64 SOUTHSIDE WIDENING AND HIGH RISE BRIDGE, PHASE 1

State Project No. 0064-131-811, P101, R201, C501, B662-B669, D637, D638

Federal Project No. NHPP-064-3(488)

Contract ID Number C00106692DB93

ACKNOWLEDGEMENT OF RFQ, REVISION AND/OR ADDENDA

Acknowledgement shall be made of receipt of the Request for Qualifications (RFQ) and/or any and all revisions and/or addenda pertaining to the above designated project which are issued by the Department prior to the Statement of Qualifications (SOQ) submission date shown herein. Failure to include this acknowledgement in the SOQ may result in the rejection of your SOQ.

By signing this Attachment 2.11, the Offeror acknowledges receipt of the RFQ and/or following revisions and/or addenda to the RFQ for the above designated project which were issued under cover letter(s) of the date(s) shown hereon:

1. Cover letter of RFQ – August 16, 2016
(Date)
2. Cover letter of _____
(Date)
3. Cover letter of _____
(Date)

_____ SIGNATURE	_____ DATE
_____ PRINTED NAME	_____ TITLE

- Watch VDOT's Design-Build RFQ website for responses to RFQ questions and Addendums
<http://www.virginiadot.org/business/request-for-qualifications.asp>

- If VDOT issues an addendum, a revised C-78 form will be included with the addendum

- Sign, date and include the C-78 with the SOQ



Contents of Statements of Qualifications (SOQ)

- **Letter of Submittal** (Section 3.2)
 - Offeror's POC, Offeror's Principal Officer, Corp. Structure, Lead Contractor and Lead Designer, Affiliate and/or Subsidiary Companies, Debarment, Prequalification, Surety, SCC, DPOR
 - Joint Ventures
 - 8% DBE Goal
- **Offeror's Team Structure** (Section 3.3)
 - Key Personnel
 - Org Chart and Narrative
- **Experience of Offeror's Team** (Section 3.4)
 - Lead Contractor Work History Form
 - Subcontractor Work History Form, if applicable
 - Lead Designer Work History Form
 - Subconsultant Work History Form, if applicable

Contents of Statements of Qualifications (SOQ)

- **Safety** (Section 3.5)
 - **Lead Contractor(s) Safety Record**
- **Project Risks** (Section 3.5)
 - **Identify and discuss three (3) unique risks the Offeror considers most relevant and critical to the success of this Project.**

Letter of Submittal

Joint Ventures

- **If the Offeror is a limited liability company, partnership or joint venture, its members/partners shall only be contractors, and shall not include any design firms.**
- **In order to pre-qualify as a Joint Venture a completed “Joint Venture Bidding Agreement” must be submitted to and approved by VDOT prior to Statement of Qualifications (SOQ) submittal due date.**
- **VDOT’s Construction Division does not provide prequalification certificates for Joint Ventures however, a prequalification number will be issued for the Joint Venture.**

Letter of Submittal

Joint Ventures (cont.)

- **Joint Venture is not required to register with the Virginia State Corporation Commission (SCC). However, each individual member of the Joint Venture must be registered with the SCC.**
- **Requirement of pre-qualification, Joint Venture entities need to be properly established with a federal tax ID number.**
- **A single 100% performance bond and a single 100% payment bond shall be provided regardless of any co-surety relationship. The Surety letter for a Joint Venture Offeror shall be in the name of the Joint Venture entity.**
- **Specific guidance relative to the prequalification process can be found at:**
<http://www.virginiadot.org/business/const/prequal.asp>.

Offeror's Team Structure

Key Personnel (7)

- **Design-Build Project Manager (DBPM)**
 - Responsible for the overall Project design, construction quality management and contract administration for the Project
 - If the Offeror is a joint venture then only one individual from the members of the joint venture shall be designated as the DBPM
- **Responsible Charge Engineer (RCE)**
 - Accepts full professional responsibility for engineering decisions relating to the design and the construction of the final work product
 - The RCE shall report directly to the DBPM with lines of communication to all other Key Personnel
 - The DBPM or DM may perform the role of the RCE

Offeror's Team Structure

Key Personnel (cont.)

- **Quality Assurance Manager (QAM)**
 - Responsible for the QA inspection and testing of all materials used and work performed on the Project
 - no contractual relationship with QC firm and no involvement in construction
 - A minimum of two lead QA inspectors (one roadway and one bridge)
 - QAM and QA inspectors required to be on the Project site full time during the duration of construction operations
- **Design Manager (DM)**
 - Responsible for coordinating the design of the Project and overseeing the QA/QC program for all design disciplines

Offeror's Team Structure

Key Personnel (cont.)

- **Construction Manager (CM)**
 - Responsible for managing the construction process, to include all Quality Control (QC) activities
 - Required to be on the Project site full time for the duration of construction operations
- **Lead Structural Engineer**
 - Supervise the design and construction of a major bridge structure over an active navigable channel
 - Demonstrated expertise in the type and size of the major bridge structure that will be proposed by the Offeror.

Offeror's Team Structure

Key Personnel (cont.)

- **Incident Management Coordinator (IMC)– Pass/Fail**
 - **Responsible for responding to all incidents within the Project limits**
 - **Key point of contact for issues arising relative to incident management**
 - **Required to be on site full time for the duration of construction operations.**

Offeror's Team Structure

Key Personnel (cont.)

- **Resumes**

- Resumes for individuals who are not identified as Key Personnel should not be included in the SOQ
- Note your role, responsibility, and specific job duties for each project, not those of the firm
- On-call contracts with multiple task orders (on multiple projects) may not be listed as a single project
- For the QAM and CM (on site full-time) provide a current list of assignments, role, and the anticipated duration of each assignment
- Provide references for each of the projects noted on the resume on the Key Personnel References form (references must be current)
- The format and appearance, including existing text, of the Key Personnel Resume and Reference should not be modified

Offeror's Team Structure

Key Personnel (cont.)

- **Requirement to Keep Team Intact (Section 11.0)**
 - The team proposed by Offeror shall remain on the Offeror's team for the duration of the procurement and, if awarded the contract, the duration of the design-build contract.
 - During the procurement, any proposed change of Key Personnel must be submitted in writing to VDOT's POC, who, in his/her sole discretion, will determine whether to authorize a change. (After submittal of the SOQ)
 - Unauthorized changes of Key Personnel by the Successful Offeror (after award) may result in the imposition of a monetary penalty in an amount to be stipulated in the RFP.

Offeror's Team Structure

Organizational Chart

- For design and construction
- Two lead QA Inspectors reporting directly to QAM
- Clear separation and independence between QC and QA to include QA and QC inspection and field/laboratory testing
- QA and QC inspectors shall be fully certified for each work element of the Project

Narrative

- Describe functional relationship and communications among participants for design and construction
- The number of construction subcontractors listed as part of the Offeror's team for the purposes of the SOQ shall be limited to no more than five (5).

Experience of Offeror's Team

Demonstration of Previous Experience

- Use of innovative design solutions and construction techniques
- Previous design-build experience
- Limiting impacts to the traveling public including commitments to effective strategies to minimize congestion and ensure safety during construction
- Previous success in the construction of major bridge structure over an active navigable channel.
- Meeting or exceeding required Disadvantage Business Enterprise Program commitments

Experience of Offeror's Team

Lead Contractor

- Total of five (5) relevant projects. *For multiple phase projects, only single phase of construction (or single contract) will be considered as a Project.*
- Three (3) projects shall be related to roadways and two (2) to bridges
- May include Subcontractor work history in which case at least three (3) of the projects shall be by the Lead Contractor and no more than two (2) may be from a subcontractor
- If work performed by affiliated or subsidiary company of the Lead Contractor need to explain justification for utilizing to satisfy project experience
- If joint venture at least one work history form shall be provided for each member of the joint venture
- The format and appearance, including existing text, should not be modified

Experience of Offerors Team

Lead Designer

- **Total of three (3) relevant projects. *For multiple phase projects, only single phase of construction (or single contract) will be considered as a Project. If additional phases are shown under the same Work History Form, only the first phase (or contract) listed will be evaluated. This also applies for lead contractor.***
- **Two (2) projects shall be related to roadways and one (1) to bridge**
- **May include Subconsultant work history in which case at least two (2) of the projects shall be by the Lead Designer and no more than one may be from a subconsultant**
- **The format and appearance, including existing text, should not be modified**

Safety

The Lead Contractor(s) shall provide information concerning their safety record on Safety Qualifications Form (Attachment 3.5)

- **The most recent three years of Workers' Compensation Experience Modification Ratio (EMR) and/or Experience Modification Factor (EMF)**
- **The total recordable injuries and illnesses incidence rate for the past three (3) years. Offeror's Team Structure**
- **The days away from work injury incidence rate for the past three (3) years**
- **The completed Occupational Safety and Health Administration (OSHA) Form 300A, Summary of Work-Related Injuries and Illnesses, for the past three (3) years.**

Project Risk

Identify and discuss three (3) unique risks the Offeror considers most relevant and critical to the success of this Project.

- **Why each risk is critical and if it applies to a particular element of the project or project as a whole.**
- **Indicate the impacts of the risks**
- **Mitigation strategies to address risk**
- **Role, if any, VDOT or other agencies may have to address these risk**
- **Limit discussion to the unique risk item identified only (example - *don't include five sub items under "Geotechnical Risk"*)**

Evaluation Criteria

(Section 4)

3.3 Offeror's Key Personnel	25%
3.4 Experience of Offeror's Team	30%
3.5 Safety	15%
3.6 Project Risks	<u>30%</u>
Total	100%

- **Scoring Range Application**
 - **Consensus Scoring**
 - **Numerical score based on a 1-10 scale**
 - **Evaluation Criteria included for each submittal requirement**
- **Design-Build Evaluation Guidelines, revised May 2014 (On VDOT'S DB Website)**

Submittal Requirements

(Section 5)

- **Deadline for submitting a Statement of Qualifications is:**
October 6, 2016 at 4:00PM
- **Review Section 5 of the RFQ carefully for all formatting requirements**
- **1 original paper version with original signatures**
- **1 CD-ROM with entire SOQ**
- **10 abbreviated paper copies**
- **Tabs will count against the page limit if project specific information is included on them**
- **Animated videos/ motion pictures are prohibited**
- **No more than 20 pages**
 - **VDOT will discard all pages in excess of the 20 page limit.**

Questions?

