

I-495 NORTHERN SECTION SHOULDER USE FAIRFAX COUNTY, VA

A Design-Build Project

From: South of Old Dominion Overpass **To:** George Washington Memorial Parkway



March 14, 2014

State Project No.: (FO) 0495-029-123, P101, C501

Federal Project No.: STP-495-5(094)

Contract ID No.: C00105130DB72

ATTACHMENT 4.0.1.1

Braddock Road (Route 620) and Pleasant Valley Road (Route 609)

Intersection Improvement in Fairfax County, Virginia

LETTER OF SUBMITTAL CHECKLIST AND CONTENTS

Offerors shall furnish a copy of this Letter of Submittal Checklist, with the page references added, with the Letter of Submittal.

Letter of Submittal Component	Form (if any)	RFP Part 1 Cross Reference	Page Reference
Letter of Submittal Checklist and Contents	Attachment 4.0.1.1	Section 4.0.1.1	NA
Acknowledgement of RFP, Revisions, and/or Addenda	Attachment 3.4 (Form C-78-RFP)	Sections 3.4; 4.0.1.1	NA
Letter of Submittal	NA	Sections 4.1	1 of 2
Letter of Submittal on Offeror's letterhead	NA	Section 4.1.1	1 of 2
Offeror's full legal name and address	NA	Section 4.1.1	1 of 2
Authorized representative's original signature	NA	Section 4.1.1	1 of 2
Declaration of intent	NA	Section 4.1.2	1 of 2
120 day declaration	NA	Section 4.1.3	1 of 2
Point of Contact information	NA	Section 4.1.4	2 of 2
Principal Officer information	NA	Section 4.1.5	2 of 2
Offeror's Corporate Structure	NA	Section 4.1.6	2 of 2
Full Legal Name of Lead Contractor and Lead Designer	NA	Section 4.1.7	2 of 2
Offeror's VDOT prequalification information	NA	Section 4.1.8	2 of 2
DBE statement confirming Offeror is committed to achieving the required DBE goal	NA	Section 4.1.9	2 of 2
Final Completion Date	NA	Section 4.1.10	2 of 2

ATTACHMENT 4.0.1.1

Braddock Road (Route 620) and Pleasant Valley Road (Route 609)

Intersection Improvement in Fairfax County, Virginia

LETTER OF SUBMITTAL CHECKLIST AND CONTENTS

Letter of Submittal Component	Form (if any)	RFP Part 1 Cross Reference	Page Reference
Attachments to the Letter of Submittal	NA	Section 4.2	NA
Affiliated and/ or Subsidiary Companies	Attachment 4.2.1	Section 4.2.1	Tab 4.2.1
Certification Regarding Debarment Forms	Attachment 4.2.2(a) Attachment 4.2.2(b)	Section 4.2.2	Tab 4.2.2
Offeror's VDOT prequalification information	NA	Section 4.2.3	Tab 4.2.3
Evidence of obtaining bonding	NA	Section 4.2.4	Tab 4.2.4
Full size copies of DPOR licenses and SCC registrations	NA	Section 4.2.5	Tab 4.2.5
SCC registration information - businesses	Attachment 4.2.5	Section 4.2.5.1	Tab 4.2.5
DPOR registration information - businesses	Attachment 4.2.5	Section 4.2.5.2	Tab 4.2.5
Lead Contractor Work History Form	Attachment 4.2.6(a)	Section 4.2.6	Tab 4.2.6
Lead Designer Work History Form	Attachment 4.2.6(b)	Section 4.2.6	Tab 4.2.6
Conceptual Roadway Plans	NA	Section 4.2.7	Tab 4.2.7
Written Statement Fully Compliant with RFP	NA	Section 4.2.8	Tab 4.2.8

ATTACHMENT 3.4**COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION**

RFP NO. C00105130DB72
 PROJECT NO.: (FO) 0495-029-123, P101, C501

ACKNOWLEDGEMENT OF RFP, REVISION AND/OR ADDENDA

Acknowledgement shall be made of receipt of the Request for Proposals (RFP) and/or any and all revisions and/or addenda pertaining to the above designated project which are issued by the Department prior to the Letter of Submittal submission date shown herein. Failure to include this acknowledgement in the Letter of Submittal may result in the rejection of your proposal.

By signing this Attachment 3.4, the Offeror acknowledges receipt of the RFP and/or following revisions and/or addenda to the RFP for the above designated project which were issued under cover letter(s) of the date(s) shown hereon:

1. Cover letter of February 6, 2014- RFP
(Date)
2. Cover letter of March 6, 2014-Addendum #1
(Date)
3. Cover letter of February 27, 2014-Q&A
March 11, 2014-Q&A
(Date)


SIGNATURE

March 13, 2014
DATE

FRANCIS O. DAY CO., INC.

CONTRACTOR

850 East Gude Drive, Suite A, Rockville, Maryland 20850

Phone 301-652-2400 Fax 301-424-3697

March 14, 2014

Mr. Bryan W. Stevenson, P.E.
Alternate Project Delivery Office
Virginia Department of Transportation
1401 East Broad Street
Richmond, VA 23219

RE: **Letter of Submittal (4.1) and Attachments (4.2)**
I-495 Northern Shoulder Use
Fairfax County, Virginia / A Design-Build Project
Project No. (FO) 0495-029-123, P101, C501 / Contract ID #C00105130DB72

Dear Mr. Stevenson:

We are pleased to submit for your consideration one (1) original of our Letter of Submittal and Attachments to the Letter of Submittal, which bears original signatures, and one (1) CD-ROM containing the entire Letter of Submittal and Attachments in a single cohesive Adobe PDF file for this Design-Build (D-B) project in Fairfax County.

In preparing this proposal, Francis O. Day Co., Inc. (F.O. Day) has carefully reviewed the Request for Proposal dated 2/6/2014; attended the Pre-Bid Conference on 2/14/2014; reviewed the RFP Q&A's dated 2/27/2014 and 3/11/2014; Addendum No. 1 dated 3/06/2014; and visited the project site.

F.O. Day will be the Lead Contractor and is the Offeror who has the overall authority on this project. F.O. Day is family-owned for over 60 years and has developed into an award-winning construction company (Site Development – Heavy/Highway – Asphalt) by building close relationships with our extended family of valued clients, trusted employees, and dedicated subcontractors and **suppliers. F.O. Day has selected Johnson, Mirmiran & Thompson, Inc. (JMT) as our lead** designer to provide all engineering services for this project. JMT is a multi-disciplined architectural/ engineering employee owned company that offers a full array of consulting and technology services for infrastructure projects throughout the United States. Both F.O. Day and JMT have been involved with innovative project delivery methods including design-build for more than 15+ years.

In accordance with Section 4.1 of the Instructions for Offerors we are submitting the following Letter of Submittal requirements for your review:

- 4.1.1 Authorized Representative Original Signature** - This submittal is signed in ink by an authorized representative of Francis O. Day Company, Inc.
- 4.1.2 Declare of Intent** - It is Francis O. Day Company, Inc.'s intent to enter into a contract with VDOT for the project in accordance with the terms of the RFP.
- 4.1.3 120 Day Declaration** - Pursuant to Part 1, Section 8.2, Francis O. Day Company, Inc.'s offer will remain in full force for one hundred twenty (120) days after the date the proposal is submitted to VDOT ("Letter of Submittal & Price Proposal Due Date").

- 4.1.4 Point of Contact Information** - The point of contact and authorized representative for the F.O. Day D-B Team for all concerns related to this submittal is Mr. Robert T. Brooks, Vice President. Mr. Brooks contact information is as follows:
- F.O. Day 850 East Gude Drive, Suite A Rockville, MD 20850
T 301-652-2400 F 301-424-3697 E Robertb@foday.com
- 4.1.5 Principal Officer Information** - The Principal Officer for this contract is Mr. Francis O. Day, IV, Vice President. Mr. Day's contact information is as follows:
- F.O. Day 850 East Gude Drive, Suite A Rockville, MD 20850
T 301-652-2400 F 301-424-3697 E Mday@foday.com
- 4.1.6 Offeror's Corporate Structure** – F.O. Day is a corporation and will undertake full financial responsibility for the project and will have joint and several liability for the performance of the work required for the project. As noted in the enclosed Surety Commitment letter in Section 4.2.4 of the Attachments, F.O. Day has more than sufficient bonding capacity. There are no liability limitations.
- 4.1.7 Full Legal Name of Lead Contractor and Lead Designer** - Francis O. Day Company, Inc. is the Offeror and Lead Contractor responsible for contract execution and construction and serves as the legal entity who will execute the Contract with VDOT. Johnson, Mirmiran & Thompson, Inc. (JMT) is F.O. Day's Lead Designer and will be responsible for the overall design of the project.
- 4.1.8 Offeror's VDOT Prequalification Information** - F.O. Day's prequalification status (No. D499) is Active and in good standing to bid on this Project as outlined in VDOT's Rules Governing Prequalification Privileges. Evidence of F.O. Day's prequalification with VDOT and good standing with the Commonwealth's State Corporation Commission is included in Section 4.1.8 of the Attachments.
- 4.1.9 DBE Statement (11% Commitment)** – The F.O. Day/JMT Team is committed to achieving the eleven percent (11%) DBE participation goal during the design and construction of the I-495 Northern Section Shoulder Use project. Additionally, our Team's formal subcontracting program will ensure maximum use of SWaM firms as required by the Governor's Executive Order No. 33.
- 4.1.10 Interim Milestone and Final Completion Dates** - The F.O. Day/JMT Team will complete all work necessary to meet the Interim Milestone Date by December 31, 2014 and complete all work necessary to meet the Final Completion Date by March 31, 2015 in accordance with the dates set forth in Part 1, Section 2.4.1 of the RFP.

We forward to the opportunity to perform the engineering and construction services for this contract. Should you questions, please do not hesitate to contact us.

Yours Truly,

FRANCIS O. DAY COMPANY, INC.



Francis O. Day, IV
Vice President



Attachment 4.2.1

Affiliated and/Subsidiary Companies



Attachment 4.2.2

Certification Regarding Debarment Forms

ATTACHMENT 4.2.2(b)
CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS

Project No.: (FO) 0495-029-123, P101, C501

- 1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.
- 2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.



February 24, 2014

Vice President

Signature

Date

Title

Johnson, Mirmiran & Thompson, Inc.

Name of Firm

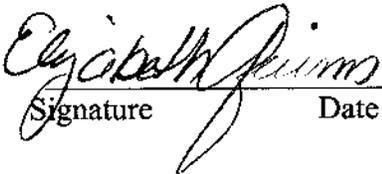
ATTACHMENT 4.2.2(b)
CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS

Project No.: (FO) 0495-029-123, P101, C501

- 1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

- 2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

	March 12, 2014	Vice President
Signature	Date	Title

Quinn Consulting Services, Inc.

Name of Firm

ATTACHMENT 4.2.2(b)
CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS

Project No.: (FO) 0495-029-123, P101, C501

- 1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

- 2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.



February 27, 2014

Vice President

Signature

Date

Title

DMY Engineering Consultants Inc.

Name of Firm

ATTACHMENT 4.2.2(b)
CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS

Project No.: (FO) 0495-029-123, P101, C501

- 1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.
- 2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.


Signature _____ Date February 28, 2014 Title President

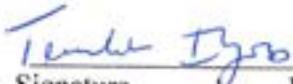
EEE Consulting, Inc.
Name of Firm _____

ATTACHMENT 4.2.2(b)
CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS

Project No.: (FO) 0495-029-123, P101, C501

- 1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.
- 2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

	March 4, 2014	President
Signature	Date	Title

Athavale, Lystad & Associates, Inc.
Name of Firm



Attachment 4.2.3

Offeror's VDOT Prequalification Information



=====
D1026
DAY AND SONS, INC.
PREQ. EXP : 04/30/2014

--PREQ ADDRESS ----- WORK CLASSES (LISTED BUT NOT LIMITED TO)
517 PUMPHREY LANE 024 - JACKING AND BORING OF PIPE
GLEN BURNIE, MD 21061-0000 045 - UNDERGROUND UTILITIES
PHONE : 410-762-5582 101 - EXCAVATING
FAX : 410-762-5584

BUSINESS CONTACT: DAY, KEVIN LAWRENCE
EMAIL: DAYANDSONSINC@AOL.COM

-----DBE INFORMATION-----

DBE TYPE : DBE
DBE CONTACT: N/A

=====
D499
FRANCIS O. DAY CO., INC.
PREQ. EXP : 04/30/2014

--PREQ ADDRESS ----- WORK CLASSES (LISTED BUT NOT LIMITED TO)
850 EAST GUDE DRIVE, STE. A 002 - GRADING
ROCKVILLE, MD 20850-1387 003 - MAJOR STRUCTURES
PHONE : 301-652-2400 004 - ASPHALT CONCRETE PAVING
FAX : 301-424-4443 007 - MINOR STRUCTURES
045 - UNDERGROUND UTILITIES

BUSINESS CONTACT: FEMIANO, DANIEL JOSEPH
EMAIL: DANF@FODAY.COM

-----DBE INFORMATION-----

DBE TYPE : N/A
DBE CONTACT: N/A
=====



Attachment 4.2.4

Evidence of Obtaining Bonding





CHUBB GROUP OF INSURANCE COMPANIES

15 Mountain View Road
P.O. Box 1615
Warren, NJ 07061-1615

February 19, 2014

Commonwealth of Virginia
Virginia Department of Transportation
1401 E. Broad Street
Richmond, Virginia 23219

Re: Contractor Prequalification Letter for Francis O. Day Company, Inc.
I-495 Northern Section Shoulder Use
Fairfax County, Virginia
Project No. (FO) 0495-029-123, P101, C501
Contract ID# C00105130DB72

To whom it may concern:

We understand that Francis O. Day Company, Inc. is submitting a proposal for the above referenced project. Accordingly, Francis O. Day Company, Inc. has asked us to provide you with the information required in the Instructions to Offerors.

As surety for Francis O. Day Company, Inc., the Federal Insurance Company, with A.M. Best Financial Strength Rating A++ and Financial Size Category XV, is capable of obtaining 100% Performance Bond and 100% Labor and Materials Payment Bond in the amount of the anticipated cost of construction, and said bonds will cover the Project and any warranty periods as provided for in the Contract Documents on behalf of the Contractor, in the event that such firm be the successful bidder and enter into a contract for this Project.

The Federal Insurance Company has participated in the surety program of Francis O. Day Company, Inc. for the past 25 years and have had favorable experience and have a high regard for them as a surety customer.

Very truly yours,
FEDERAL INSURANCE COMPANY

A handwritten signature in black ink that reads 'Laurie L. Kessler' in a cursive script.

Laurie L. Kessler
Attorney-in-fact



Attachment 4.2.5

SCC Registrations and DPOR Licenses



ATTACHMENT 4.2.5

State Project No. (FO)0495-029-123, P101, C501

SCC and DPOR Information

Offerors shall complete the table and include the required state registration and licensure information. By completing this table, Offerors certify that their team complies with the requirements set forth in Section 4.2.5 and that all businesses listed are active and in good standing.

SCC & DPOR INFORMATION FOR BUSINESSES (RFP Sections 4.2.5.1 and 4.2.5.2)							
Business Name	SCC Information (4.2.5.1)			DPOR Information (4.2.5.2)			
	SCC Number	SCC Type of Corporation	SCC Status	DPOR Registered Address	DPOR Registration Type	DPOR Registration Number	DPOR Expiration Date
Francis O. Day Co., Inc.	F048224-2	Corporation	Active	850 East Gude Drive Suite A Rockville, MD 20850	Class A Contractor	2705051002	06-30-2015
Johnson, Mirmiran & Thompson, Inc.	F149901-3	Corporation	Active	72 Loveton Circle Sparks, MD 21152	ENG, LA, ARC, LA	0407001314	12-31-2015
				13921 Park Center Rd. Suite 140 Herndon, VA 20171	ENG, LS	0411000441	02-29-2016
				9201 Arboretum Pkwy. Suite 310 Richmond, VA 23236	ENG, LS	0411000029	02-29-2016
				272 Bendix Rd., St. 260 Virginia Beach, VA 23452	ENG, LS	0411000440	02-29-2016
Quinn Consulting Services, Inc.	0492551-7	Corporation	Active	14160 Newbrook Dr. Suite 229 Chantilly, VA 20151	ENG	0407003733	12-31-2015
DMY Engineering Consultants, Inc.	0768895-5	Corporation	Active	45662 Terminal Drive Suite 110 Dulles, VA 20166	ENG	0407005631	12-31-2015
EEE Consulting, Inc.	0504941-6	Corporation	Active	8525 Bell Creek Road Mechanicsville, VA 23116	ENG	0407003798	12-31-2015
Athavale, Lystad & Associates, Inc.	F060584-2	Corporation	Active	8180 Greensboro Dr. Suite 550 McLean, VA 22102	ENG	0407002804	12-31-2015



Attachment 4.2.5

SCC Registrations



Commonwealth of Virginia
State Corporation Commission

Virg

CISM0180

CORPORATE DATA INQUIRY

02/27/14

14:43:16

CORP ID: F048224 - 2 STATUS: 00 ACTIVE STATUS DATE: 05/22/85
 CORP NAME: DAY CO., INC., FRANCIS O.

DATE OF CERTIFICATE: 05/23/1985 PERIOD OF DURATION: INDUSTRY CODE: 00
 STATE OF INCORPORATION: MD MARYLAND STOCK INDICATOR: S STOCK
 MERGER IND: S SURVIVOR CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: TRAC - THE REGISTERED AGENT COMPANY

STREET: 201 N. UNION STREET AR RTN MAIL:
 SUITE 230
 CITY: ALEXANDRIA STATE : VA ZIP: 22314
 R/A STATUS: 5 B.E. AUTH IN VI EFF. DATE: 01/08/14 LOC : 200
 ACCEPTED AR#: 213 07 7551 DATE: 04/30/13 ALEXANDRIA CITY
 CURRENT AR#: 213 07 7551 DATE: 04/30/13 STATUS: A ASSESSMENT INDICATOR: 0

YEAR	FEES	PENALTY	INTEREST	TAXES	BALANCE	TOTAL SHARES
13	100.00					1,500

 (Screen Id:/Corp_Data_Inquiry)



Commonwealth of Virginia
State Corporation Commission

CISM0180

CORPORATE DATA INQUIRY

02/24/14

09:58:04

CORP ID: F149901 - 3 STATUS: 00 ACTIVE STATUS DATE: 10/17/06
 CORP NAME: Johnson, Mirmiran & Thompson, Inc.

DATE OF CERTIFICATE: 10/17/2006 PERIOD OF DURATION: INDUSTRY CODE: 70
 STATE OF INCORPORATION: MD MARYLAND STOCK INDICATOR: S STOCK
 MERGER IND: CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: 50.00 MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: ROBERT GALLAGHER

STREET: 9201 ARBORETUM PKY STE 140 AR RTN MAIL:

CITY: RICHMOND STATE : VA ZIP: 23236
 R/A STATUS: 2 OFFICER EFF. DATE: 09/06/07 LOC : 120
 ACCEPTED AR#: 213 56 5183 DATE: 02/20/14 CHESTERFIELD CO
 CURRENT AR#: 213 56 5183 DATE: 02/20/14 STATUS: A ASSESSMENT INDICATOR: 0

YEAR	FEES	PENALTY	INTEREST	TAXES	BALANCE	TOTAL SHARES
13	100.00					1,000

 (Screen Id:/Corp_Data_Inquiry)



Commonwealth of Virginia
State Corporation Commission

CISM0180

CORPORATE DATA INQUIRY

03/12/14

17:07:22

CORP ID: 0492551 - 7 STATUS: 00 ACTIVE STATUS DATE: 12/01/08
 CORP NAME: QUINN CONSULTING SERVICES INCORPORATED

DATE OF CERTIFICATE: 10/24/1997 PERIOD OF DURATION: INDUSTRY CODE: 00
 STATE OF INCORPORATION: VA VIRGINIA STOCK INDICATOR: S STOCK
 MERGER IND: S SURVIVOR CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: 50.00 MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: JOHN H QUINN JR

STREET: 2208 S KNOLL ST AR RTN MAIL:

CITY: ARLINGTON STATE : VA ZIP: 22202 2134
 R/A STATUS: 4 ATTORNEY EFF. DATE: 10/24/97 LOC : 106
 ACCEPTED AR#: 213 12 8953 DATE: 08/21/13 ARLINGTON COUNT
 CURRENT AR#: 213 12 8953 DATE: 08/21/13 STATUS: A ASSESSMENT INDICATOR: 0

YEAR	FEES	PENALTY	INTEREST	TAXES	BALANCE	TOTAL SHARES
13	100.00					5,000

 (Screen Id:/Corp_Data_Inquiry)



Commonwealth of Virginia
State Corporation Commission

Virg

CISM0180

CORPORATE DATA INQUIRY

02/26/14

21:06:08

CORP ID: 0768895 - 5 STATUS: 00 ACTIVE STATUS DATE: 09/06/13
 CORP NAME: DMY ENGINEERING CONSULTANTS INC.
 DATE OF CERTIFICATE: 09/06/2013 PERIOD OF DURATION: INDUSTRY CODE: 00
 STATE OF INCORPORATION: VA VIRGINIA STOCK INDICATOR: S STOCK
 MERGER IND: CONVERSION/DOMESTICATION IND: Y
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: 50.00 MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: WEIYI MA
 STREET: 45662 TERMINAL DRIVE AR RTN MAIL:
 SUITE 110
 CITY: DULLES STATE : VA ZIP: 20166
 R/A STATUS: 1 DIRECTOR EFF. DATE: 09/06/13 LOC : 153
 ACCEPTED AR#: 000 00 0000 DATE: LOUDOUN COUNTY
 CURRENT AR#: 000 00 0000 DATE: STATUS: ASSESSMENT INDICATOR: 0
 YEAR FEES PENALTY INTEREST TAXES BALANCE TOTAL SHARES
 00 10,000

 (Screen Id:/Corp_Data_Inquiry)



Commonwealth of Virginia State Corporation Commission



CISM0180

CORPORATE DATA INQUIRY

02/26/14

21:05:23

CORP ID: 0504941 - 6 STATUS: 00 ACTIVE STATUS DATE: 08/04/04
CORP NAME: EEE CONSULTING, INC.

DATE OF CERTIFICATE: 06/23/1998 PERIOD OF DURATION: INDUSTRY CODE: 00
STATE OF INCORPORATION: VA VIRGINIA STOCK INDICATOR: S STOCK
MERGER IND: CONVERSION/DOMESTICATION IND:
GOOD STANDING IND: Y MONITOR INDICATOR:
CHARTER FEE: 700.00 MON NO: MON STATUS: MONITOR DTE:
R/A NAME: CT CORPORATION SYSTEM

STREET: 4701 COX ROAD, SUITE 285 AR RTN MAIL:

CITY: GLEN ALLEN STATE : VA ZIP: 23060
R/A STATUS: 5 B.E. AUTH IN VI EFF. DATE: 10/04/13 LOC : 143
ACCEPTED AR#: 213 52 4464 DATE: 05/24/13 HENRICO COUNTY
CURRENT AR#: 213 52 4464 DATE: 05/24/13 STATUS: A ASSESSMENT INDICATOR: 0
YEAR FEES PENALTY INTEREST TAXES BALANCE TOTAL SHARES
13 100.00 333,000

(Screen Id:/Corp_Data_Inquiry)



Commonwealth of Virginia
State Corporation Commission

Virg

CISM0180

CORPORATE DATA INQUIRY

02/26/14

21:03:14

CORP ID: F060584 - 2 STATUS: 00 ACTIVE STATUS DATE: 03/02/89
 CORP NAME: ATHAVALLE, LYSTAD & ASSOCIATES, INC.

DATE OF CERTIFICATE: 03/02/1989 PERIOD OF DURATION: INDUSTRY CODE: 00
 STATE OF INCORPORATION: MD MARYLAND STOCK INDICATOR: S STOCK
 MERGER IND: CONVERSION/DOMESTICATION IND:
 GOOD STANDING IND: Y MONITOR INDICATOR:
 CHARTER FEE: MON NO: MON STATUS: MONITOR DTE:
 R/A NAME: REES BROOME, PC

STREET: 1900 GALLOWS RD STE 700 AR RTN MAIL:

CITY: TYSONS CORNER STATE : VA ZIP: 22182
 R/A STATUS: 5 B.E. AUTH IN VI EFF. DATE: 09/01/12 LOC : 129
 ACCEPTED AR#: 214 03 4212 DATE: 02/06/14 FAIRFAX COUNTY
 CURRENT AR#: 214 03 4212 DATE: 02/06/14 STATUS: A ASSESSMENT INDICATOR: 0

YEAR	FEES	PENALTY	INTEREST	TAXES	BALANCE	TOTAL SHARES
14	100.00					1,000

 (Screen Id:/Corp_Data_Inquiry)



Attachment 4.2.5

DPOR Licenses

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA

EXPIRES ON
06-30-2015

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER
2705051002

BOARD FOR CONTRACTORS
CLASS A CONTRACTOR
CLASSIFICATIONS H/H

FRANCIS O DAY CO INC
850 EAST GUDE DRIVE SUITE A
ROCKVILLE, MD 20850



Gordon N. Dixon
Gordon N. Dixon, Director

ALTERATION OF THIS DOCUMENT, USE AFTER EXPIRATION, OR USE BY PERSONS OR FIRMS OTHER THAN THOSE NAMED MAY RESULT IN CRIMINAL PROSECUTION UNDER THE CODE OF VIRGINIA.

(SEE REVERSE SIDE FOR NAME AND/OR ADDRESS CHANGE)

(POCKET CARD)

COMMONWEALTH OF VIRGINIA
CLASS A BOARD FOR CONTRACTORS
CONTRACTOR

CLASSIFICATIONS H/H
NUMBER: 2705051002 EXPIRES: 06-30-2015

FRANCIS O DAY CO INC
850 EAST GUDE DRIVE SUITE A
ROCKVILLE, MD 20850



(FOLD)

(DETACH HERE)

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233

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DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA

EXPIRES ON
12-31-2015

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER
0407001314

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION

PROFESSIONS: ENG, LA, ARC, LS

JOHNSON MIRMIRAN & THOMPSON INC
72 LOVETON CIRCLE
SPARKS, MD 21152



Gordon N. Dixon
Gordon N. Dixon, Director

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COMMONWEALTH OF VIRGINIA
BOARD FOR APPEALS
BUSINESS ENTITY REGISTRATION
NUMBER: 0407001314 EXPIRES: 12-31-2015
PROFESSIONS: ENG, LA, ARC, LS
JOHNSON MIRMIRAN & THOMPSON INC
72 LOVETON CIRCLE
SPARKS, MD 21152



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DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA

EXPIRES ON
02-28-2014

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER
0411000441

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY BRANCH OFFICE REGISTRATION

PROFESSIONS: ENG, LS

JOHNSON MIRMIRAN & THOMPSON INC
13921 PARK CENTER RD
HERNDON, VA 20171



Gordon K. Dumas
Gordon K. Dumas, Director

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COMMONWEALTH OF VIRGINIA
BOARD FOR APPLSCIDLA
BUSINESS ENTITY BRANCH OFFICE REGISTRATION
NUMBER: 0411000441 EXPIRES: 02-28-2014
PROFESSIONS: ENG, LS
JOHNSON MIRMIRAN & THOMPSON INC
13921 PARK CENTER RD
HERNDON, VA 20171



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DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA

EXPIRES ON
02-29-2016

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER
0411000029

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY BRANCH OFFICE REGISTRATION

PROFESSIONS: ENG, LS

JOHNSON, MIRMIRAN & THOMPSON, INC.
9201 ARBORETUM PKWY
SUITE 310
RICHMOND, VA 23236



Nick A. Christner
Nick A. Christner, Interim Director

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COMMONWEALTH OF VIRGINIA
BOARD FOR AP/ELSC/DLA
BUSINESS ENTITY BRANCH OFFICE REGISTRATION
NUMBER: 0411000029 EXPIRES: 02-29-2016
PROFESSIONS: ENG, LS
JOHNSON, MIRMIRAN & THOMPSON, INC.
9201 ARBORETUM PKWY
SUITE 310
RICHMOND, VA 23236



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COMMONWEALTH OF VIRGINIA

EXPIRES ON
02-29-2016

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER
0411000440

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY BRANCH OFFICE REGISTRATION

PROFESSIONS: ENG, LS

JOHNSON MIRMIRAN & THOMPSON INC
272 BENDIX ROAD
SUITE 260
VIRGINIA BEACH, VA 23452



Nick A. Christner
Nick A. Christner, Interim Director

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COMMONWEALTH OF VIRGINIA
BOARD FOR APPLICANTS
BUSINESS ENTITY BRANCH OFFICE REGISTRATION
NUMBER: 0411000440 EXPIRES: 02-29-2016
PROFESSIONS: ENG, LS
JOHNSON MIRMIRAN & THOMPSON INC
272 BENDIX ROAD
SUITE 260
VIRGINIA BEACH, VA 23452



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COMMONWEALTH OF VIRGINIA

EXPIRES ON
12-31-2015

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER
0407003733

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION

PROFESSIONS: ENG

QUINN CONSULTING SERVICES INC
14160 NEWBROOK DR
SUITE 220
CHANTILLY, VA 20151



Gordon N. Dixon
Gordon N. Dixon, Director

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COMMONWEALTH OF VIRGINIA

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BOARD FOR APELSCIDLA
BUSINESS ENTITY REGISTRATION
NUMBER: 0407003733 EXPIRES: 12-31-2015
PROFESSIONS: ENG
QUINN CONSULTING SERVICES INC
14160 NEWBROOK DR
SUITE 220
CHANTILLY, VA 20151



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COMMONWEALTH OF VIRGINIA

EXPIRES ON

12-31-2015

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER

0407005631

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION

PROFESSIONS: ENG

DMY ENGINEERING CONSULTANTS INC
45662 TERMINAL DRIVE
SUITE 110
DULLES, VA 20166



Gordon N. Dixon
Gordon N. Dixon, Director

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EXPIRES ON
12-31-2015

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER
0407003798

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION

PROFESSIONS: ENG

EEE CONSULTING INC
8525 BELL CREEK RD
MECHANICSVILLE, VA 23116



Gordon N. Dixon
Gordon N. Dixon, Director

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COMMONWEALTH OF VIRGINIA
BOARD FOR APPEALS
BUSINESS ENTITY REGISTRATION
NUMBER: 0407003798 EXPIRES: 12-31-2015
PROFESSIONS: ENG
EEE CONSULTING INC
8525 BELL CREEK RD
MECHANICSVILLE, VA 23116



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DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA

EXPIRES ON
12-31-2015

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER
0407002804

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION

PROFESSIONS: ENG

ATHAVALE, LYSTAD AND ASSOCIATES INC
8180 GREENSBORO DRIVE
#550
MCLEAN, VA 22102



Gordon N. Dixon
Gordon N. Dixon, Director

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Attachment 4.2.6

Lead Contractor and Designer Work History Forms





Attachment 4.2.6(a)

Lead Contractor Work History Forms



ATTACHMENT 4.2.6(a)

LEAD CONTRACTOR - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

a. Project Name & Location	b. Name of the prime design consulting firm responsible for the overall project design.	c. Contact information of the Client or Owner and their Project Manager who can verify Firm's responsibilities.	d. Contract Completion Date (Original)	e. Contract Completion Date (Actual or Estimated)	f. Contract Value (in thousands)		g. Dollar Value of Work Performed by the Firm identified as the Lead Contractor for this procurement.(in thousands)
					Original Contract Value	Final or Estimated Contract Value	
Name: I-270 West Spur at Democracy Boulevard and Westlake Terrace Location: Montgomery County, MD	Name: The Wilson T. Ballard Company	Name of Client/Owner: Maryland State Highway Admin. District 3 Phone: 301.513.7300 Project Manager: Darrell Mobley (currently Director of Prince George's County DPW&T) Phone: 301.883.5600 Email: dbmobley@co.pg.md.us	September 2003 (Original)	November 2004 (Actual) MSHA issued Extra Work Order for additional 414 days	\$17,023 Contract Value (Original)	\$18,096 Contract Value (Final)	\$9,469 Construction Fee

h. Narrative describing the Work Performed by the Firm identified as the Lead Contractor for this procurement. If the Offeror chooses to submit work completed by an affiliated or subsidiary company of the Lead Contractor, identify the full legal name of the affiliate or subsidiary and the role they will have on this Project, so the relevancy of that work can be considered accordingly.

Lead Contractor



Francis O. Day Co., Inc. (F.O. Day) was the low bidder on this project in January 2001, and work commenced on July 9, 2001. This interstate paving project located in an urban area involved:

- Maintenance of traffic;
- Reconstruction of the Democracy Boulevard bridge over I-270 under traffic;
- Construction of a new HOV bridge;
- Widening of the existing Westlake Terrace Bridge;
- Installation of concrete barrier wall, retaining walls;
- Construction of a new noise wall;
- Temporary under-pinning for pier cap repairs;
- Reconstruction of existing ramp interchange at I-270 and Democracy Boulevard;
- Construction of new ramps from Democracy Boulevard to I-270;
- Storm drain;
- Asphalt paving;
- Signalization installations;
- Permanent signage (includes overhead and cantilever signs);
- Construction of traffic barrier W-beam; and
- Final pavement markings.

The reconstruction of the Democracy Boulevard bridge occurred under traffic in phases. The entire existing bridge was removed and replaced with new construction. The traffic was controlled via bolting down single face temporary barrier wall for each phase.

The newly constructed HOV Bridge was constructed via placing temporary barrier wall on both sides of I-270 with ingress/egress points created. This bridge had four piers and tied in to the existing Westlake Terrace Bridge.

The Westlake Terrace Bridge was widened one lane under traffic as well. I-270 was improved and overlaid with various HMA mixes as well.

SIMILAR SCOPE ACTIVITIES

- ✓ ITS Devices/Systems (DMS, CCTV, Vehicle Detection, RWIS)
- ✓ Electrical, Communications and Fiber Optic Conduit, Wiring and Hook-ups
- ✓ Configuring, Integrating and Testing New ATM Field Devices for Proper Operation and Control
- ✓ Field Support for System Software
- ✓ Integration and Testing
- ✓ Surveying
- ✓ Asphalt Paving
- ✓ Underlying concrete joint repairs
- ✓ Signing and Striping
- ✓ Overhead Sign Structures/Lighting
- ✓ Dynamic Message Signs
- ✓ Drainage, E&S Control and SWM
- ✓ Concrete Barrier and Pier Protection
- ✓ Maintenance of Traffic
- ✓ Inspection, Testing, QA and QC

PROJECT HIGHLIGHTS

- ✓ Completed project on time within Owner adjusted time frame

This project fulfills RFP/ Addendum#1 requirements 4.2.6 for a project completed constr. within the last fifteen (15) years. In addition this project meets the requirements for:

1. An interstate paving project with a min. contract value of \$10M.
2. An interstate paving project located in urban area with a min. contract value of \$15M.

ATTACHMENT 4.2.6(a)

LEAD CONTRACTOR - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

a. Project Name & Location	b. Name of the prime design consulting firm responsible for the overall project design.	c. Contact information of the Client or Owner and their Project Manager who can verify Firm's responsibilities.	d. Contract Completion Date (Original)	e. Contract Completion Date (Actual or Estimated)	f. Contract Value (in thousands)		g. Dollar Value of Work Performed by the Firm identified as the Lead Contractor for this procurement.(in thousands)
					Original Contract Value	Final or Estimated Contract Value	
Name: MD 650 (New Hampshire Avenue) from MD 212 (Powder Mill Road) to North of U.S. 29 Location: Montgomery County, MD	Name: Jacobs Civil, Inc.	Name of Client/Owner: Maryland State Highway Admin. District 3 Phone: 301.513.7300 Project Manager: Neil Haines (currently Resident Construction Engineer, Office of Construction) Phone: 443.572.5233 Email: nhaines@sha.state.md.us	March 2006 (Original)	Jan 2006 (Actual) Substantial Completion	\$9,597 Contract Value (Original)	\$10,901 Contract Value (Final)	\$6,741 Construction Fee

h. Narrative describing the Work Performed by the Firm identified as the Lead Contractor for this procurement. If the Offeror chooses to submit work completed by an affiliated or subsidiary company of the Lead Contractor, identify the full legal name of the affiliate or subsidiary and the role they will have on this Project, so the relevancy of that work can be considered accordingly.

<p>Lead Contractor</p>   	<p>Francis O. Day Co., Inc. (F.O. Day) was the low bidder on this unique project as it provided the infrastructure for the new Food and Drug Administration (FDA) White Oak Campus. This major arterial paving project in an urban area included the widening of MD 650 (New Hampshire Avenue) from MD 212 (Powder Mill Road) to north of U.S. 29 for a total length of 2.058 miles just outside the I-495 (Capital Beltway). Construction services on this project included:</p> <ul style="list-style-type: none"> ▪ Construction of three 11-foot through lanes southbound; ▪ Four 11-foot through lanes northbound; ▪ 5-foot bike lanes in each direction, curb and gutter both sides; ▪ 5-foot sidewalk on both sides; ▪ 8-foot bike trail along part of northbound side; ▪ Retaining walls at the Shopping Center which included a brick form liner with stain applied; ▪ Widening of Lockwood Drive (east and west) including sidewalk and bike path construction; ▪ Widening of eastbound Powder Mill Road; ▪ Widening and removal of all ramps between MD 650 and U.S. 29; ▪ Improvements to Shopping Center parking lot; ▪ Stormwater management including newly constructed two SWM Ponds; and ▪ Grading and drainage improvements for all phases, permanent signs, pavement markings, and permanent signalization at five different intersections. <p>Through the course of the project, F.O. Day encountered an unknown tie-back system. F.O. Day worked aggressively with the MSHA to restore the structural integrity of the tie-back system and through redesign and construction, while significantly mitigating the impact to MSHA. This project also included numerous permanent utility relocations that required extensive coordination with the utility companies and F.O. Day, along MD 650.</p> <p>The paving on the project consisted of 13,608 TN of 12.5 MM HMA, 9,036 TN of 25 MM HMA, 4,497 TN of 19 MM HMA, and 4,077 TN of 9.5 MM for Wege/Leveling. The Project also required 91,544 SY of Grinding of existing asphalt.</p> <p>F.O. Day completed the Lockwood Drive portion of this project ahead of schedule at the acceptance of MSHA, based on the incentive program established.</p>	<p>SIMILAR SCOPE ACTIVITIES</p> <ul style="list-style-type: none"> ✓ ITS Devices/Systems (DMS, CCTV, Vehicle Detection, RWIS) ✓ Electrical, Communications and Fiber Optic Conduit, Wiring and Hook-ups ✓ Configuring, Integrating and Testing New ATM Field Devices for Proper Operation and Control ✓ Field Support for System Software ✓ Integration and Testing ✓ Surveying ✓ Asphalt Paving ✓ Underlying concrete joint repairs ✓ Signing and Striping ✓ Overhead Sign Structures/Lighting ✓ Dynamic Message Signs ✓ Drainage, E&S Control and SWM ✓ Maintenance of Traffic ✓ Inspection, Testing, QA and QC ✓ Retaining Wall <p>PROJECT HIGHLIGHTS</p> <ul style="list-style-type: none"> ✓ Nominated for an MdQI Partnering Award. ✓ Completed a portion ahead of schedule, which was key to the opening of FDA's White Oak Campus. ✓ Completed the contract within the allotted Contract time of 450 Working Days
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This project fulfills RFP/ Addendum#1 requirements 4.2.6 for a project completed constr. within the last fifteen (15) years. In addition this project meets the requirements for:

1. A major arterial paving project with a min. contract value of \$10M.

ATTACHMENT 4.2.6(a)

LEAD CONTRACTOR - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

a. Project Name & Location	b. Name of the prime design consulting firm responsible for the overall project design.	c. Contact information of the Client or Owner and their Project Manager who can verify Firm's responsibilities.	d. Contract Completion Date (Original)	e. Contract Completion Date (Actual or Estimated)	f. Contract Value (in thousands)		g. Dollar Value of Work Performed by the Firm identified as the Lead Contractor for this procurement.(in thousands)
					Original Contract Value	Final or Estimated Contract Value	
Name: I-270 from North of MD 189 (Falls Road) to Muddy Branch Road Location: Montgomery County, MD	Name: Maryland State Highway Administration	Name of Client/Owner: Maryland State Highway Admin. District 3 Phone: 301.513.7300 Project Manager: Brian Young (currently District Engineer) Phone: 301.513.7300 Email: byoung@sha.state.md.us	December 2007 (Original)	November 2007 (Actual)	\$9,910 Contract Value (Original)	\$10,133 Contract Value (Final)	\$6,753 Construction Fee

h. Narrative describing the Work Performed by the Firm identified as the Lead Contractor for this procurement. If the Offeror chooses to submit work completed by an affiliated or subsidiary company of the Lead Contractor, identify the full legal name of the affiliate or subsidiary and the role they will have on this Project, so the relevancy of that work can be considered accordingly.

<p>Lead Contractor</p>    <div style="background-color: #4a86e8; color: white; padding: 5px; border-radius: 10px; margin-top: 10px;"> <p>This project fulfills RFP/ Addendum#1 requirements 4.2.6 for a project completed constr. within the last fifteen (15) years. In addition this project meets the requirements for:</p> <ol style="list-style-type: none"> 1. An interstate paving project with a min. contract value of \$10M. </div>	<p>Francis O. Day Co., Inc. (F.O. Day) was the low bidder on this project in February 2007, and work commenced on April 23, 2007.</p> <p>This interstate paving project in an urban area involved the resurfacing and safety improvements on I-270 from 1,000 feet north of MD 189 (Falls Road) to Muddy Branch Road. The work consisted of:</p> <ul style="list-style-type: none"> ○ Maintenance of traffic; ○ Patching; ○ 686,630 SY of grinding; ○ 80,745 Tons of gap graded resurfacing; ○ ADA ramps; and ○ Traffic barrier W-beam. <p>The maintenance of traffic included major detours at each interchange, resulting in numerous temporary signs and Variable Message Signs. The sidewalk ramp works were per ADA requirements. All storm drain located within the project limits was cleaned out and flushed. The majority of the permanent striping consisted of Preformed Patterned markings applied during surface paving operations, and all other was Thermoplastic. The Traffic Barrier W-Beam works consisted of special 'Brown Polyester Coating' applied prior to shipping. A 'Road Weather Information Sensor Station' was installed with detection loops for indicators as well.</p>	<p>SIMILAR SCOPE ACTIVITIES</p> <ul style="list-style-type: none"> ✓ ITS Devices/Systems (DMS, CCTV, Vehicle Detection, RWIS) ✓ Electrical, Communications and Fiber Optic Conduit, Wiring and Hook-ups ✓ Configuring, Integrating and Testing New ATM Field Devices for Proper Operation and Control ✓ Field Support for System Software ✓ Integration and Testing ✓ Surveying ✓ Asphalt Paving ✓ Underlying concrete joint repairs ✓ Signing and Striping ✓ Overhead Sign Structures/Lighting ✓ Dynamic Message Signs ✓ Drainage, E&S Control and SWM ✓ Concrete Barrier and Pier Protection ✓ Maintenance of Traffic ✓ Inspection, Testing, QA and QC <p>PROJECT HIGHLIGHTS</p> <ul style="list-style-type: none"> ✓ Complete ahead of schedule.
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Attachment 4.2.6(b)

Lead Designer Work History Forms

ATTACHMENT 4.2.6(b)

LEAD DESIGNER - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

a. Project Name & Location	b. Name of the prime/ general contractor responsible for overall construction of the project.	c. Contact information of the Client and their Project Manager who can verify Firm's responsibilities.	d. Construction Contract Completion Date (Original)	e. Construction Contract Completion Date (Actual or Estimated)	f. Contract Value (in thousands)		g. Design Fee for the Work Performed by the Firm identified as the Lead Designer for this procurement.(in thousands)
					Construction Contract Value (Original)	Construction Contract Value (Actual or Estimated)	
Name: I-95/I-695 Interchange (Section 100) Preliminary and Final Design Location: Baltimore/Balt. Co., MD	Name: G.A. & F.C. Wagman, Inc. / McLean Contracting Company A Joint Venture	Name of Client: Maryland Transportation Authority Phone: 410.931.0110 x251 Project Manager: David Labella, P.E. Phone: 410.931.0110 x251 Email: dlabela@mdta.state.md.us	April 2011 (Original)	April 2011 (Actual)	\$450,000 Contract Value (Original)	\$450,000 Contract Value (Actual)	\$26,000 Design Fee

h. Narrative describing the Work Performed by the Firm identified as the Lead Designer for this procurement. Include the office location(s) where the design work was performed and whether the firm was the prime designer or a subconsultant.		
<p>Prime Designer</p>  <p>Design Work Performed at: Sparks, MD (Corporate); Herndon, VA; and York, PA</p>   <div style="border: 1px solid blue; border-radius: 15px; padding: 5px; background-color: #e6f2ff; margin-top: 10px;"> <p>This project fulfills RFP/ Addendum#1 requirements 4.2.6 for a project completed constr. within the last fifteen (15) years. In addition this project meets the requirements for:</p> <ol style="list-style-type: none"> 1. An interstate project located in an urban area with a min. constr. value of \$15M. 2. An ITS highway project with a min. constr. Value of \$2M. </div>	<p>Developed the planning and preliminary design for Section 100 of the I-95 Express Toll Lane project and final design on the I-95/I-695 Interchange. This \$1B project involved complete replacement of three (3) major interchanges and mainline interstate design which implemented the latest technologies in Traffic and ITS Management. Section 100 is the first project within Maryland to implement both general purpose lanes and managed lanes in the same facility. Preliminary and final design services included:</p> <ul style="list-style-type: none"> ▪ Traffic/ITS/Lighting - Designed the communications system for the toll facility as well as field equipment locations used for overall traffic management purposes. The systems included CCTV surveillance, Remote Weather Information Systems, Highway Advisory Radio, vehicle detection and classification and DMS systems were all included in the design. Interim plans used wireless communications to existing and temporary field devices were developed. Special provisions for all of the specialized ITS equipment were researched, developed and finalized for this project and for use with the adjacent projects. Responsible for electrical design drawings, specifications; voltage drop and computer lighting calculations; and coordination with utilities agencies. This project consisted of High Mast Lighting and Conventional Lighting design in accordance with MDOT/AASHTO requirements. Power distribution and branch circuit designs for both lighting and ITS elements were developed. Performed capacity and operational analyses, design of MOT plans, signing, pavement marking plans, and evaluated constructability issues. The signing plans included new guide signing on the I-95 and I-695 approaches to the interchange including the use of overhead sign structures. ▪ Highway Design - The preliminary design included developing and analyzing multiple concepts for General Purpose and Managed Lanes for I-95's mainline. In addition, JMT evaluated multiple line and grade options for the I-695 MD 43, and I-895 interchanges. One of the unique challenges was the re-design of the existing "double-braided" directional I-695 interchange to a fully directional multilevel design serving both the Express Toll Lanes and the General Purpose lanes of I-95. The preliminary design encompassed an array of design elements i.e. horizontal and vertical alignment, typical sections, developing and reviewing design standards, minimizing impacts to the existing ROW and utilities. Final design included (11) lane-miles of I-95, (12) lane-miles of I-695, (1) lane-mile of local roads and (16) lane-miles of ramps. ▪ Structural Engineering - Prepared preliminary layouts of bridge structures in a complex network of multilevel interchange design at the MD 43, I-695 & I-895 interchanges that involved many elevated structures for the directional ramps. The preliminary structural design work included superstructure types, span arrangements, pier location and sizes and phased construction. Prepared final design plans for (22) bridges, (38) retaining walls, (7) noise barriers and (5) culverts at the I-95/I-695 interchange. ▪ Additional Services - Wetland and forest delineation, coordination with environmental agencies (MDE, USACOE, MHT, etc.), permitting requirements, drainage and SWM, utility coordination and relocations, H/H analyses and an extensive public involvement and interagency coordination. JMT used focus group meetings with agencies and communities to establish the Purpose and Need, ARDS document and FONSI. Brochures, displays and material for the I-95 Section 100 website were prepared. All design was performed using GEOPAK and MicroStation, in accordance with MDTA, AASHTO and MSHA guidelines. <p>This project received the ACEC/MD Honor Award for Studies, Research & Consulting Engineering Services; the Mid-Atlantic Award of Merit; the ACI/MD Excellence in Concrete Award; National Partnership for Highway Quality Award; and the MdQI Award of Excellence (Partnering Silver).</p>	<p>SIMILAR SCOPE ACTIVITIES</p> <ul style="list-style-type: none"> ✓ ITS Devices/Systems (DMS, CCTV, Vehicle Detection, RWIS) ✓ Electrical, Communications and Fiber Optic Conduit, Wiring and Hook-ups ✓ Configuring, Integrating and Testing New ATM Field Devices for Proper Operation and Control ✓ Field Support for System Software ✓ Integration and Testing ✓ Surveying ✓ Asphalt Paving ✓ Underlying concrete joint repairs ✓ Signing and Striping ✓ Overhead Sign Structures/Lighting ✓ Dynamic Message Signs ✓ Drainage, E&S Control and SWM ✓ Concrete Barrier and Pier Protection ✓ Maintenance of Traffic ✓ Inspection, Testing, QA and QC <p>PROJECT HIGHLIGHTS</p> <ul style="list-style-type: none"> ✓ First highway project in MD to implement general purpose lanes and express toll lanes together in the same facility. ✓ Implemented the latest technologies in Traffic and ITS Management. ✓ Led the design of improvements to several miles of I-95. ✓ Complete realignment of the I-95/I-695 interchange. ✓ JMT fast tracked the design in 10 months.

ATTACHMENT 4.2.6(b)

LEAD DESIGNER - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

a. Project Name & Location	b. Name of the prime/ general contractor responsible for overall construction of the project.	c. Contact information of the Client and their Project Manager who can verify Firm's responsibilities.	d. Construction Contract Completion Date (Original)	e. Construction Contract Completion Date (Actual or Estimated)	f. Contract Value (in thousands)		g. Design Fee for the Work Performed by the Firm identified as the Lead Designer for this procurement.(in thousands)
					Construction Contract Value (Original)	Construction Contract Value (Actual or Estimated)	
Name: MD 200 Intercounty Connector (ICC) Contract C (Design-Build) Location: Montgomery and Prince George's Counties, MD	Name: ICC Constructors (IC3) - Shirley Contracting Company, LLC - Clark Construction Group, LLC - Trumbull Corporation - Atkinson Construction Co., LLC - Facchina Construction Co., Inc. - Dewberry & Davis, LLC	Name of Client: Maryland State Highway Admin. Phone: 410.545.0400 Project Manager: Melinda Peters (Currently MSHA Administrator) Phone: 410.545.0400 Email: mpeters@sha.state.md.us	November 2011 (Original)	November 2011 (Actual)	\$513,900 Contract Value (Original)	\$513,900 Contract Value (Actual)	\$7,077 Design Fee
h. Narrative describing the Work Performed by the Firm identified as the Lead Designer for this procurement. Include the office location(s) where the design work was performed and whether the firm was the prime designer or a subconsultant.							
<p>Prime Designer</p>  <p>Design Work Performed at: Sparks, MD (Corporate) and Herndon, VA</p>  		<p>This project, one of the largest Design-Build contracts in Maryland, included 3.8 miles of new roadway, MD 200, to accommodate three highway lanes in each direction from west of U.S. 29 to east of I-95. Three new interchanges were designed at MD 200/US 29, MD 200/Briggs Chaney Road, and MD 200/I-95. The work also included the reconstruction and widening of 1.3 miles of U.S. 29 from south of Briggs Chaney Road to south of Fairland Road and the reconstruction of 1.9 miles of C-D roads along I-95 from south of MD 198 to north of MD 212. JMT as a subconsultant to the IC3 was the Lead ITS/Electronic Toll Collection (ETC) Engineer and Structural Engineer in addition to support water resources design.</p> <ul style="list-style-type: none"> ▪ Traffic/ITS/Lighting - Led the design of the ETC and the traffic management systems for Contract C. The system included CCTV surveillance, Dynamic Message Signs, vehicle detection systems, toll rate information signs, ETC toll gantries and the communication systems required to connect the field equipment to the central operations center. This project required constant coordination with utility companies. The design accommodates future improvements within the project limits, including several interchanges, light rail in the U.S. 29 median and electronic toll lanes in the I-95 median. Documents were in accordance with AASHTO, FHWA, MSHA and MDTA policies and coordinated to ensure compatibility and consistency with the overall ICC toll system. Also provided lighting design for the entire project. Both the U.S. 29 and Briggs Chaney Rd. Interchanges were designed for low level lighting using standard, decorative poles and fixtures. Along I-95, high mast lighting was utilized consistent with other areas of I-95. The work also included design of the power supply system for all lighting, toll gantries and signing. ▪ Structural Engineering - The design of over 50 structures, including bridges, culverts, retaining walls and noise barriers, was led by JMT. This work included the design of toll gantries, sign structures and high mast light poles, coordination of four design consultants as well as the liaison between the design team and MSHA, MDTA and the contractor. JMT's design work included 6 bridges, 3 noise barriers and 14 retaining walls. The bridges included both steel girder and concrete beam superstructures ranging from a 75 foot simple span to a 5-span 1,218 foot curved girder structure. Multiple plan packages were provided to facilitate early construction activities such as pier footings and ordering of long lead items such as structural steel. The bridges, retaining walls and noise barriers included many different aesthetic features unique to the project. ▪ Water Resources - Provided H/H studies, analysis and design; open/closed storm drain design; SWM and ESC analysis, computation and design; H/H, SWM and ESC plans, reports and permits; and consultation during construction for the MD 200 mainline. The SWM and ESC plans were developed to allow multiple phases to reduce areas of earth disturbance while allowing the contractor to meet his schedule of activities throughout the project limits. The ESC plans were coordinated with the MOT plans and utility relocations to facilitate construction activities. Provided detailed H/H design and scour analysis for Little Paint Branch, an environmentally sensitive stream. Performed H/H design for a temporary bridge crossing to be used as part of a haul road during construction. Coordinated the designs with the MSHA, the ICC Project Team and the Maryland Department of the Environment (MDE) and performed much of the design at the "Hub Office" facilitating the "over the shoulder" review process. JMT obtained MDE permits and approvals from the MDE for the stream crossings, ESC and SWM. 					<p>SIMILAR SCOPE ACTIVITIES</p> <ul style="list-style-type: none"> ✓ Surveying ✓ Asphalt Paving ✓ Underlying concrete joint repairs ✓ Signing and Striping ✓ Overhead Sign Structures/Lighting ✓ Dynamic Message Signs ✓ Overhead Lane Control System ✓ ITS Devices/Systems (DMS, CCTV, Detection, Video Analytics & Lane Control) ✓ Electrical, Communications and Fiber Optic Conduit, Wiring and Hook-ups ✓ Configuring, Integrating and Testing New ATM Field Devices for Proper Operation and Control ✓ Field Support for System Software ✓ Integration and Testing ✓ Drainage, E&S Control and SWM ✓ Concrete Barrier and Pier Protection ✓ Maintenance of Traffic ✓ Inspection, Testing, QA and QC <p>PROJECT HIGHLIGHTS</p> <ul style="list-style-type: none"> ✓ One of the largest D-B projects in MD. ✓ Approx.3.8 miles of new six-lane highway. ✓ State-of-the-art electronic toll collection system and traffic management system. ✓ Design of over 50 structures (toll gantries, sign structures and high mast light poles).
<p>This project fulfills RFP/ Addendum#1 requirements 4.2.6 for a project completed constr. within the last fifteen (15) years. In addition this project meets the requirements for:</p> <ol style="list-style-type: none"> 1. A major arterial project located in an urban area with a min. constr. value of \$15M. 2. An ITS highway project with a min. constr. Value of \$2M. 							

ATTACHMENT 4.2.6(b)

LEAD DESIGNER - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

a. Project Name & Location	b. Name of the prime/ general contractor responsible for overall construction of the project.	c. Contact information of the Client and their Project Manager who can verify Firm's responsibilities.	d. Construction Contract Completion Date (Original)	e. Construction Contract Completion Date (Actual or Estimated)	f. Contract Value (in thousands)		g. Design Fee for the Work Performed by the Firm identified as the Lead Designer for this procurement.(in thousands)
					Construction Contract Value (Original)	Construction Contract Value (Actual or Estimated)	
Name: Fairfax County Parkway Extension Phases I/II and IV (Design-Build) Location: Springfield, VA	Name: Cherry Hill Construction, Inc.	Name of Client: Federal Highway Administration Eastern Federal Lands Highway Div. Phone: 703.404.6302 Project Manager: Robert Morris Phone: 703.404.6302 Email: robert.morris@dot.gov	Phases I/II December 2010 (Original) Phase IV July 2011 (Original)	Phases I/II September 2010 (Actual) Phase IV July 2011 (Actual)	\$73,756 Contract Value (Excluded Owners Option #3 and Phase IV)	\$112,416 Contract Value (Actual) Increase due to be awarded Phase IV	\$11,538 Design Fee

h. Narrative describing the Work Performed by the Firm identified as the Lead Designer for this procurement. Include the office location(s) where the design work was performed and whether the firm was the prime designer or a subconsultant.

Prime Designer

Design Work Performed at:
 Sparks, MD (Corporate);
 Herndon and Richmond, VA




This project fulfills RFP/ Addendum#1 requirements 4.2.6 for a project completed constr. within the last fifteen (15) years. In addition this project meets the requirements for:

1. A major arterial project located in an urban area with a min. constr. value of \$15M.

The EFLHD, as the representative for VDOT/U.S. Army Garrison Fort Belvoir /Fairfax County, selected the Design-Build Team that included Cherry Hill Construction as the Contract and JMT as the Lead Designer, for this \$112M D-B project. The 4-lane divided limited access highway completed the missing connection of FCP to I-95 and provides critical access to the National Geospatial-Intelligence Agency's (NGA) facility at Fort Belvoir EPG. To meet the requirements of BRAC the FCP project had an extremely aggressive schedule of 750 calendar days to design, permit, relocate utilities, and construct the parkway. The design team initiated design upon notice of award beginning in October 2008 and delivered approval for construction plans that allowed constr. of the western end (west of Accotink Creek) of the project to commence in April 2009. Full plan approval for the remainder of the contract was received in August 2009, approximately 3 months ahead of schedule. This accelerated design schedule allowed earlier construction starts that enabled the opening of Phases I/II in Sept. 2010, nearly a month ahead of scheduled.

- **Traffic/ITS/Lighting** - JMT's Lead Traffic/ITS engineer Randy Boice, PE, originally designed the ITS communication backbone through the project area around the I-95 ramps. This knowledge allowed JMT to design around the backbone such that there was absolutely no disturbance to the I-95 traffic management system during construction. Other responsibilities included the signing and pavement marking design for the mainline, side streets and ramps; traffic analysis; signal designs for 6 intersections; and roadway lighting (full and partial interchange and crossroad lighting) design. Corridors with new signals were modeled and optimized. CORSIM, VISSIM and Synchro were all used in the design development and corridor optimization.
- **Design** - The design included a new interchange at FCP and Barta Rd. for access to West North Loop Rd. (NGA interior roadway network) within the new NGA facility. Extensive design collaboration/coordination with the U.S. Army for this access point was required and included coordination for UXO clearing, security lighting, overheight vehicle detection, geometry/utility connections. A majority of this roadway was located on the southern portion of the EPG. Environmental challenges were further complicated by the fast-track schedule, involvement of multiple stakeholders, and complex environmental and regulatory issues. Design services included a comprehensive investigation of the nature and extent of the contamination on several of these areas, including groundwater modeling to evaluate the impact of construction on the fate and transport of multiple contaminated groundwater plumes. The project also included widening of I-95 to accommodate a new exit lane designed as a certified Defense Access Rd. that provides direct access to the NGA. JMT completed the Base Option and Options 1-3 including Phase IV. Phase IV was completed in July 2011 and provides access to N.B. FCP from Boudinot Dr. by extending Boudinot Dr. beneath the FCP and constructing Loop B. Phase IV also provided a connection from FCP N.B. to Boudinot Dr. via Ramp B; and access from S.B. FCP to Boudinot Dr. via Ramp D and a new bridge crossing Accotink Creek

Key staff members received "Star Partner" awards for their exceptional dedication, teamwork, and professionalism in support of the project's goals by the NGA. The President of the U.S. made two visits to the project, initially to indicate the importance of the project to the NOVA roadway network, and secondly for Phase IV, which was heralded for the project's timely and effective use of ARRA funds.

- SIMILAR SCOPE ACTIVITIES**
- ✓ Surveying
 - ✓ Asphalt Paving
 - ✓ Underlying concrete joint repairs
 - ✓ Signing and Striping
 - ✓ Overhead Sign Structures/Lighting
 - ✓ Dynamic Message Signs
 - ✓ Overhead Lane Control System
 - ✓ ITS Devices/Systems (DMS, CCTV, Detection, Video Analytics & Lane Control)
 - ✓ Electrical, Communications and Fiber Optic Conduit, Wiring and Hook-ups
 - ✓ Configuring, Integrating and Testing New ATM Field Devices for Proper Operation and Control
 - ✓ Field Support for System Software
 - ✓ Integration and Testing
 - ✓ Drainage, E&S Control and SWM
 - ✓ Concrete Barrier and Pier Protection
 - ✓ Maintenance of Traffic
 - ✓ Inspection, Testing, QA and QC
- PROJECT HIGHLIGHTS**
- ✓ Completed a vital link to I-95 in NOVA.
 - ✓ The project was recognized by DBIA National & Mid-Atlantic Region, ACEC/VA, ACEC/MD, ACEC/MW and VTCA
 - ✓ Project finished a month ahead of schedule with an extremely aggressive schedule.



Attachment 4.2.7

Conceptual Roadway Plans

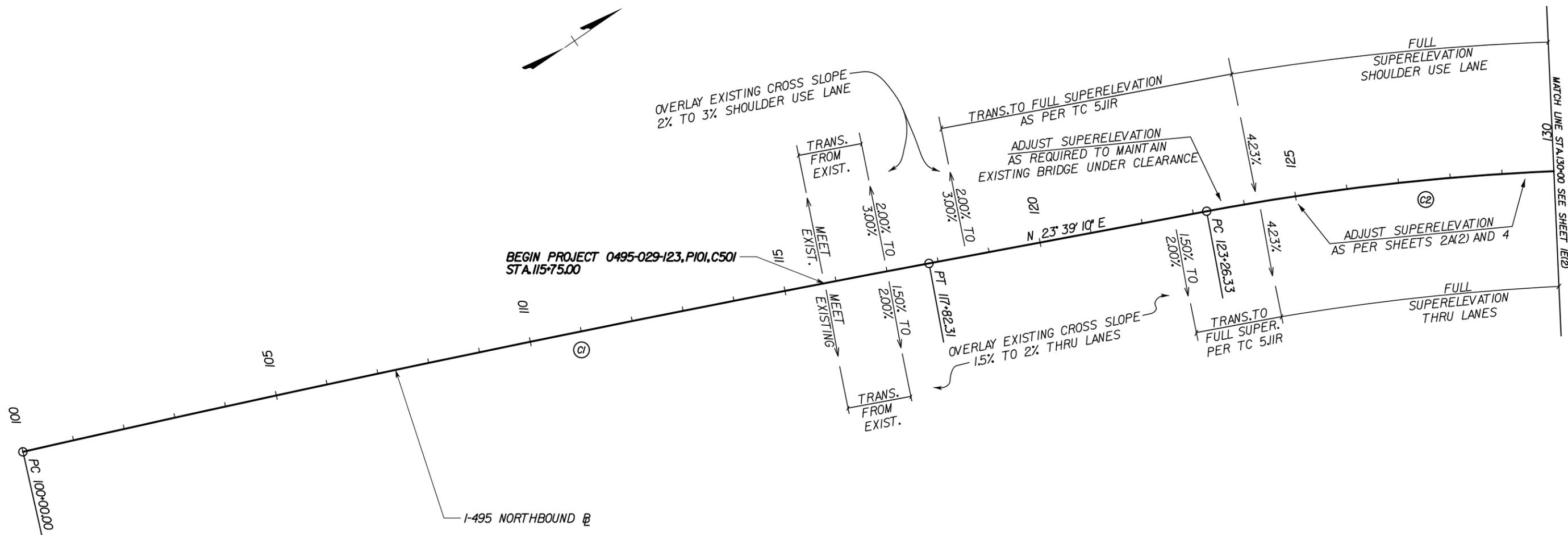


PROJECT MANAGER Paul Nishimura (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

CONSTRUCTION ALIGNMENT AND SUPERELEVATION DATA SHEET

REVISED	STATE		SHEET NO.
	STATE	PROJECT	
	VA.	495	0495-029-123 C501
			IE(1)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



Ⓒ1 Curve NBB001
 PI = 108+91.27
 DELTA = 2° 13' 04.40" (RT)
 D = 0° 07' 28"
 T = 891.27'
 L = 1,782.31'
 R = 46,043.27'
 PC = 100+00.00
 PT = 117+82.31
 E = MATCH EXISTING

Ⓒ2 Curve NBB002
 PI = 131+87.79
 DELTA = 20° 28' 27.99" (RT)
 D = 1° 12' 04"
 T = 861.46'
 L = 1,704.54'
 R = 4,770.00'
 PC = 123+26.33
 PT = 140+30.87
 E = 4.23%
 V = 70 MPH



CONCEPT PLANS
MARCH 12, 2014

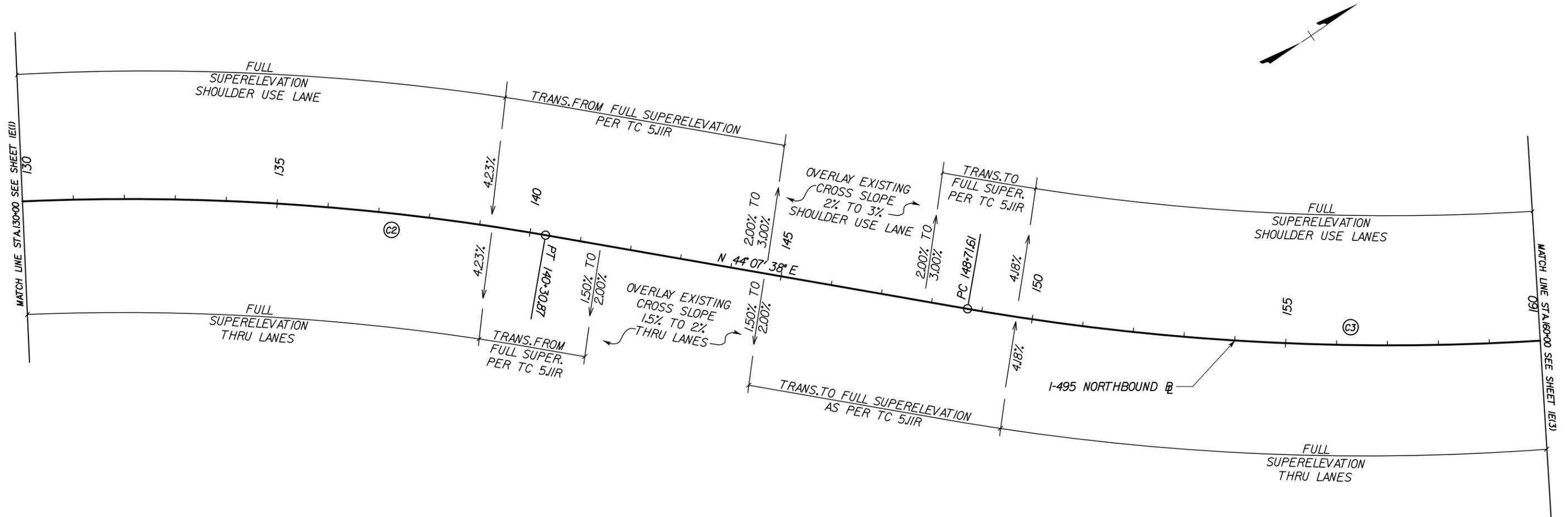
PROJECT	SHEET NO.
0495-029-123	IE(1)

PROJECT MANAGER Paul M. Ishimura (571) 483-2622
 SURVEYED BY, DATE VDOT
 DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
 Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE		PROJECT	SHEET NO.
	STATE	ROUTE		
	VA.	495	0495-029-123 C501	1E(2)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

CONSTRUCTION ALIGNMENT AND SUPERELEVATION DATA SHEET



(C2) Curve NBB002
 PI = 131+87.79
 DELTA = 20° 28' 27.99" (RT)
 D = 1' 12' 04"
 T = 861.46'
 L = 1704.54'
 R = 4770.00'
 PC = 123+26.33
 PT = 140+30.87
 E = 4.23%
 V = 70 MPH

(C3) Curve NBB003
 PI = 160+34.88
 DELTA = 27° 04' 57.77" (LT)
 D = 1' 11' 10"
 T = 1163.27'
 L = 2283.06'
 R = 4830.00'
 PC = 148+71.61
 PT = 171+54.66
 E = 4.18%
 V = 70 MPH



CONCEPT PLANS
 MARCH 12, 2014

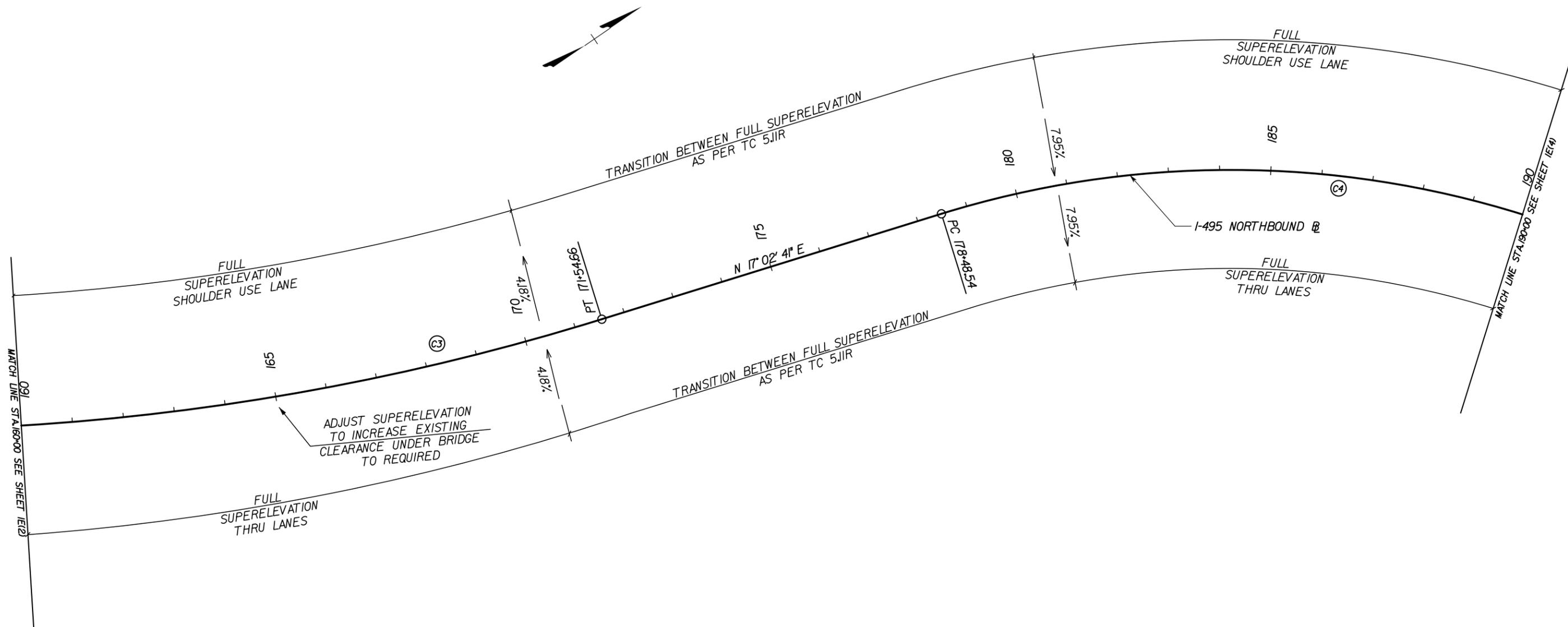
PROJECT	SHEET NO.
0495-029-123	1E(2)

PROJECT MANAGER Paul Mirmiran (571) 483-2622
 SURVEYED BY, DATE VDOT
 DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
 Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE		PROJECT	SHEET NO.
	STATE	ROUTE		
	VA.	495	0495-029-123 C501	1E(3)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

CONSTRUCTION ALIGNMENT AND SUPERELEVATION DATA SHEET



- (C3)** Curve NBB003
 PI - 160+34.88
 DELTA - 27° 04' 57.7" (LT)
 D - 111' 10"
 T - 1463.27'
 L - 2283.06'
 R - 4830.00'
 PC - 148+71.61
 PT - 171+54.66
 E - 4.18%
 V - 70 MPH
- (C4)** Curve NBB004
 PI - 186+04.22
 DELTA - 43° 10' 17.8" (RT)
 D - 2° 59' 59"
 T - 755.68'
 L - 1439.16'
 R - 1910.00'
 PC - 178+48.54
 PT - 192+87.70
 E - 7.95%
 V - 70 MPH



F.O. DAY
 Since 1944

JMT JOHNSON, MIRMIRAN & THOMPSON
 Engineering A Brighter Future®

CONCEPT PLANS
 MARCH 12, 2014

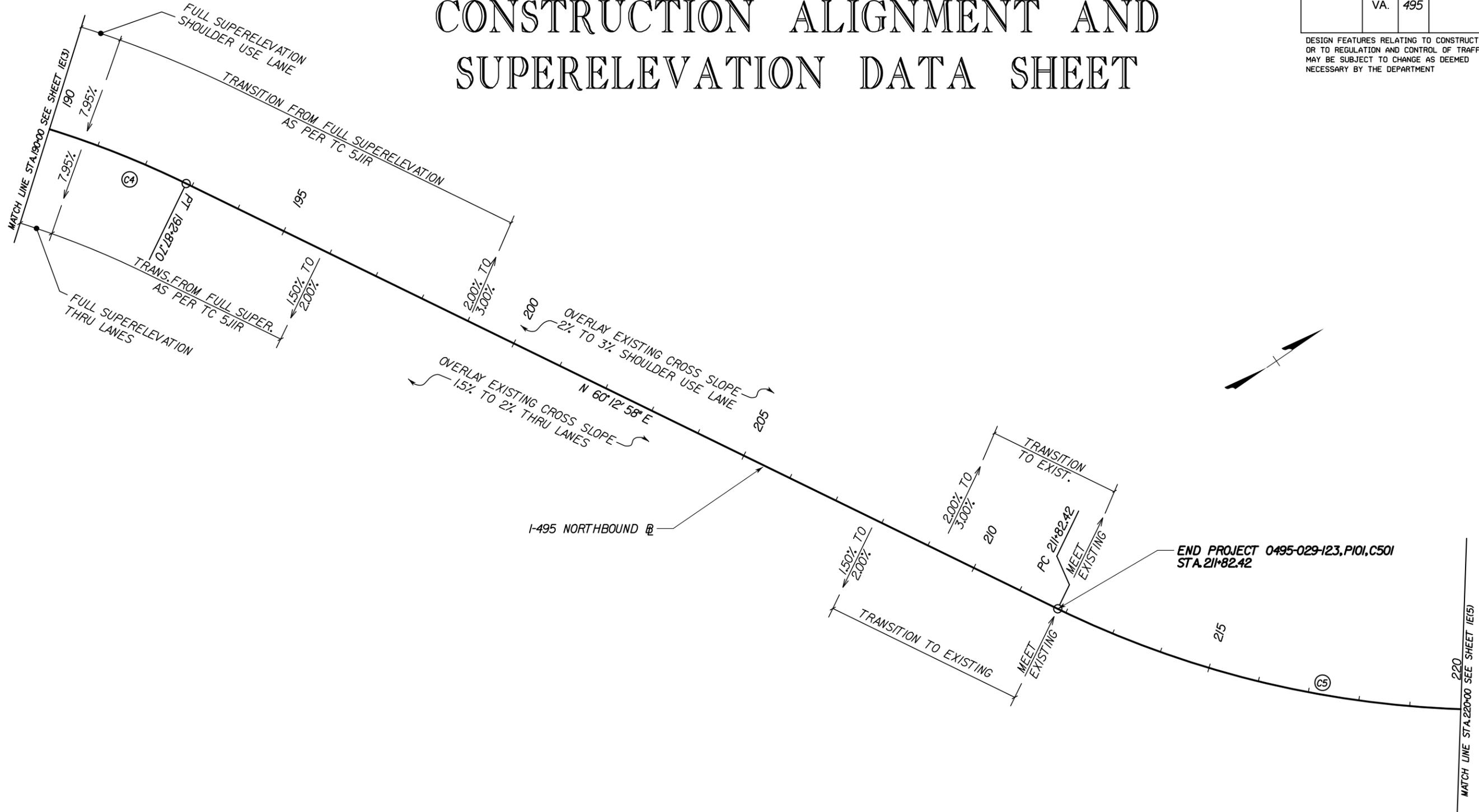
PROJECT	SHEET NO.
0495-029-123	1E(3)

PROJECT MANAGER Paul M. Ishimura (1571) 483-2622
 SURVEYED BY, DATE VDOT
 DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
 Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE		PROJECT	SHEET NO.
	STATE	ROUTE		
	VA.	495	0495-029-123 C501	1E(4)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

CONSTRUCTION ALIGNMENT AND SUPERELEVATION DATA SHEET



- (C4)** Curve NBB004
 PI = 186+04.22
 DELTA = 43°10'17.8" (RT)
 D = 2°59'59"
 T = 755.68'
 L = 1,439.16'
 R = 1,910.00'
 PC = 178+48.54
 PT = 192+87.70
 E = 7.95%
 V = 70 MPH
- (C5)** Curve NBB005
 PI = 221+05.40
 DELTA = 50°39'31.36" (LT)
 D = 2°56'18"
 T = 922.98'
 L = 1,724.11'
 R = 1,950.00'
 PC = 211+82.42
 PT = 229+06.54
 E = MATCH EXISTING
 V = 70 MPH



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MARCH 12, 2014

PROJECT	SHEET NO.
0495-029-123	1E(4)

CONSTRUCTION ALIGNMENT AND SUPERELEVATION DATA SHEET

REVISED	STATE		PROJECT	SHEET NO.
	STATE	ROUTE		
	VA.	495	0495-029-123 C501	1E(5)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

I-495 NORTHBOUND CONSTRUCTION BASELINE

Beginning chain 495REV2 description

Curve Data			
(C1)	Curve NBBO01	P.I. Station	108+91.27 N
		Delta	= 2° 13' 04.40" (RT)
		Degree	= 0° 07' 27.98"
		Tangent	= 891.27
		Length	= 1,782.31
		Radius	= 46,043.27
		External	= 8.63
		Long Chord	= 1,782.20
		Mid. Ord.	= 8.62
		P.C. Station	100+00.00 N
		P.T. Station	117+82.31 N
		C.C.	= N 21° 26' 06.04" E
		Back	= N 23° 39' 10.44" E
		Ahead	= N 22° 32' 38.24" E
		Chord Bear	= N 22° 32' 38.24" E

Course from PT NBBO01 to PC NBBO02 N 23° 39' 10.44" E Dist 544.02

Curve Data			
(C2)	Curve NBBO02	P.I. Station	131+87.79 N
		Delta	= 20° 28' 27.99" (RT)
		Degree	= 1° 12' 04.21"
		Tangent	= 861.46
		Length	= 1,704.54
		Radius	= 4,770.00
		External	= 77.17
		Long Chord	= 1,695.49
		Mid. Ord.	= 75.94
		P.C. Station	123+26.33 N
		P.T. Station	140+30.87 N
		C.C.	= N 23° 39' 10.44" E
		Back	= N 44° 07' 38.43" E
		Ahead	= N 33° 53' 24.43" E
		Chord Bear	= N 33° 53' 24.43" E

Course from PT NBBO02 to PC NBBO03 N 44° 07' 38.43" E Dist 840.73

Curve Data			
(C3)	Curve NBBO03	P.I. Station	160+34.88 N
		Delta	= 27° 04' 57.77" (LT)
		Degree	= 1° 11' 10.49"
		Tangent	= 1,163.27
		Length	= 2,283.06
		Radius	= 4,830.00
		External	= 138.11
		Long Chord	= 2,261.86
		Mid. Ord.	= 134.27
		P.C. Station	148+71.61 N
		P.T. Station	171+54.66 N
		C.C.	= N 44° 07' 38.43" E
		Back	= N 17° 02' 40.66" E
		Ahead	= N 30° 35' 09.55" E
		Chord Bear	= N 30° 35' 09.55" E

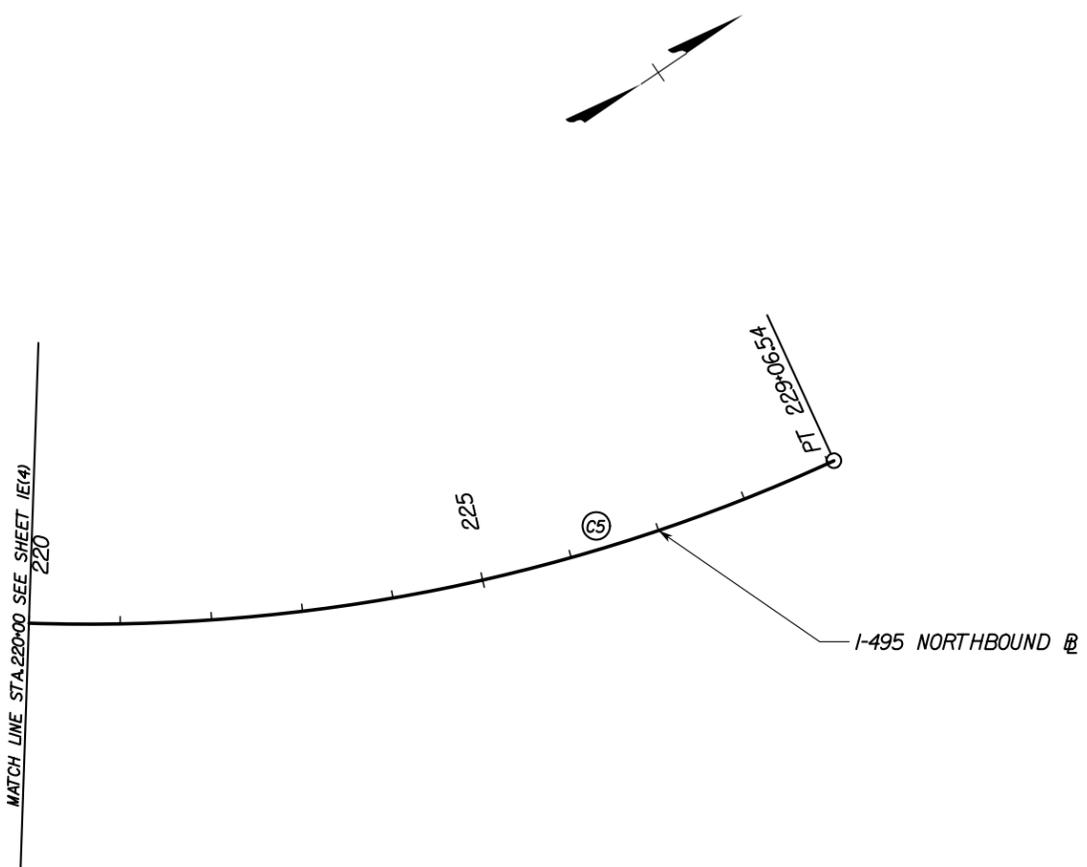
Course from PT NBBO03 to PC NBBO04 N 17° 02' 40.66" E Dist 693.88

Curve Data			
(C4)	Curve NBBO04	P.I. Station	186+04.22 N
		Delta	= 43° 10' 17.84" (RT)
		Degree	= 2° 59' 59.20"
		Tangent	= 755.69
		Length	= 1,438.16
		Radius	= 1,910.00
		External	= 144.06
		Long Chord	= 1,406.36
		Mid. Ord.	= 133.95
		P.C. Station	178+48.54 N
		P.T. Station	192+87.70 N
		C.C.	= N 17° 02' 40.66" E
		Back	= N 60° 12' 58.50" E
		Ahead	= N 38° 37' 49.58" E
		Chord Bear	= N 38° 37' 49.58" E

Course from PT NBBO04 to PC NBBO05 N 60° 12' 58.50" E Dist 1,894.72

Curve Data			
(C5)	Curve NBBO05	P.I. Station	221+05.40 N
		Delta	= 50° 39' 31.36" (LT)
		Degree	= 2° 56' 17.68"
		Tangent	= 922.98
		Length	= 1,724.11
		Radius	= 1,950.00
		External	= 207.41
		Long Chord	= 1,668.50
		Mid. Ord.	= 187.47
		P.C. Station	211+82.42 N
		P.T. Station	229+06.54 N
		C.C.	= N 60° 12' 58.50" E
		Back	= N 9° 33' 27.14" E
		Ahead	= N 34° 53' 12.82" E
		Chord Bear	= N 34° 53' 12.82" E

Ending chain 495REV2 description



(C5) Curve NBBO05
 PI = 221+05.40
 DELTA = 50° 39' 31.36" (LT)
 D = 2° 56' 18"
 T = 922.98'
 L = 1,724.11'
 R = 1,950.00'
 PC = 211+82.42
 PT = 229+06.54
 E = MATCH EXISTING
 V = 70 MPH



CONCEPT PLANS
 MARCH 12, 2014

PROJECT	SHEET NO.
0495-029-123	1E(5)

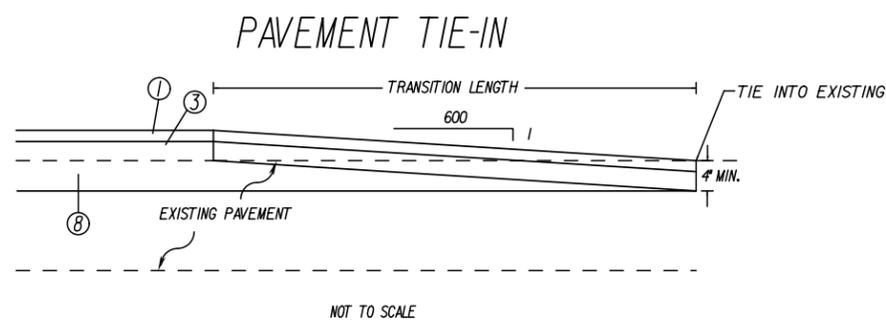
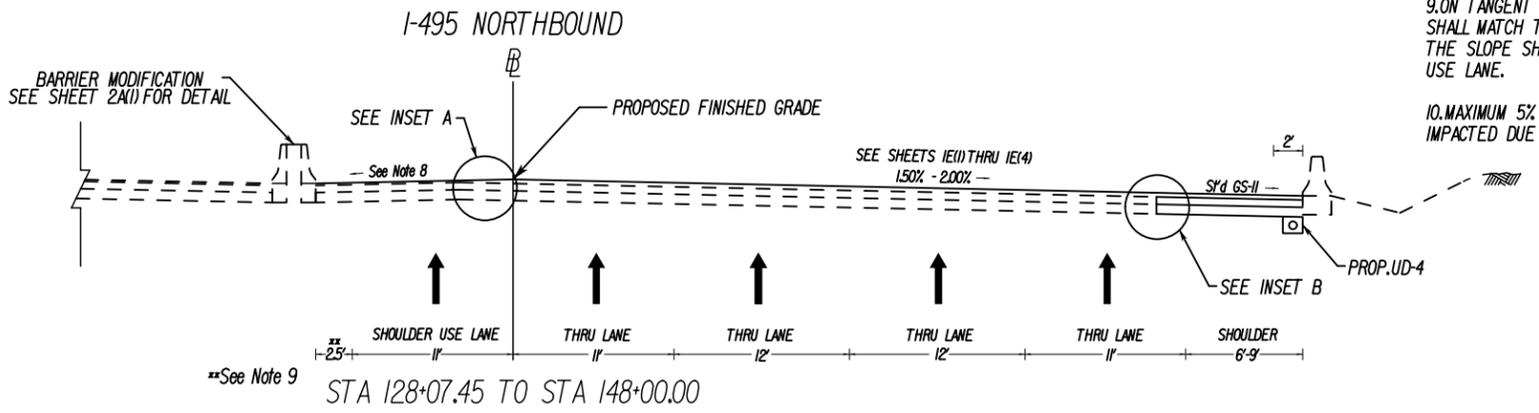
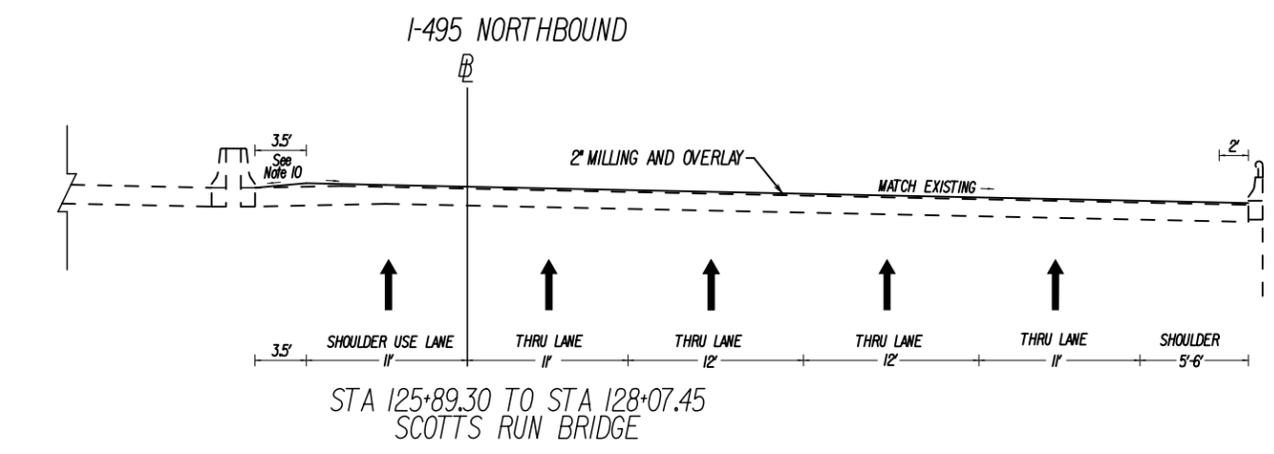
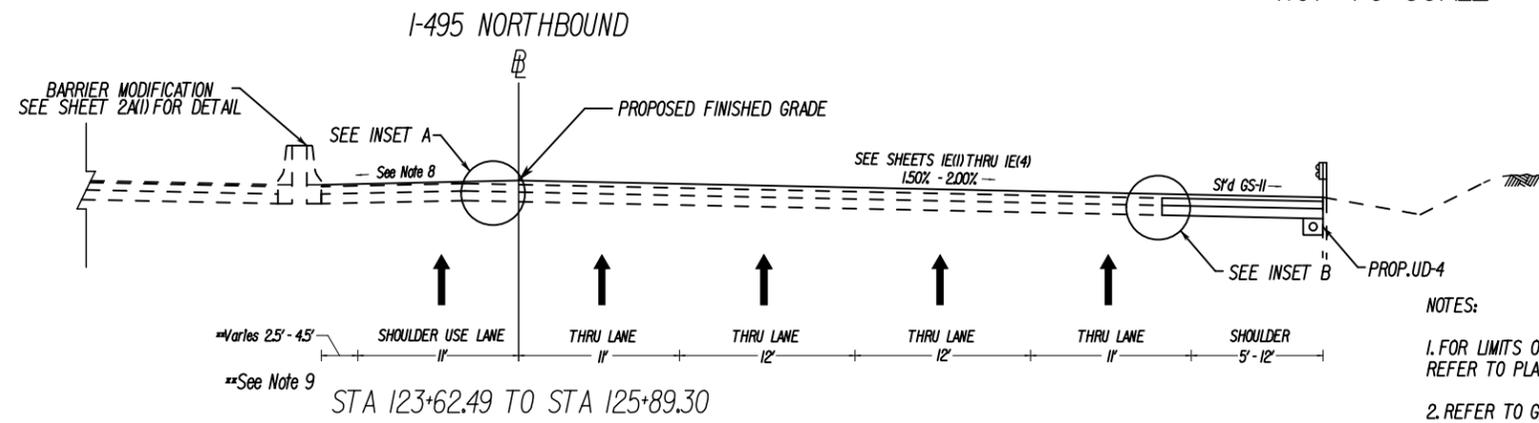
PROJECT MANAGER Paul M. Ishimaru (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE		PROJECT		SHEET NO.
	VA.	ROUTE	0495-029-123	C501	
					2A(2)

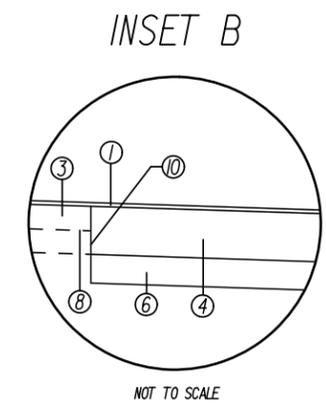
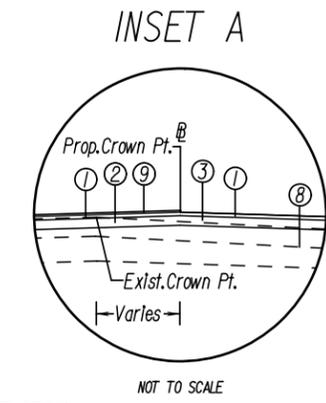
TYPICAL SECTIONS

NOT TO SCALE

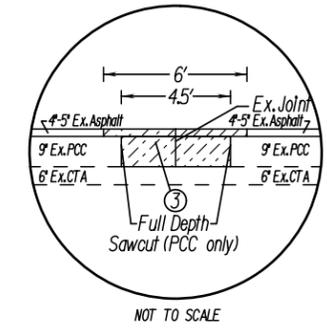
DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



- NOTES:
- FOR LIMITS OF MILL AND OVERLAY AND FULL DEPTH PAVEMENT, REFER TO PLAN SHEETS.
 - REFER TO GEOTECHNICAL ENGINEERING DATA REPORT FOR PAVEMENT DESIGN.
 - VARIABLE DEPTH MILL AND OVERLAY MAY BE NECESSARY FOR SLOPE CORRECTION.
 - FULL DEPTH/Joint REPAIRS ARE TO BE PERFORMED ON THE FULL WIDTH OF PAVEMENT AND SHOULDER PRIOR TO THE PLACEMENT OF FINAL SURFACE LAYERS OF ASPHALT.
 - ALL CRACKS SHALL BE CLEANED OF ALL DEBRIS AFTER MILLING AND SEALED WITH TYPE B CRACK SEALANT IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR SEALING CRACKS IN AC OR PCC PAVEMENT.
 - FINAL SURFACE TO BE PLACED UNIFORMLY ACROSS THE ENTIRE PAVEMENT.
 - WHEN LIQUID ASPHALT IS USED AS A CURING MATERIAL FOR THE CEMENT STABILIZED COURSE, IT SHALL BE LIQUID ASPHALT CRS-1, CRS-1H OR CMS-2 APPLIED AT A RATE OF 0.2 GAL/SY. WHERE NECESSARY FOR MAINTENANCE OF TRAFFIC, COVER MATERIAL CONSISTING OF NO. 10 AGGREGATE OR GRADING B SAND SHALL BE APPLIED AT A RATE OF 10 LB/SY.
 - ON HIGH SIDE OF SUPERELEVATION THE SLOPE IS TO MATCH THE SUPERELEVATION WITHOUT A ROLLOVER. ON TANGENT AND LOW SIDE OF SUPERELEVATION THE SLOPE SHALL MATCH EXISTING, BUT MAINTAIN MAXIMUM 5% ROLLOVER.
 - ON TANGENT AND LOW SIDE OF SUPERELEVATION THE SLOPE OF THE LATERAL OFFSET SHALL MATCH THE SHOULDER USE LANE SLOPE. ON THE HIGH SIDE OF SUPERELEVATION THE SLOPE SHALL MAINTAIN 5% ROLLOVER AT THE EDGE OF THE SHOULDER USE LANE.
 - MAXIMUM 5% ROLLOVER. EXISTING EXPANSION JOINT TO BE REPLACED WHERE IMPACTED DUE TO PAVEMENT BUILDUP.



FULL DEPTH PATCH/ JOINT REPAIR



- Surface - 1.5\"/>
- 2.5\"/>
- Variable Depth Asphalt Concrete Type BM-25.0D + 0.4 (HMHB, PG 70-22)
- Base - 1\"/>
- Base - 12\"/>
- Subbase - 8\"/>
- Subbase - 6\"/>
- Mill and Overlay Existing Pavement Minimum Depth of 4\"/>
- High Friction Surface Coating To Delineate Shoulder Use Lane
- Full depth sawcut at edge of existing mainline pavement to expose existing full strength pavement with PCC or expose existing edge of PCC pavement

- NOTES:
- PAVEMENT TIE-IN SHALL BE USED TO PROVIDE A SMOOTH TRANSITION BETWEEN PAVEMENT OVERLAY AND EXISTING PAVEMENT AT SCOTTS RUN BRIDGE, AT RAMP TIE-INS AND AT THE BEGINNING/END OF PROJECT. TRANSITION LENGTH SHALL BE 150' MINIMUM.
 - PAVEMENT TIE-IN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 315.05(C) OF THE SPECIFICATION EXCEPT THAT ALL JOINTS AT TIE-IN LOCATIONS SHALL BE DESIGNED USING A 10-FOOT STRAIGHT EDGE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 315.07(A) OF THE SPECIFICATIONS. THE VARIATION FROM THE TESTING EDGE OF THE STRAIGHT EDGE BETWEEN ANY TWO CONTACT POINTS WITH THE PAVEMENT SURFACE SHALL NOT EXCEED 1/4\"/>



CONCEPT PLANS
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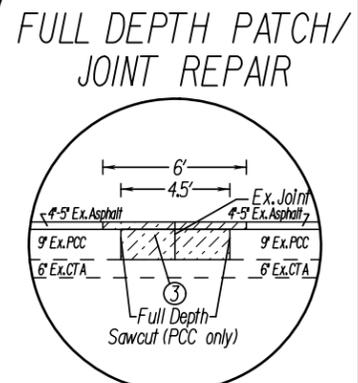
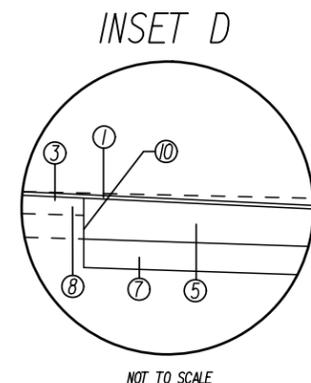
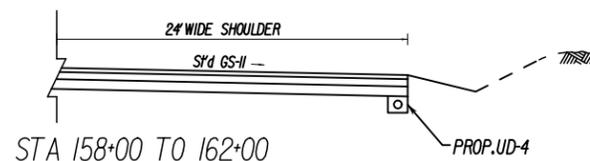
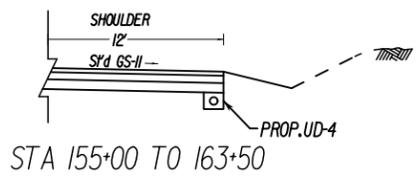
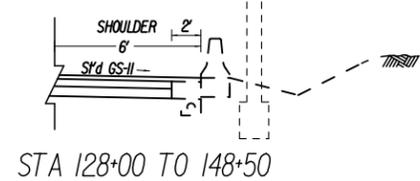
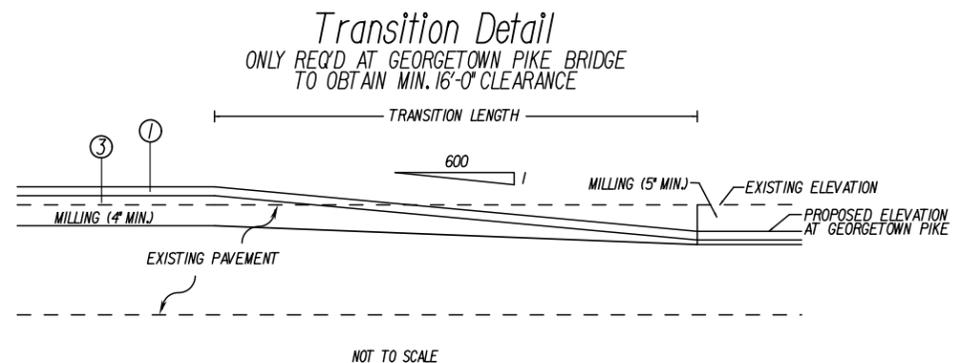
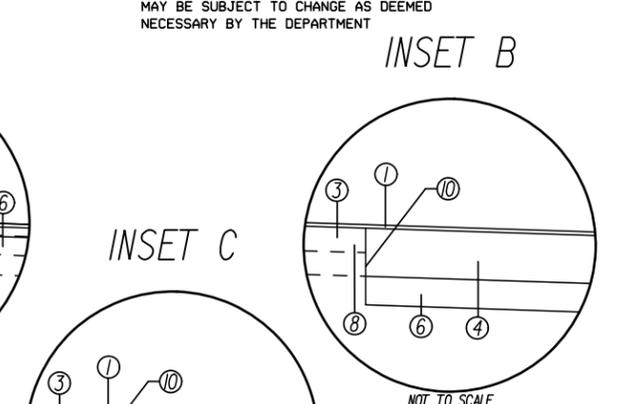
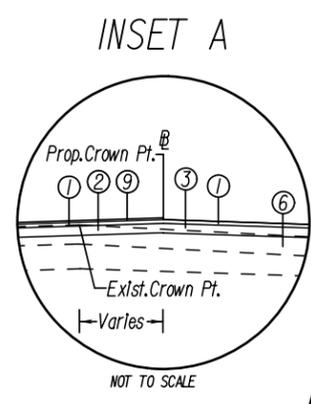
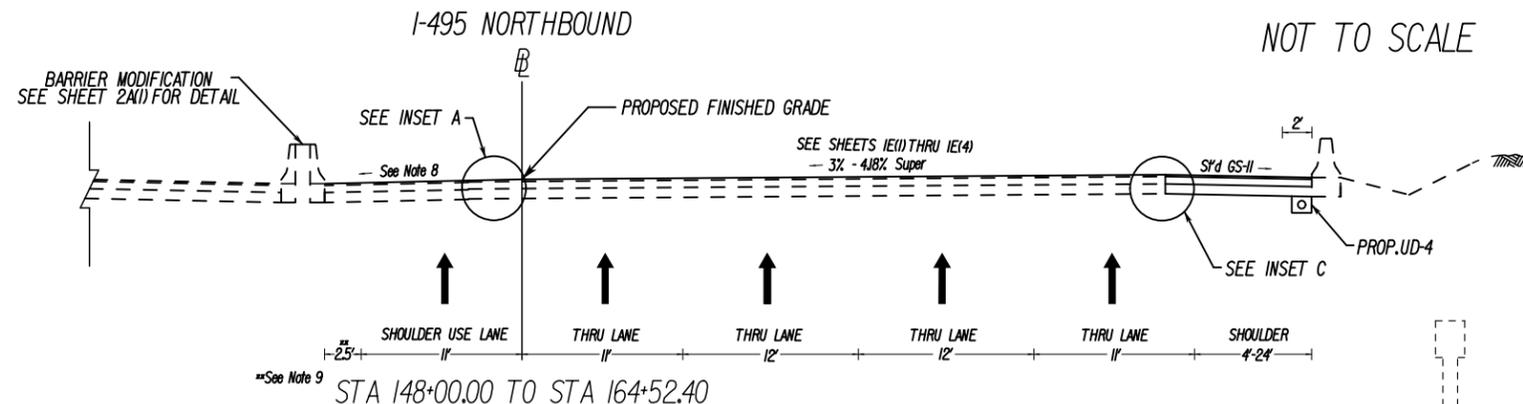
PROJECT	0495-029-123	SHEET NO.	2A(2)
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PROJECT MANAGER Paul M. Ishimaru (571) 483-2622
SURVEYED BY DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE		PROJECT		SHEET NO.
	VA.	ROUTE 495	0495-029-123	C501	

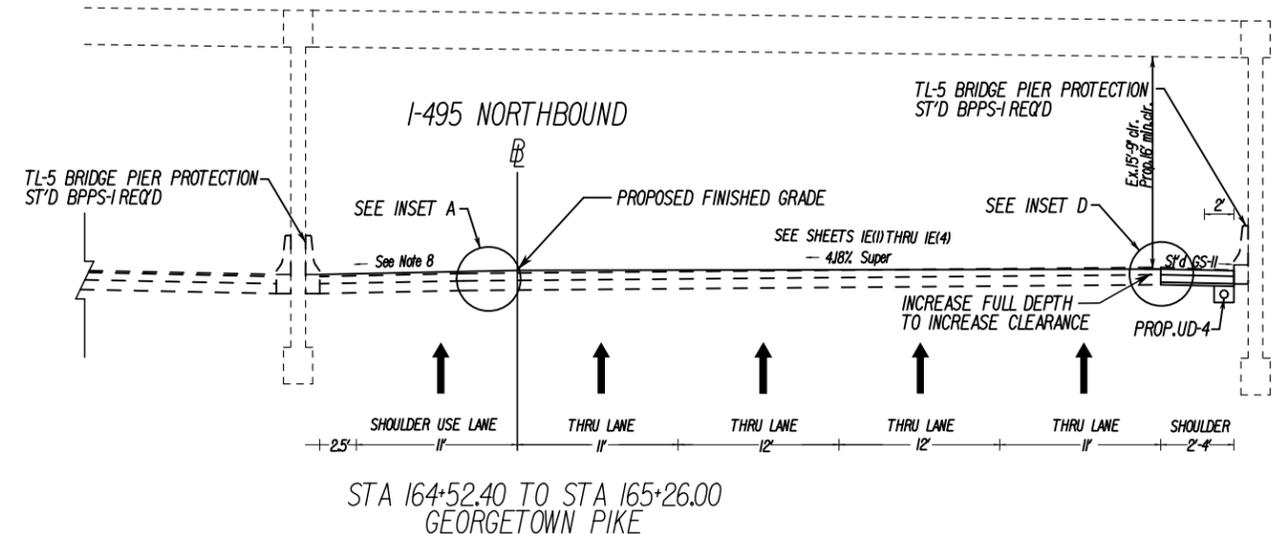
TYPICAL SECTIONS

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



- ① Surface - 1.5" Stone Matrix Asphalt, SMA-9.5E (PG 76-22)
- ② 2.5" Asphalt Concrete Type BM-25.0D + 0.4 (HMHB, PG 70-22)
- ③ Variable Depth Asphalt Concrete Type BM-25.0D + 0.4 (HMHB, PG 70-22)
- ④ Base - 1.4" Asphalt Concrete Type BM-25.0D + 0.4 (HMHB, PG 70-22)
- ⑤ Base - 1.2" Asphalt Concrete Type BM-25.0D + 0.4 (HMHB, PG 70-22)
- ⑥ Subbase - 8" Aggregate Base Material, Type 1, Size No. 21B
- ⑦ Subbase - 6" Aggregate Base Material, Type 1, Size No. 21A pugmill mixed with 4% hydraulic cement by weight
- ⑧ Mill and Overlay Existing Pavement Minimum Depth of 4"
- ⑨ High Friction Surface Coating To Delineate Shoulder Use Lane
- ⑩ Full depth sawcut at edge of existing mainline pavement to expose existing full strength pavement with PCC or expose existing edge of PCC pavement

- NOTES:
- FOR LIMITS OF MILL AND OVERLAY AND FULL DEPTH PAVEMENT, REFER TO PLAN SHEETS.
 - REFER TO GEOTECHNICAL ENGINEERING DATA REPORT FOR PAVEMENT DESIGN.
 - VARIABLE DEPTH MILL AND OVERLAY MAY BE NECESSARY FOR SLOPE CORRECTION.
 - FULL DEPTH/JOINT REPAIRS ARE TO BE PERFORMED ON THE FULL WIDTH OF PAVEMENT AND SHOULDER PRIOR TO THE PLACEMENT OF FINAL SURFACE LAYERS OF ASPHALT.
 - ALL CRACKS SHALL BE CLEANED OF ALL DEBRIS AFTER MILLING AND SEALED WITH TYPE B CRACK SEALANT IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR SEALING CRACKS IN AC OR PCC PAVEMENT.
 - FINAL SURFACE TO BE PLACED UNIFORMLY ACROSS THE ENTIRE PAVEMENT.
 - WHEN LIQUID ASPHALT IS USED AS A CURING MATERIAL FOR THE CEMENT STABILIZED COURSE, IT SHALL BE LIQUID ASPHALT CRS-1, CRS-1H OR CMS-2 APPLIED AT A RATE OF 0.2 GAL/SY. WHERE NECESSARY FOR MAINTENANCE OF TRAFFIC, COVER MATERIAL CONSISTING OF NO. 10 AGGREGATE OR GRADING B SAND SHALL BE APPLIED AT A RATE OF 10 LB/SY.
 - ON HIGH SIDE OF SUPERELEVATION THE SLOPE IS TO MATCH THE SUPERELEVATION WITHOUT A ROLLOVER. ON TANGENT AND LOW SIDE OF SUPERELEVATION THE SLOPE SHALL MATCH EXISTING, BUT MAINTAIN MAXIMUM 5% ROLLOVER.
 - ON TANGENT AND LOW SIDE OF SUPERELEVATION THE SLOPE OF THE LATERAL OFFSET SHALL MATCH THE SHOULDER USE LANE SLOPE. ON THE HIGH SIDE OF SUPERELEVATION THE SLOPE SHALL MAINTAIN 5% ROLLOVER AT THE EDGE OF THE SHOULDER USE LANE.



CONCEPT PLANS
MARCH 12, 2014

PROJECT	0495-029-123	SHEET NO.	2A(3)
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PROJECT MANAGER Paul M. Ishimura (571) 483-2622
SURVEYED BY DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

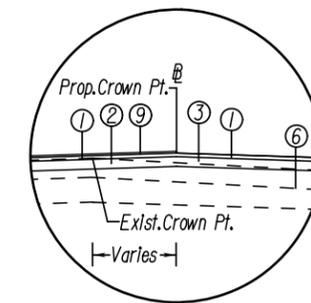
REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	495	0495-029-123 C501	2A(4)

TYPICAL SECTIONS

NOT TO SCALE

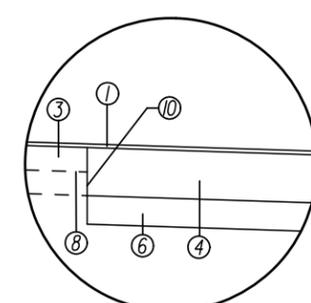
DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

INSET A



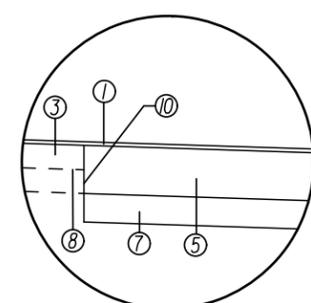
NOT TO SCALE

INSET B



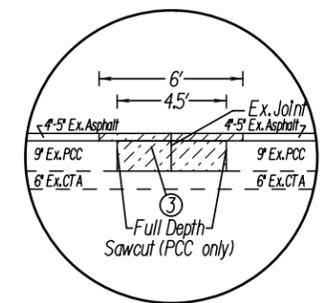
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INSET C

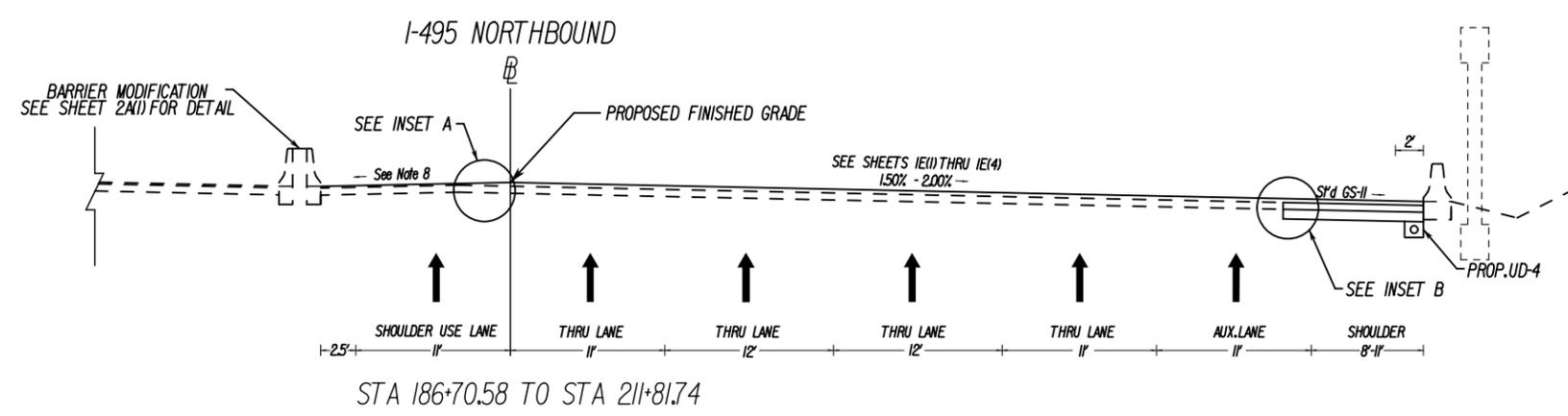
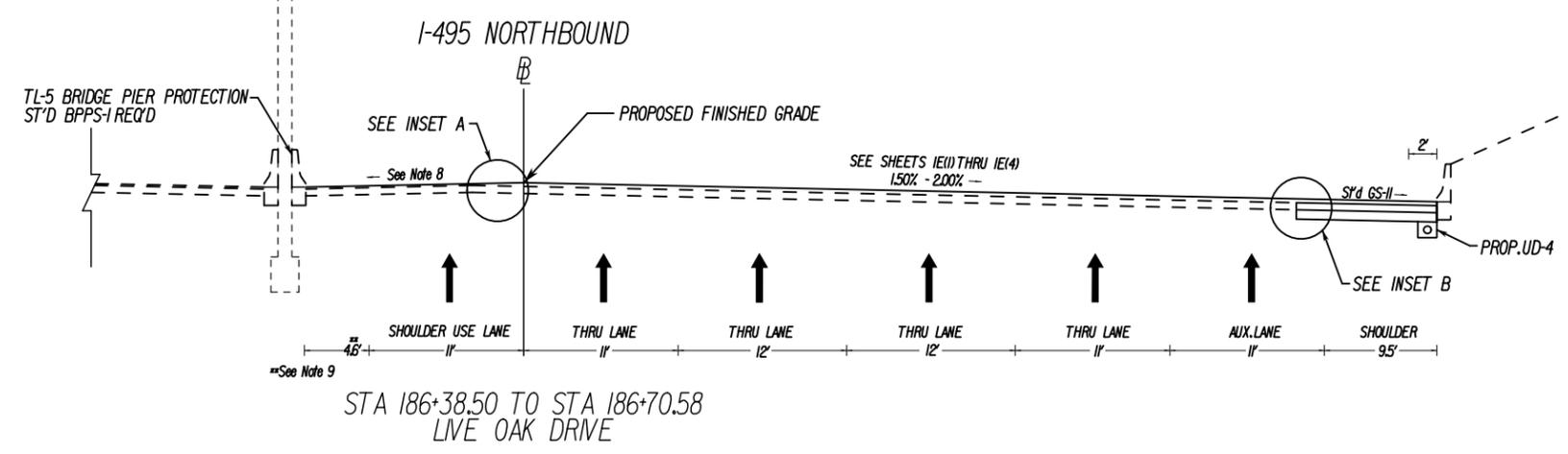
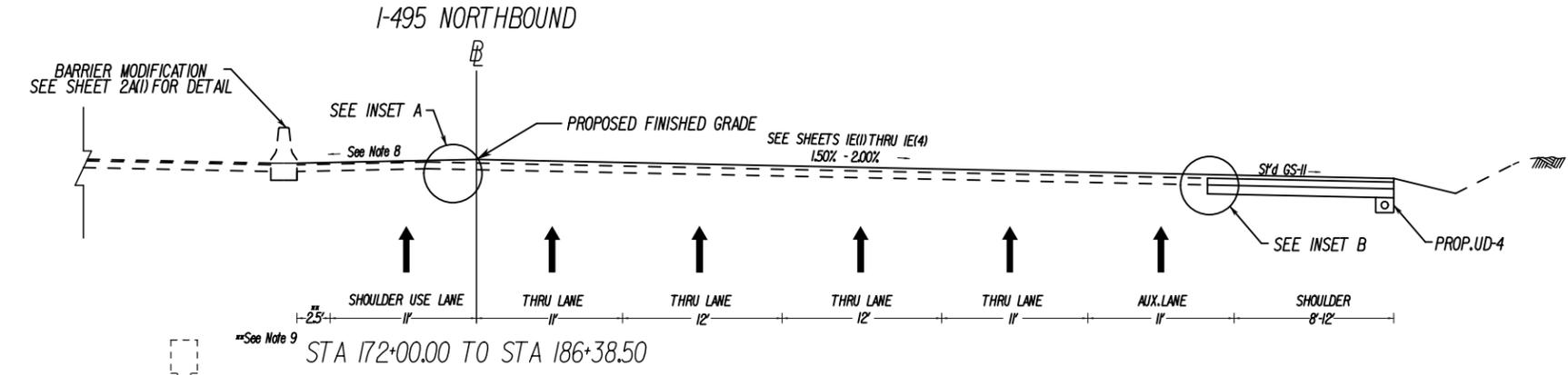
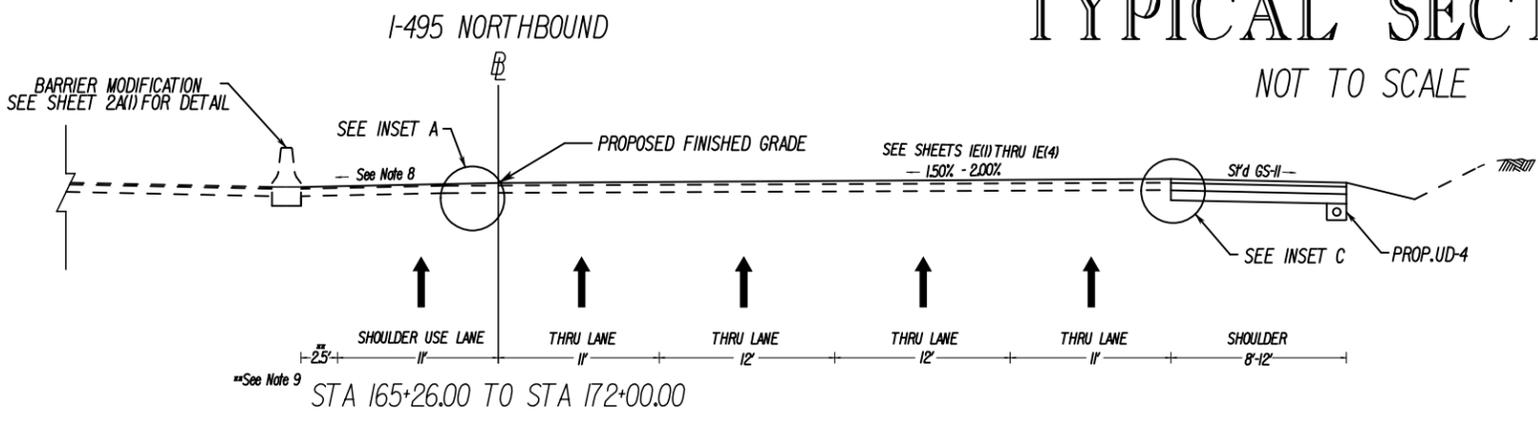


NOT TO SCALE

FULL DEPTH PATCH/ JOINT REPAIR



NOT TO SCALE



NOTES:

1. FOR LIMITS OF MILL AND OVERLAY AND FULL DEPTH PAVEMENT, REFER TO PLAN SHEETS.
2. REFER TO GEOTECHNICAL ENGINEERING DATA REPORT FOR PAVEMENT DESIGN.
3. VARIABLE DEPTH MILL AND OVERLAY MAY BE NECESSARY FOR SLOPE CORRECTION.
4. FULL DEPTH/JOINT REPAIRS ARE TO BE PERFORMED ON THE FULL WIDTH OF PAVEMENT AND SHOULDER PRIOR TO THE PLACEMENT OF FINAL SURFACE LAYERS OF ASPHALT.
5. ALL CRACKS SHALL BE CLEANED OF ALL DEBRIS AFTER MILLING AND SEALED WITH TYPE B CRACK SEALANT IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR SEALING CRACKS IN AC OR PCC PAVEMENT.
6. FINAL SURFACE TO BE PLACED UNIFORMLY ACROSS THE ENTIRE PAVEMENT.
7. WHEN LIQUID ASPHALT IS USED AS A CURING MATERIAL FOR THE CEMENT STABILIZED COURSE, IT SHALL BE LIQUID ASPHALT CRS-1, CRS-1H OR CMS-2 APPLIED AT A RATE OF 0.2 GAL/SY WHERE NECESSARY FOR MAINTENANCE OF TRAFFIC. COVER MATERIAL CONSISTING OF NO. 10 AGGREGATE OR GRADING B SAND SHALL BE APPLIED AT A RATE OF 10 LB/SY.
8. ON HIGH SIDE OF SUPERELEVATION THE SLOPE IS TO MATCH THE SUPERELEVATION WITHOUT A ROLLOVER. ON TANGENT AND LOW SIDE OF SUPERELEVATION THE SLOPE SHALL MATCH EXISTING, BUT MAINTAIN MAXIMUM 5% ROLLOVER.
9. ON TANGENT AND LOW SIDE OF SUPERELEVATION THE SLOPE OF THE LATERAL OFFSET SHALL MATCH THE SHOULDER USE LANE SLOPE. ON THE HIGH SIDE OF SUPERELEVATION THE SLOPE SHALL MAINTAIN 5% ROLLOVER AT THE EDGE OF THE SHOULDER USE LANE.

- ① Surface - 1.5" Stone Matrix Asphalt, SMA-9.5E (PG 76-22)
- ② 2.5" Asphalt Concrete Type BM-25.0D +0.4 (HMHB, PG 70-22)
- ③ Variable Depth Asphalt Concrete Type BM-25.0D +0.4 (HMHB, PG 70-22)
- ④ Base - 14" Asphalt Concrete Type BM-25.0D +0.4 (HMHB, PG 70-22)
- ⑤ Base - 12" Asphalt Concrete Type BM-25.0D +0.4 (HMHB, PG 70-22)
- ⑥ Subbase - 8" Aggregate Base Material, Type I, Size No. 21B
- ⑦ Subbase - 6" Aggregate Base Material, Type I, Size No. 21A pugmill mixed with 4% hydraulic cement by weight
- ⑧ Mill and Overlay Existing Pavement Minimum Depth of 4"
- ⑨ High Friction Surface Coating To Delineate Shoulder Use Lane
- ⑩ Full depth sawcut at edge of existing mainline pavement to expose existing full strength pavement with PCC or expose existing edge of PCC pavement



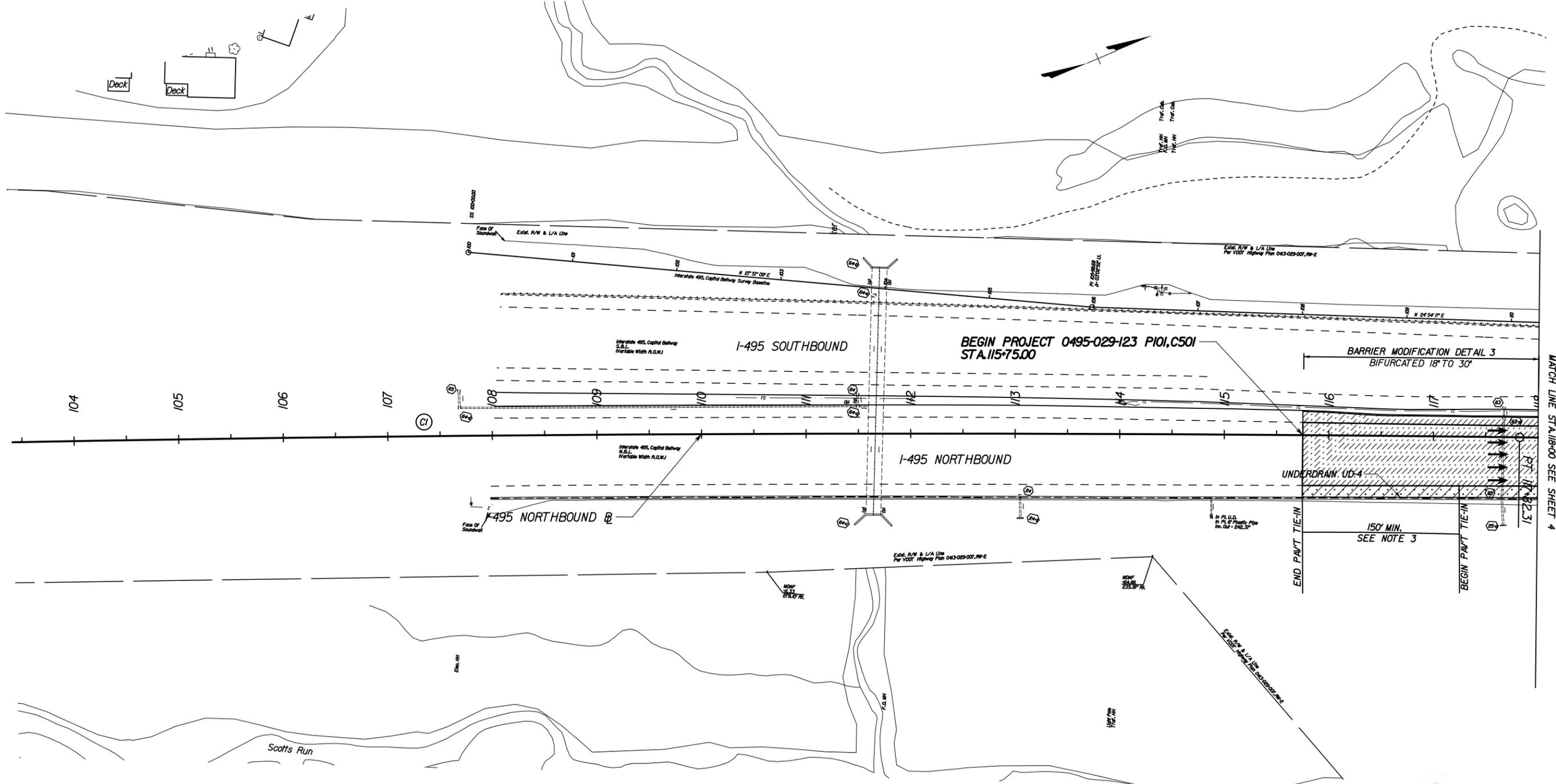
CONCEPT PLANS
MARCH 12, 2014

PROJECT 0495-029-123	SHEET NO. 2A(4)
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PROJECT MANAGER Paul M. Ishimura (571) 483-2622
 SURVEYED BY, DATE VDOT
 DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
 Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE		PROJECT	SHEET NO.
	STATE	ROUTE		
	VA.	495	0495-029-123 C501	3

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



LEGEND

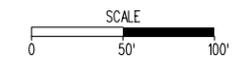
- Denotes Proposed Pavement
- Denotes Resurfacing of Pavement
- Denotes Demolition of Pavement
- Denotes Full Depth Patch/ Joint Repair Location and Joint Repair Number

(C1) Curve NBB001
 PI = 108+91.27
 DELTA = 2° 13' 04.40" (RT)
 D = 0° 07' 28"
 T = 891.27'
 L = 1782.31'
 R = 46,043.27'
 PC = 100+00.00
 PT = 117+82.31
 E = MATCH EXISTING
 V = 70 MPH

- NOTES:**
1. FULL DEPTH PATCHES/JOINT REPAIRS SHALL BE PERFORMED PRIOR TO MILLING AND OVERLAY.
 2. SUPPLEMENTAL SURVEY WAS PERFORMED IN 2005 AND IS SHOWN FOR REFERENCE PURPOSES ONLY.
 3. FOR TRANSVERSE PAVEMENT TIE-IN DETAIL SEE TYPICAL SECTION SHEET 2A(2).
 4. MOVABLE MEDIAN BARRIER SHALL BE PLACED AT A LOCATION APPROVED BY VDOT (SEE SPECIAL PROVISION).



CONCEPT PLANS
 MARCH 12, 2014

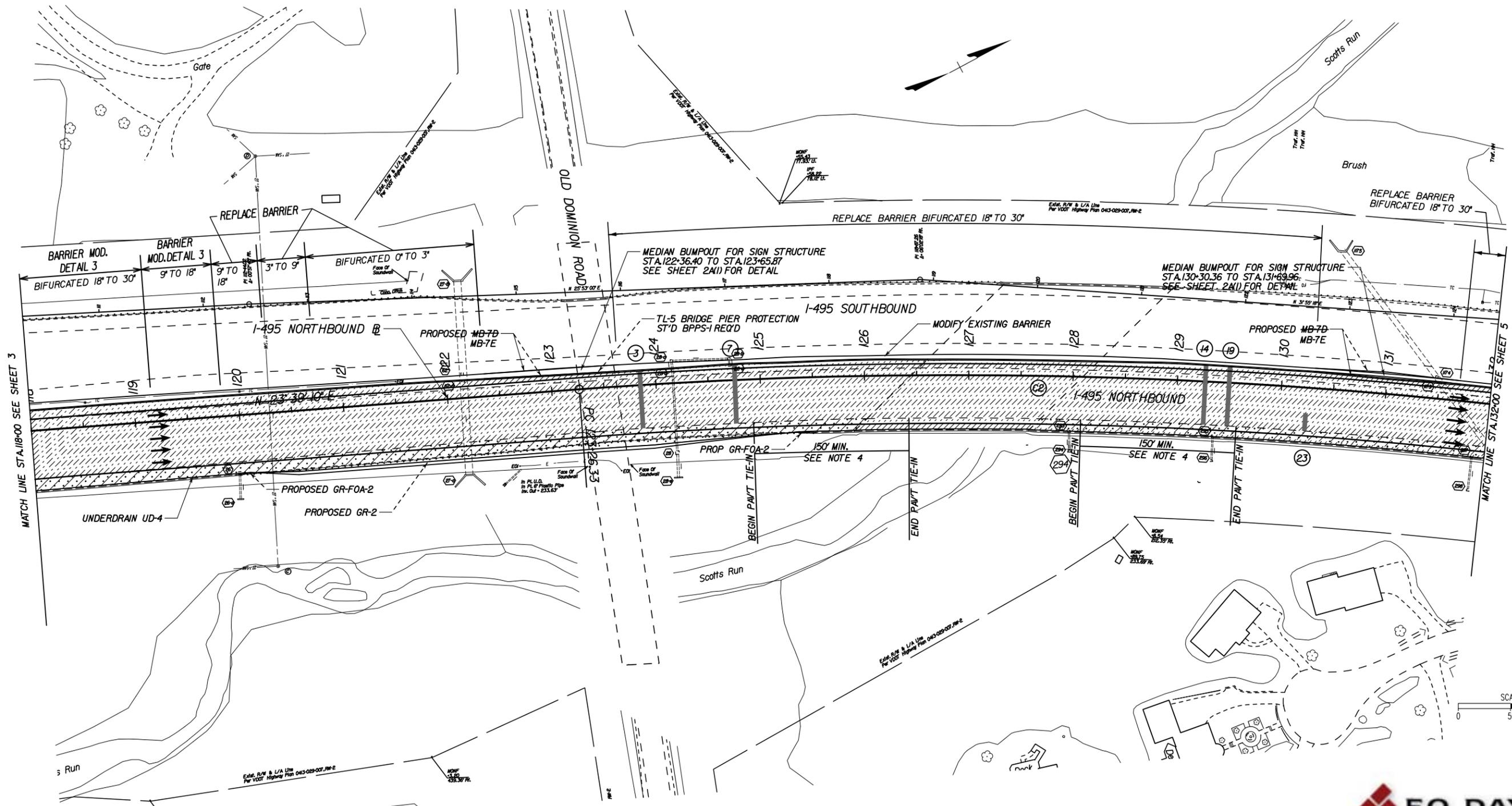


PROJECT	SHEET NO.
0495-029-123	3

PROJECT MANAGER Paul M. Ishimaru (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE		STATE PROJECT	SHEET NO.
	VA.	ROUTE		
	495	0495-029-123	C501	4

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

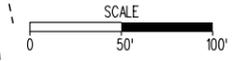


LEGEND

	Denotes Proposed Pavement
	Denotes Resurfacing of Pavement
	Denotes Demolition of Pavement
	Denotes Full Depth Patch/ Joint Repair Location and Joint Repair Number

(C2) Curve NBB002
 PI = 131+87.79
 DELTA = 20° 28' 27.99" (RT)
 D = 12' 0.4"
 T = 861.46'
 L = 1704.54'
 R = 4770.00'
 PC = 123+26.33
 PT = 140+30.87
 E = 4.23%
 V = 70 MPH

- NOTES:**
1. FULL DEPTH PATCHES/JOINT REPAIRS SHALL BE PERFORMED PRIOR TO MILLING AND OVERLAY.
 2. SUPPLEMENTAL SURVEY WAS PERFORMED IN 2005 AND IS SHOWN FOR REFERENCE PURPOSES ONLY.
 3. THE OLD DOMINION BRIDGE SHOWN FROM SUPPLEMENTAL SURVEY.
 4. FOR TRANSVERSE PAVEMENT TIE-IN DETAIL SEE TYPICAL SECTION SHEET 2A(2).
 5. MOVABLE MEDIAN BARRIER SHALL BE PLACE AT A LOCATION APPROVED BY VDOT (SEE SPECIAL PROVISION).



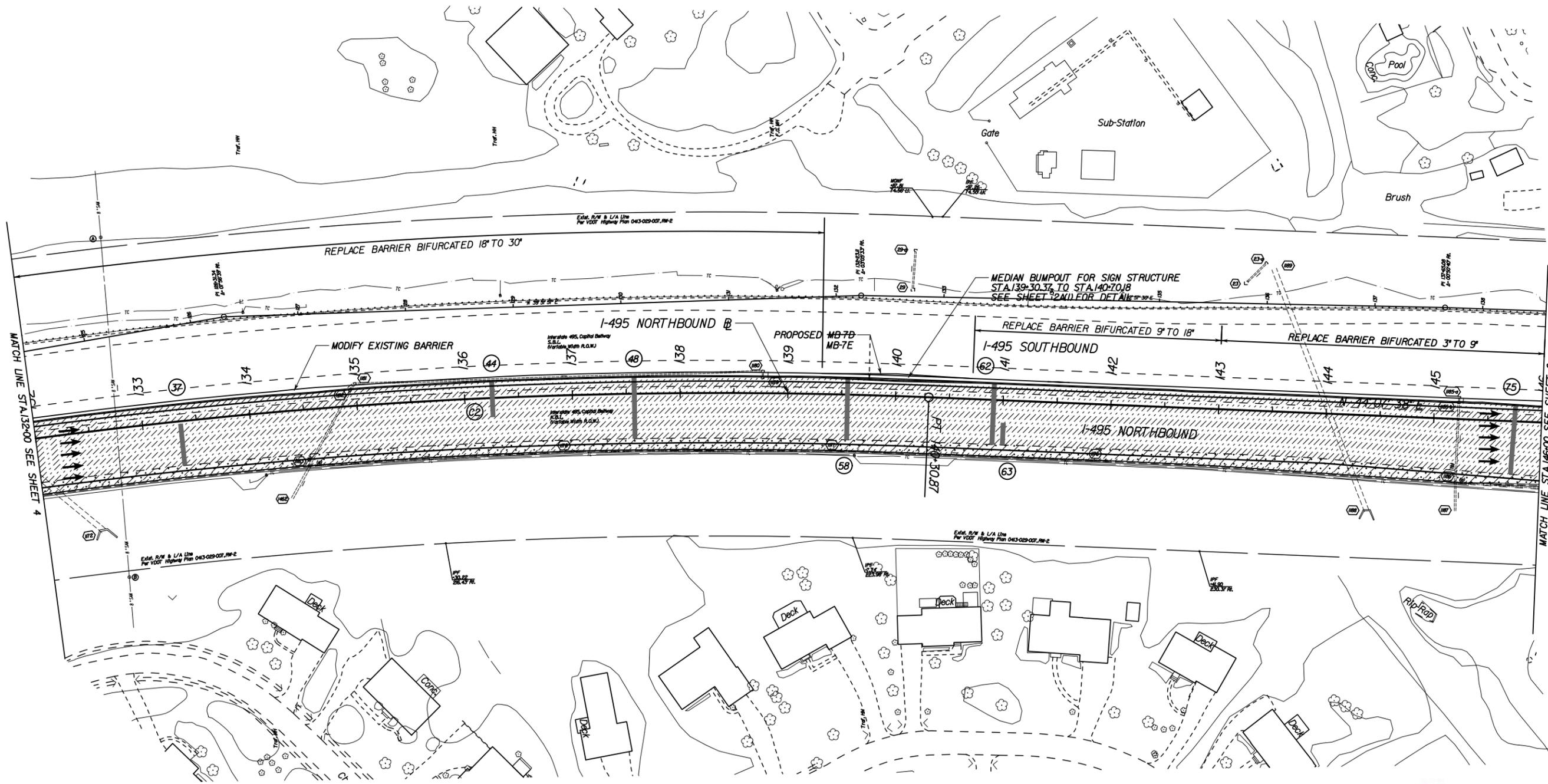
CONCEPT PLANS
MARCH 12, 2014

PROJECT	0495-029-123	SHEET NO.	4
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PROJECT MANAGER Paul M. Ishimura (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE		PROJECT	SHEET NO.
	STATE	ROUTE		
	VA.	495	0495-029-123 C501	5

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



LEGEND

- Denotes Proposed Pavement
- Denotes Resurfacing of Pavement
- Denotes Demolition of Pavement
- Denotes Full Depth Patch/JoInt Repair Location and JoInt Repair Number

(C2) Curve NBB002
 PI = 131+87.79
 DELTA = 20° 28' 27.99" (RT)
 D = 112' 0"
 T = 861.46'
 L = 1704.54'
 R = 4770.00'
 PC = 123+26.33
 PT = 140+30.87
 E = 4.23%
 V = 70 MPH

NOTES:
 1. FULL DEPTH PATCHES/JOINT REPAIRS SHALL BE PERFORMED PRIOR TO MILLING AND OVERLAY.
 2. SUPPLEMENTAL SURVEY WAS PERFORMED IN 2005 AND IS SHOWN FOR REFERENCE PURPOSES ONLY.
 3. MOVABLE MEDIAN BARRIER SHALL BE PLACED AT A LOCATION APPROVED BY VDOT (SEE SPECIAL PROVISION).

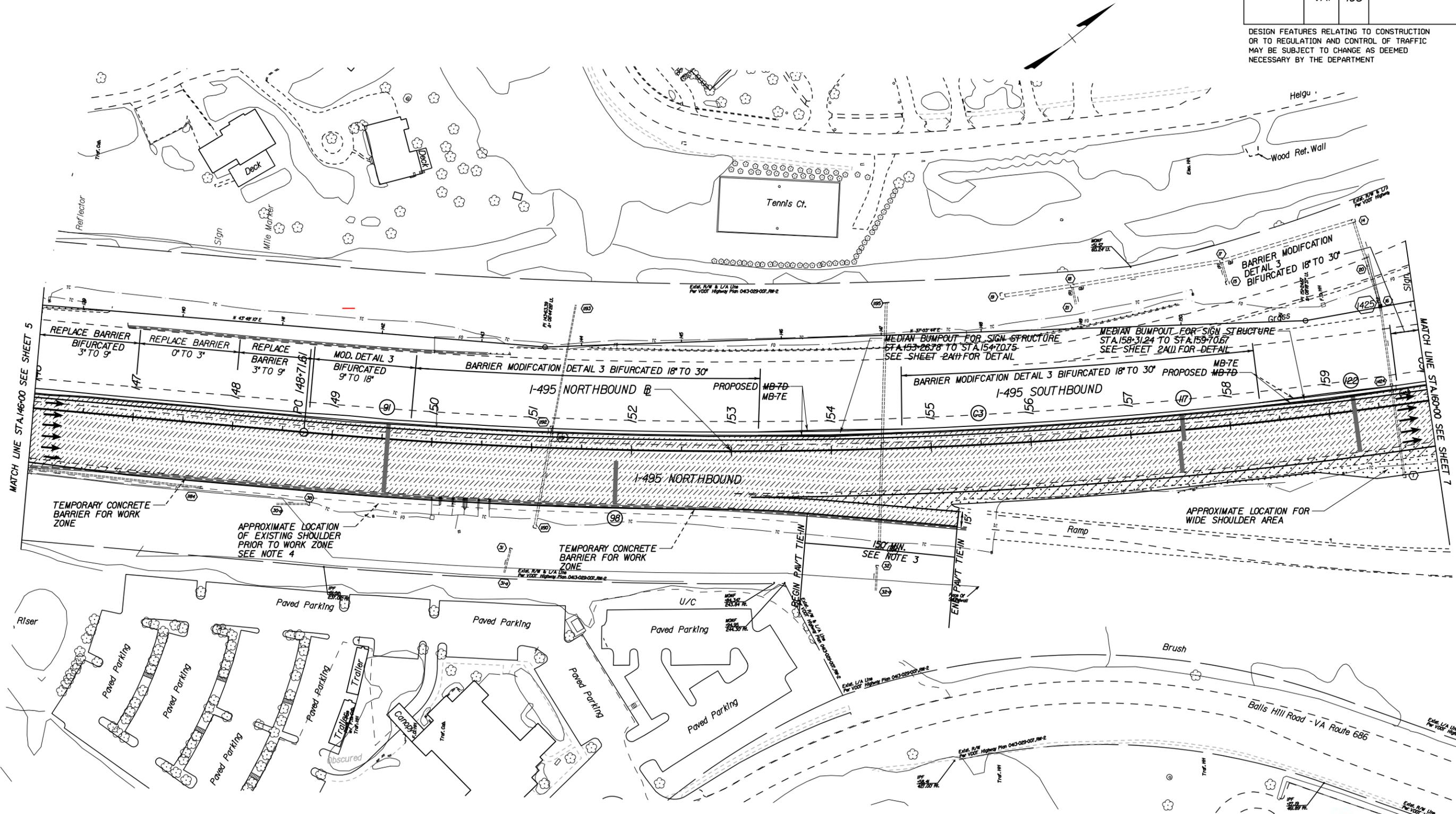


CONCEPT PLANS
MARCH 12, 2014

PROJECT 0495-029-123	SHEET NO. 5
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REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	495	0495-029-123 C501	6

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

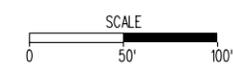


LEGEND

	Denotes Proposed Pavement
	Denotes Resurfacing of Pavement
	Denotes Demolition of Pavement
	Denotes Full Depth Patch/ Joint Repair Location and Joint Repair Number

C3 Curve NBB003
 PI = 160+34.88
 DELTA = 27° 04' 57.77" (LT)
 D = 111' 10"
 T = 1163.27'
 L = 2283.06'
 R = 4830.00'
 PC = 148+71.61
 PT = 171+54.66
 E = 4.18%
 V = 70 MPH

- NOTES:**
1. FULL DEPTH PATCHES/JOINT REPAIRS SHALL BE PERFORMED PRIOR TO MILLING AND OVERLAY.
 2. SUPPLEMENTAL SURVEY WAS PERFORMED IN 2005 AND IS SHOWN FOR REFERENCE PURPOSES ONLY.
 3. FOR TRANSVERSE PAVEMENT TIE-IN DETAIL SEE TYPICAL SECTION SHEET 2A(2).
 4. FINAL FULL DEPTH SHOULDER WIDTH TO BE DETERMINED AT TIME OF CONSTRUCTION.
 5. MOVABLE MEDIAN BARRIER SHALL BE PLACED AT A LOCATION APPROVED BY VDOT (SEE SPECIAL PROVISION).



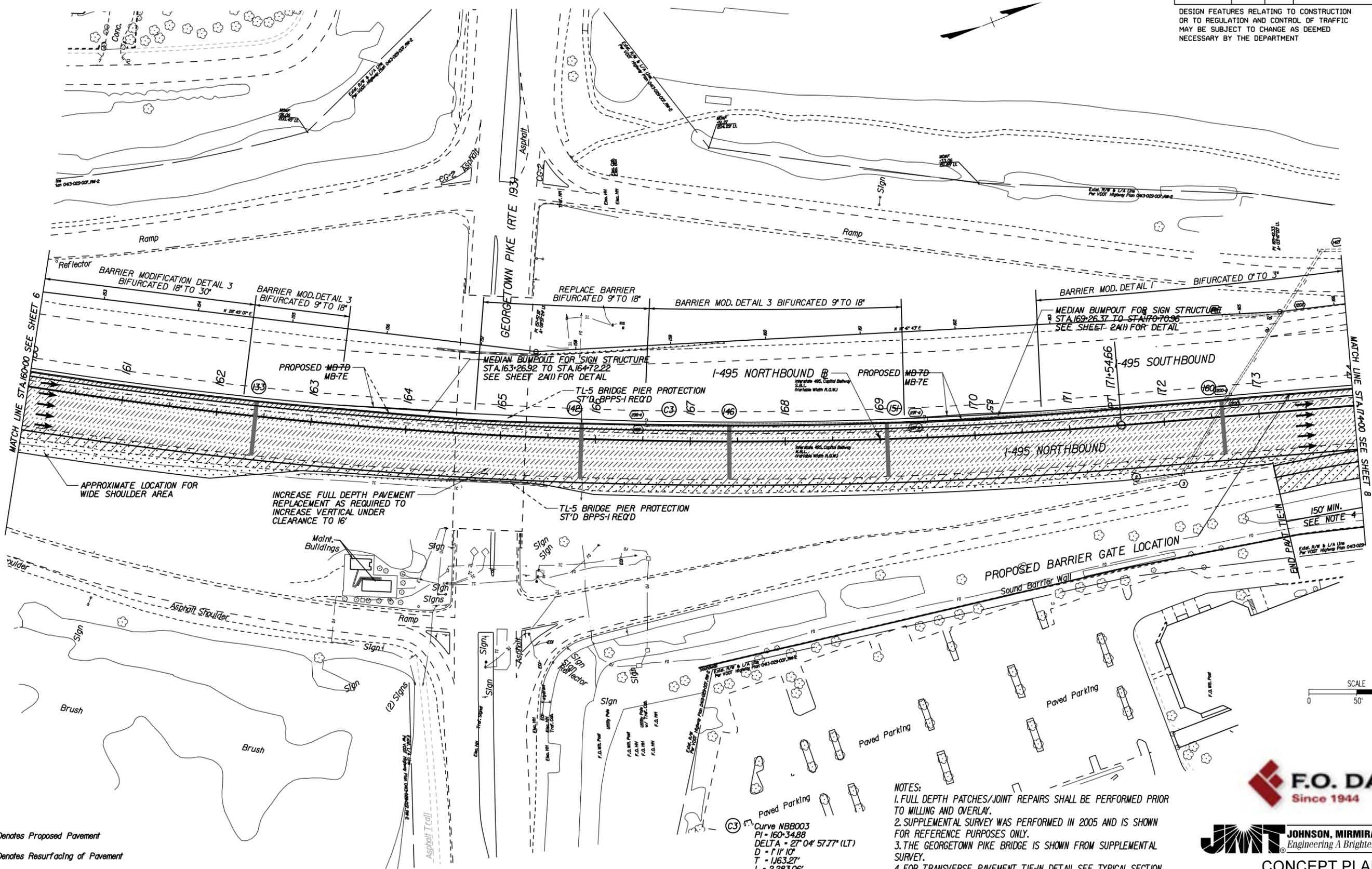
CONCEPT PLANS
 MARCH 12, 2014

PROJECT	SHEET NO.
0495-029-123	6

PROJECT MANAGER Paul M. Ishimura (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE		STATE PROJECT	SHEET NO.
	VA.	ROUTE		
		495	0495-029-123 C501	7

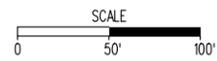
DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



- LEGEND**
- Denotes Proposed Pavement
 - Denotes Resurfacing of Pavement
 - Denotes Demolition of Pavement
 - Denotes Full Depth Patch/ Joint Repair Location and Joint Repair Number

Curve NBB003
 PI = 160+34.88
 DELTA = 27° 04' 57.77" (LT)
 D = 111' 10"
 T = 1163.27'
 L = 2283.06'
 R = 4830.00'
 PC = 148+71.61
 PT = 171+54.66
 E = 4.18%
 V = 70 MPH

- NOTES:**
1. FULL DEPTH PATCHES/Joint REPAIRS SHALL BE PERFORMED PRIOR TO MILLING AND OVERLAY.
 2. SUPPLEMENTAL SURVEY WAS PERFORMED IN 2005 AND IS SHOWN FOR REFERENCE PURPOSES ONLY.
 3. THE GEORGETOWN PIKE BRIDGE IS SHOWN FROM SUPPLEMENTAL SURVEY.
 4. FOR TRANSVERSE PAVEMENT TIE-IN DETAIL SEE TYPICAL SECTION SHEET 2A2.
 5. MOVABLE MEDIAN BARRIER SHALL BE PLACED AT A LOCATION APPROVED BY VDOT (SEE SPECIAL PROVISION).



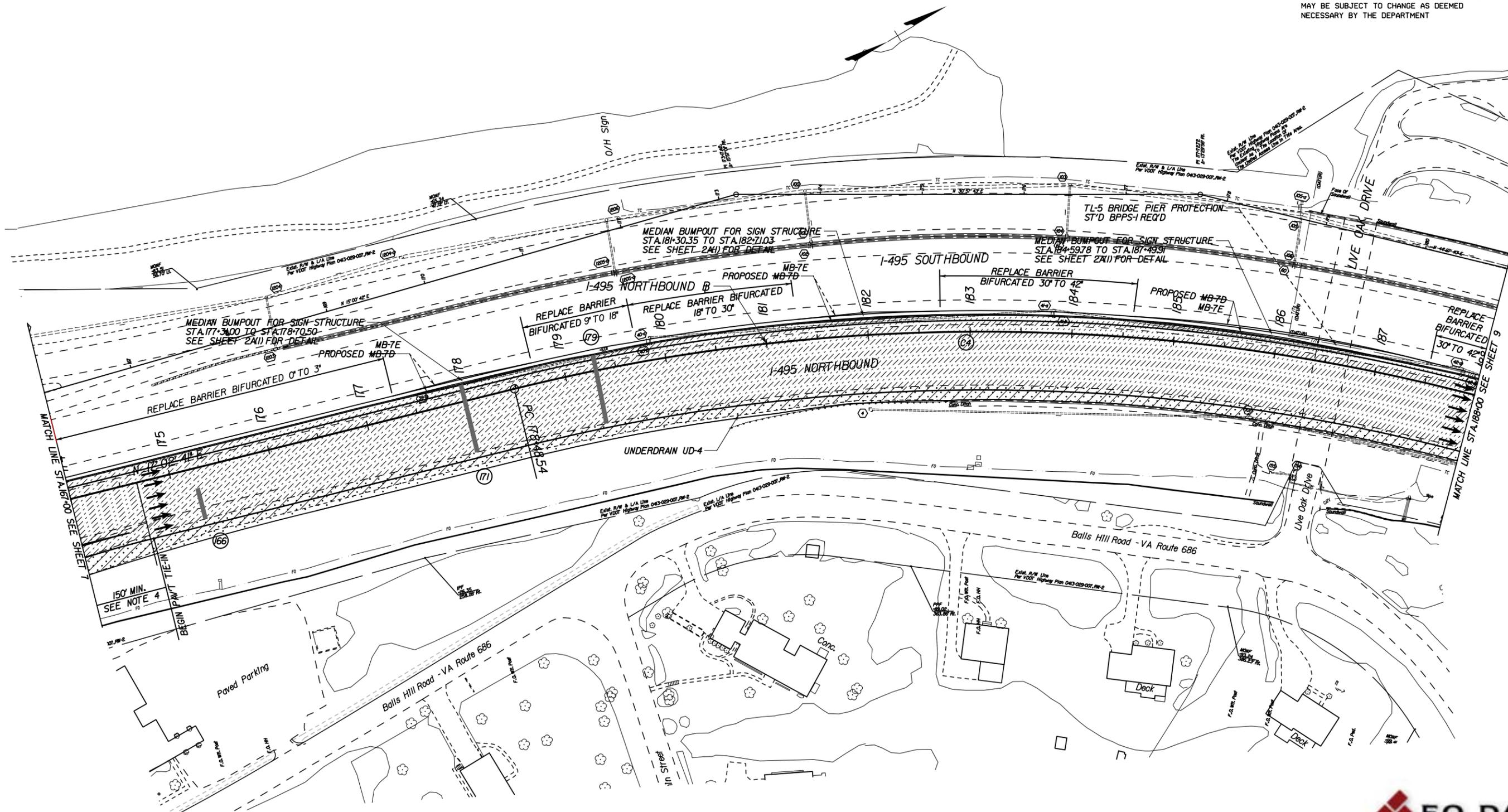
CONCEPT PLANS
MARCH 12, 2014

PROJECT	SHEET NO.
0495-029-123	7

PROJECT MANAGER Paul M. Ishimaru (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE		PROJECT	SHEET NO.
	ROUTE	PROJECT		
	VA.	495	0495-029-123 C501	8

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



LEGEND

- Denotes Proposed Pavement
- Denotes Resurfacing of Pavement
- Denotes Demolition of Pavement
- Denotes Full Depth Patch/ Joint Repair Location and Joint Repair Number

(C4) Curve NBB004
 PI = 186+04.22
 DELTA = 43° 10' 17.84" (RT)
 D = 2° 59' 59"
 T = 755.68'
 L = 1,439.16'
 R = 1,910.00'
 PC = 178+48.54
 PT = 192+87.70
 E = 7.95%
 V = 70 MPH

NOTES:
 1. FULL DEPTH PATCHES/JOINT REPAIRS SHALL BE PERFORMED PRIOR TO MILLING AND OVERLAY.
 2. SUPPLEMENTAL SURVEY WAS PERFORMED IN 2005 AND IS SHOWN FOR REFERENCE PURPOSES ONLY.
 3. MOVABLE MEDIAN BARRIER SHALL BE PLACED AT A LOCATION APPROVED BY VDOT (SEE SPECIAL PROVISION).



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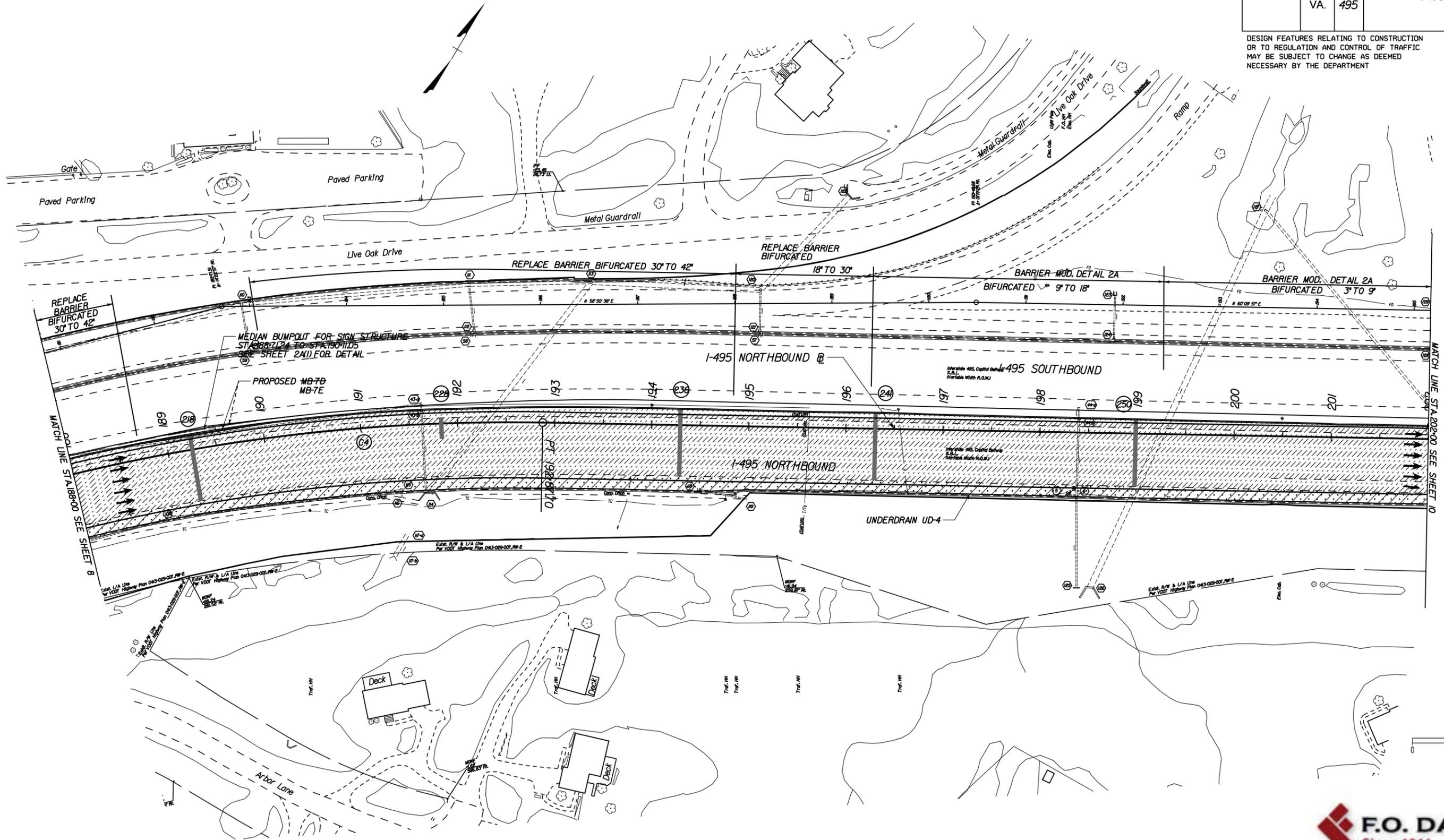
CONCEPT PLANS
MARCH 12, 2014

PROJECT	SHEET NO.
0495-029-123	8

PROJECT MANAGER Paul M. Ishimura (571) 483-2622
 SURVEYED BY, DATE VDOT
 DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
 Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	495	0495-029-123 C501	9

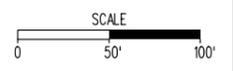
DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



LEGEND

- Denotes Proposed Pavement
- Denotes Resurfacing of Pavement
- Denotes Demolition of Pavement
- Denotes Full Depth Patch/ Joint Repair Location and Joint Repair Number
- Curve NBB004
 PI = 186+04.22
 DELTA = 43° 10' 17.8" (RT)
 D = 2' 59' 59"
 T = 755.68'
 L = 1,439.16'
 R = 1,910.00'
 PC = 178+48.54
 PT = 192+87.70
 E = 7.95%
 V = 70 MPH

NOTES:
 1. FULL DEPTH PATCHES/JOINT REPAIRS SHALL BE PERFORMED PRIOR TO MILLING AND OVERLAY.
 2. SUPPLEMENTAL SURVEY WAS PERFORMED IN 2005 AND IS SHOWN FOR REFERENCE PURPOSES ONLY.
 3. MOVABLE MEDIAN BARRIER SHALL BE PLACED AT A LOCATION APPROVED BY VDOT (SEE SPECIAL PROVISION).



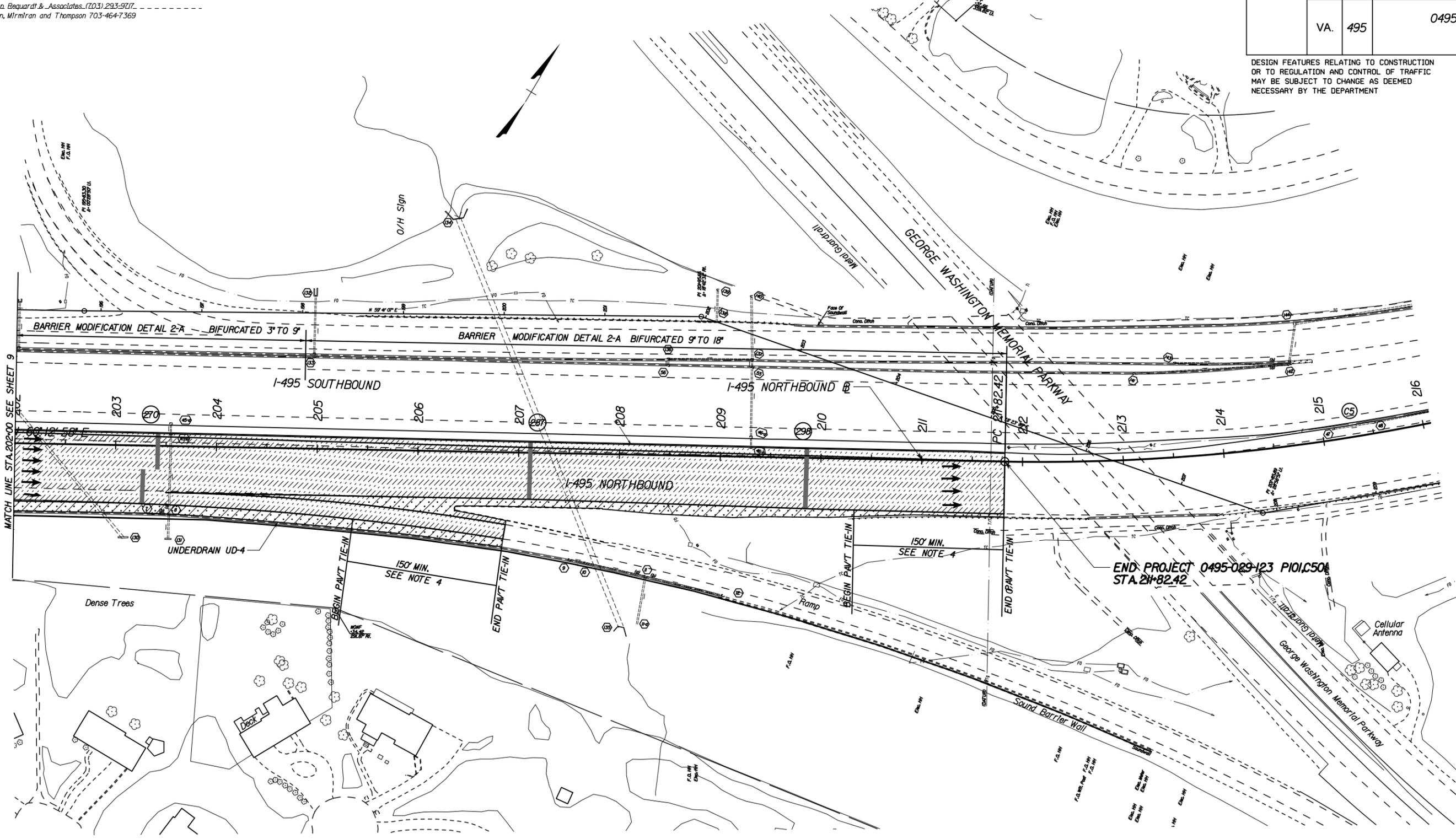
CONCEPT PLANS
 MARCH 12, 2014

PROJECT 0495-029-123	SHEET NO. 9
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PROJECT MANAGER Paul M. Ishimaru (571) 483-2622
SURVEYED BY DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	495	0495-029-123 C501	10

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

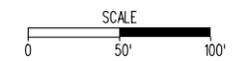


LEGEND

- Denotes Proposed Pavement
- Denotes Resurfacing of Pavement
- Denotes Demolition of Pavement
- Denotes Full Depth Patch/JoInt Repair Location and JoInt Repair Number

(C5) Curve NBB005
 PI = 221+05.40
 DELTA = 50° 39' 31.36" (LT)
 D = 2' 56" 18"
 T = 922.98'
 L = 1724.11'
 R = 1950.00'
 PC = 211+82.42
 PT = 229+06.54
 E = MATCH EXISTING
 V = 70 MPH

- NOTES:**
1. FULL DEPTH PATCHES/JOINT REPAIRS SHALL BE PERFORMED PRIOR TO MILLING AND OVERLAY.
 2. SUPPLEMENTAL SURVEY WAS PERFORMED IN 2005 AND IS SHOWN FOR REFERENCE PURPOSES ONLY.
 3. THE GEORGE WASHINGTON MEMORIAL PARKWAY BRIDGE IS SHOWN FROM SUPPLEMENTAL SURVEY.
 4. FOR TRANSVERSE PAVEMENT TIE-IN DETAIL SEE TYPICAL SECTION SHEET 2A2.
 5. MOVABLE MEDIAN BARRIER SHALL BE PLACED AT A LOCATION APPROVED BY VDOT (SEE SPECIAL PROVISION).



CONCEPT PLANS
MARCH 12, 2014

PROJECT 0495-029-123	SHEET NO. 10
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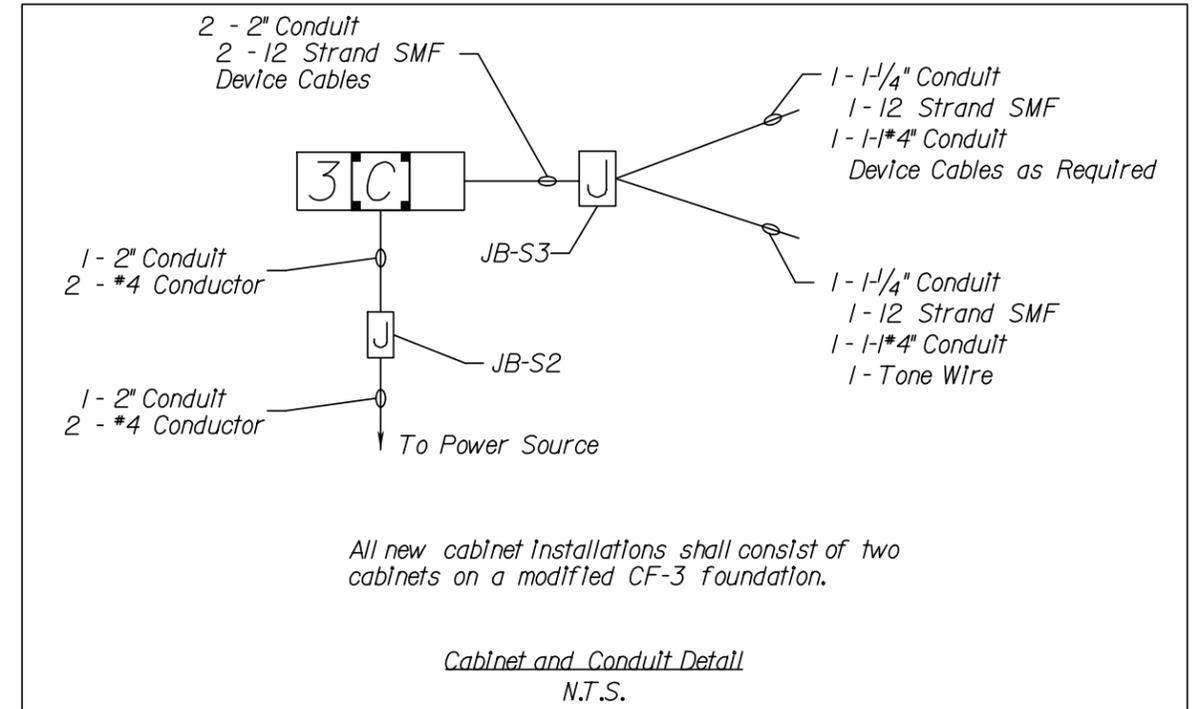
CONDUIT AND CABLE SCHEDULE

REVISED	STATE	STATE PROJECT		SHEET NO.
		ROUTE	PROJECT	
	VA.	495	0495-029-123 C501	12(0)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

All conduit shall be trenched unless otherwise noted.

- A** 1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
1 - 2" Conduit (Power)
4 - #4 Cables
- B** 1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
1 - 2" Conduit (Power)
2 - #4 Cables
- C** Mounted on Structure
1 - 1 1/4" Metal Conduit (ITS)
1 - 12 Strand SMF
1 - 1 1/4" Metal Conduit (Empty)
1 - 1 1/2" Metal Conduit (ITS Spare)
2 - #4 Cables
- D** 1 - 2" Conduit (Power)
2 - #4 Cables
- E** BORED CONDUIT
1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
1 - 2" Conduit (Power)
4 - #4 Cables
- F** 1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
1 - 2" Conduit (Power)
6 - #4 Cables
- G** Bored Conduit
1 - 2" Conduit (Power)
2 - #4 Cables
- H** Bored Conduit
1 - 1 1/4" Conduit (ITS)
1 - DMS Cable
1 - CCTV Cable
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
- I** Bored Conduit
1 - 1 1/4" Conduit (ITS)
2 - CCTV Cables
1 - LCS Cable
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
- J** Bored Conduit
1 - 1 1/4" Conduit (ITS)
2 - CCTV Cables
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
- K** 1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
1 - 2" Conduit (Power)
8 - #4 Cables
- L** Bored Conduit
1 - 1 1/4" Conduit (ITS)
1 - LCS Cable
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
- M** Mounted on Structure
2 - 1 1/4" Metal Conduit (ITS)
1 - CCTV Cable
- N** 1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
1 - LCS Cable
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
- O** Mounted on Structure
2 - 1 1/4" Metal Conduit (ITS)
2 - CCTV Cables
- P** 1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
2 - CCTV Cables
1 - LCS Cable
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
- R** 1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
1 - Radar Detector Cable
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
1 - 2" Conduit (Power - Empty)
- S** 1 - 1 1/4" Conduit (ITS)
2 - CCTV Cables
1 - LCS Cable
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
1 - 2" Conduit (Power)
4 - #4 Cables



- T** 1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
2 - CCTV Cables
1 - Radar Detector Cable
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
1 - 2" Conduit
4 - #4 Cables
- U** 1 - 1 1/4" Conduit (ITS)
1 - DMS Cable
1 - CCTV Cable
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
- W** 1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
- X** Bored Conduit
1 - 1 1/4" Conduit (ITS)
1 - 12 Strand SMF
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire
1 - 2" Conduit (Power)
2 - #4 Cables
- Y** 1 - 1 1/4" Conduit (ITS)
2 - CCTV Cables
1 - 1 1/4" Conduit (ITS Spare)
1 - Tone Wire



CONCEPT PLANS
MARCH 12, 2014

PROJECT 0495-029-123	SHEET NO. 12(0)
-------------------------	--------------------

PROJECT MANAGER Paul M. Ishimura (571) 483-2622
 SURVEYED BY, DATE VDOT
 DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
 Johnson, Mirmiran and Thompson 703-464-7369
 SIGN/ITS NOTES:

CONDUIT AND CABLE SCHEDULE

REVISED	STATE	STATE		SHEET NO.
		ROUTE	PROJECT	
	VA.	495	0495-029-123 C501	12(0A)

DESIGN FEATURES RELATING TO CONSTRUCTION
 OR TO REGULATION AND CONTROL OF TRAFFIC
 MAY BE SUBJECT TO CHANGE AS DEEMED
 NECESSARY BY THE DEPARTMENT

All conduit shall be trenched unless otherwise noted.

◇Z 1 - 1 1/4" Conduit (ITS)
 2 - CCTV Cables
 1 - DMS Cable
 1 - 1 1/4" Conduit (ITS Spare)
 1 - Tone Wire
 1 - 2" Conduit (Power)
 2 - #4 Cables

◇AA 1 - 2" Conduit (Power)
 8 - #4 Cables

◇BB Bored Conduit
 1 - 1 1/4" Conduit (ITS)
 1 - DMS Cable
 1 - 1 1/4" Conduit (ITS Spare)
 1 - Tone Wire
 1 - 2" Conduit (Power)
 2 - #4 Cables

◇CC 1 - 1 1/4" Conduit (ITS)
 2 - 12 Strand SMF
 1 - 1 1/4" Conduit (ITS Spare)
 1 - Tone Wire

◇DD 1 - 1 1/4" Conduit (ITS)
 1 - 12 Strand SMF
 1 - 1 1/4" Conduit (ITS)
 1 - CCTV Cable
 1 - 1 1/4" Conduit (ITS Spare)
 1 - Tone Wire

◇EE 1 - 1 1/4" Conduit (ITS)
 1 - 12 Strand SMF
 1 - 1 1/4" Conduit (ITS Spare)
 1 - CCTV Cable
 1 - 2" Conduit (Power)
 2 - #4 Cables

◇FF 1 - 1 1/4" Conduit (ITS)
 1 - 12 Strand SMF
 2 - CCTV Cables
 1 - 1 1/4" Conduit (ITS Spare)
 1 - Tone Wire
 1 - 2" Conduit (Power)
 2 - #4 Cables

◇GG Empty Existing Conduit
 1 - 12 Strand SMF

◇HH Bored Conduit
 1 - 1 1/4" Conduit (ITS)
 1 - 12 Strand SMF
 1 - 1 1/4" Conduit (ITS Spare)
 1 - Tone Wire
 1 - 2" Conduit (Power)
 8 - #4 Cables

◇II 1 - 1 1/4" Conduit (ITS)
 1 - 12 Strand SMF
 1 - LCS Cable
 1 - 1 1/4" Conduit (ITS Spare)
 1 - Tone Wire
 1 - 2" Conduit (Power)
 4 - #4 Cables

◇JJ 1 - 1 1/4" Conduit (ITS)
 1 - CCTV Cables

◇KK 1 - 1 1/4" Conduit (ITS)
 1 - 12 Strand SMF
 1 - DMS Cable
 1 - CCTV Cable
 1 - 1 1/4" Conduit (ITS Spare)
 1 - Tone Wire
 1 - 2" Conduit (Power)
 4 - #4 Cables

◇LL Mounted on Structure
 1 - 1 1/4" Metal Conduit (ITS)
 2 - CCTV Cables
 1 - 1 1/4" Metal Conduit (Empty)



CONCEPT PLANS
 MARCH 12, 2014

PROJECT
 0495-029-123

SHEET NO.
 12(0A)

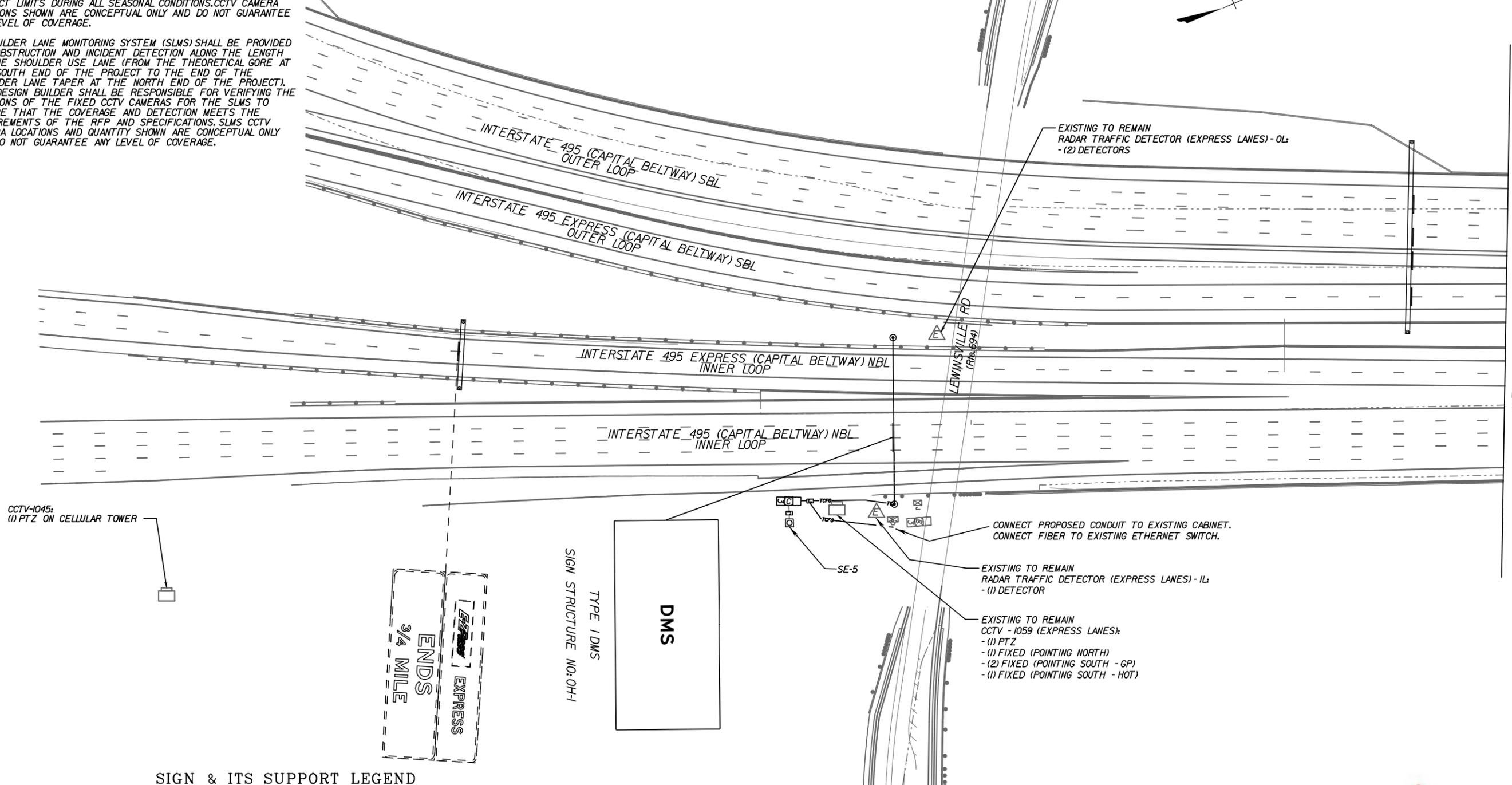
PROJECT MANAGER: Paul M. Ishikawa (571) 483-2622
 SURVEYED BY, DATE: VDOT
 DESIGN BY: Whitman, Bequardt & Associates (703) 293-9717
 Johnson, Mirmiran and Thompson 703-464-7369

1. PROPOSED SIGN/ITS STRUCTURES AND LOCATIONS ARE CONCEPTUAL. THE DESIGN BUILDER SHALL REFER TO THE RFP TECHNICAL INFORMATION AND REQUIREMENTS (PART 2 OF THE RFP) FOR ALL SIGNING AND ITS REQUIREMENTS.
2. THE DESIGN BUILDER SHALL BE RESPONSIBLE FOR VERIFYING 100% CCTV CAMERA VIEWING COVERAGE OF I-495 NB AND SB TRAFFIC AND ALL LANE USE CONTROL SIGNALS AND DMSs WITHIN THE PROJECT LIMITS DURING ALL SEASONAL CONDITIONS. CCTV CAMERA LOCATIONS SHOWN ARE CONCEPTUAL ONLY AND DO NOT GUARANTEE ANY LEVEL OF COVERAGE.
3. A SHOULDER LANE MONITORING SYSTEM (SLMS) SHALL BE PROVIDED FOR OBSTRUCTION AND INCIDENT DETECTION ALONG THE LENGTH OF THE SHOULDER USE LANE (FROM THE THEORETICAL GORE AT THE SOUTH END OF THE PROJECT TO THE END OF THE SHOULDER LANE TAPER AT THE NORTH END OF THE PROJECT). THE DESIGN BUILDER SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATIONS OF THE FIXED CCTV CAMERAS FOR THE SLMS TO ENSURE THAT THE COVERAGE AND DETECTION MEETS THE REQUIREMENTS OF THE RFP AND SPECIFICATIONS. SLMS CCTV CAMERA LOCATIONS AND QUANTITY SHOWN ARE CONCEPTUAL ONLY AND DO NOT GUARANTEE ANY LEVEL OF COVERAGE.

SIGNING AND ITS COMMUNICATION CONTROL PLANS

REVISED	STATE		STATE PROJECT		SHEET NO.
	VA.	ROUTE	0495-029-123	C501	
					12(1)

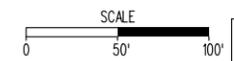
DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



SIGN & ITS SUPPORT LEGEND

SUPPORTS	SYMBOL		SIGNS
	PROPOSED	EXISTING	
Single Post Sign/ITS Support			Existing Sign to Remain or to be Relocated
Double Post Sign/ITS Support			Existing Sign to be Removed
O/H Cantilever Sign/ITS Support			Proposed Sign Panel
O/H Span Sign/ITS Support			Proposed Shoulder Lane Control Signal (LCS)
O/H Butterfly Sign/ITS Support			

ITS LEGEND	PLAN SYMBOL	
	PROPOSED	EXISTING
TRAFFIC DETECTOR SITE (EXPRESS LANES OR VDOT OWNED)		
CCTV CAMERA SITE		
FIXED CCTV CAMERA FOR SHOULDER LANE MONITORING SYSTEM		



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MARCH 12, 2014

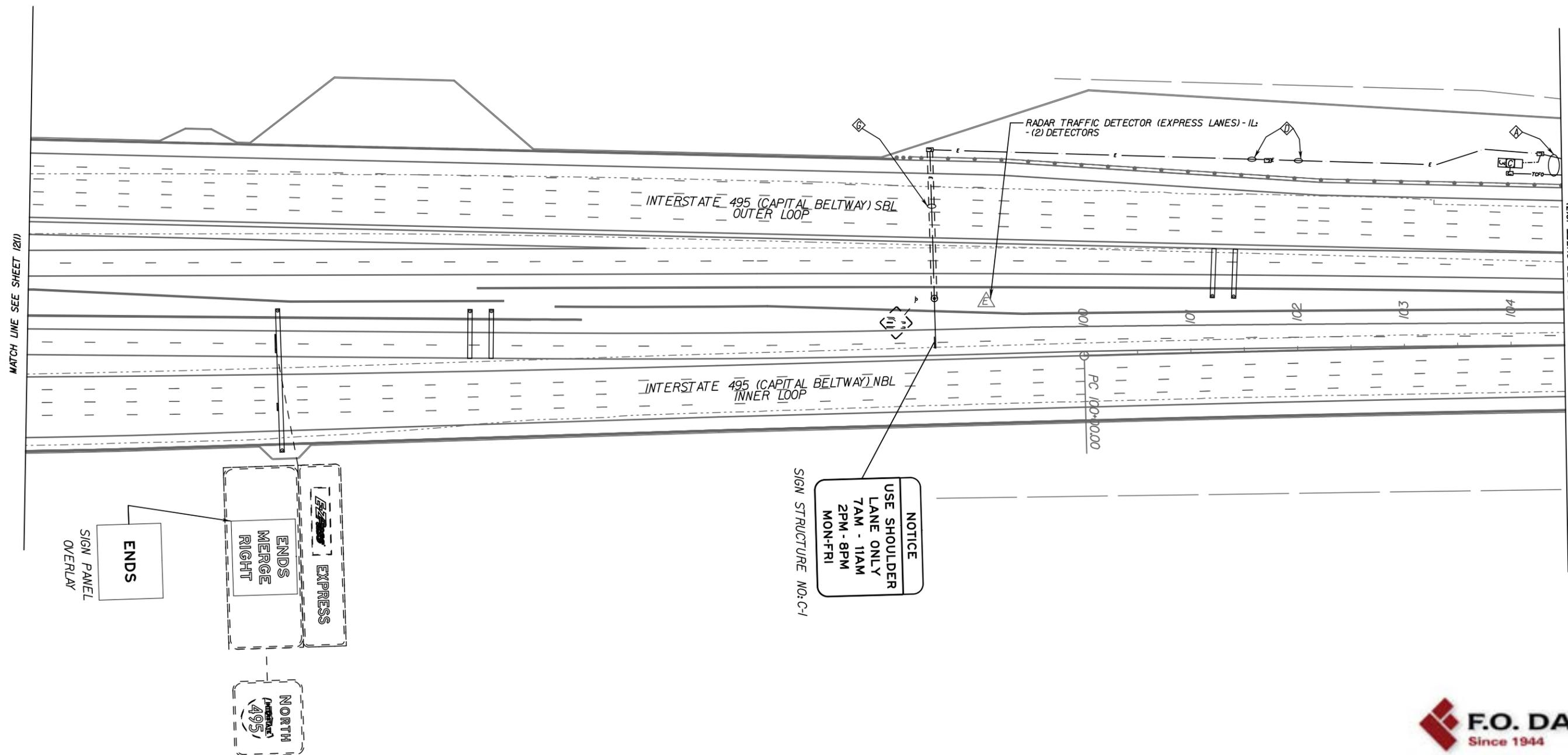
PROJECT	SHEET NO.
0495-029-123	12(1)

PROJECT MANAGER Paul Mirmiran (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

SIGNING AND ITS COMMUNICATION CONTROL PLANS

REVISED	STATE		SHEET NO.
	STATE	ROUTE	
	VA.	495	12(2)
PROJECT			
0495-029-123 C501			

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



CONCEPT PLANS
MARCH 12, 2014

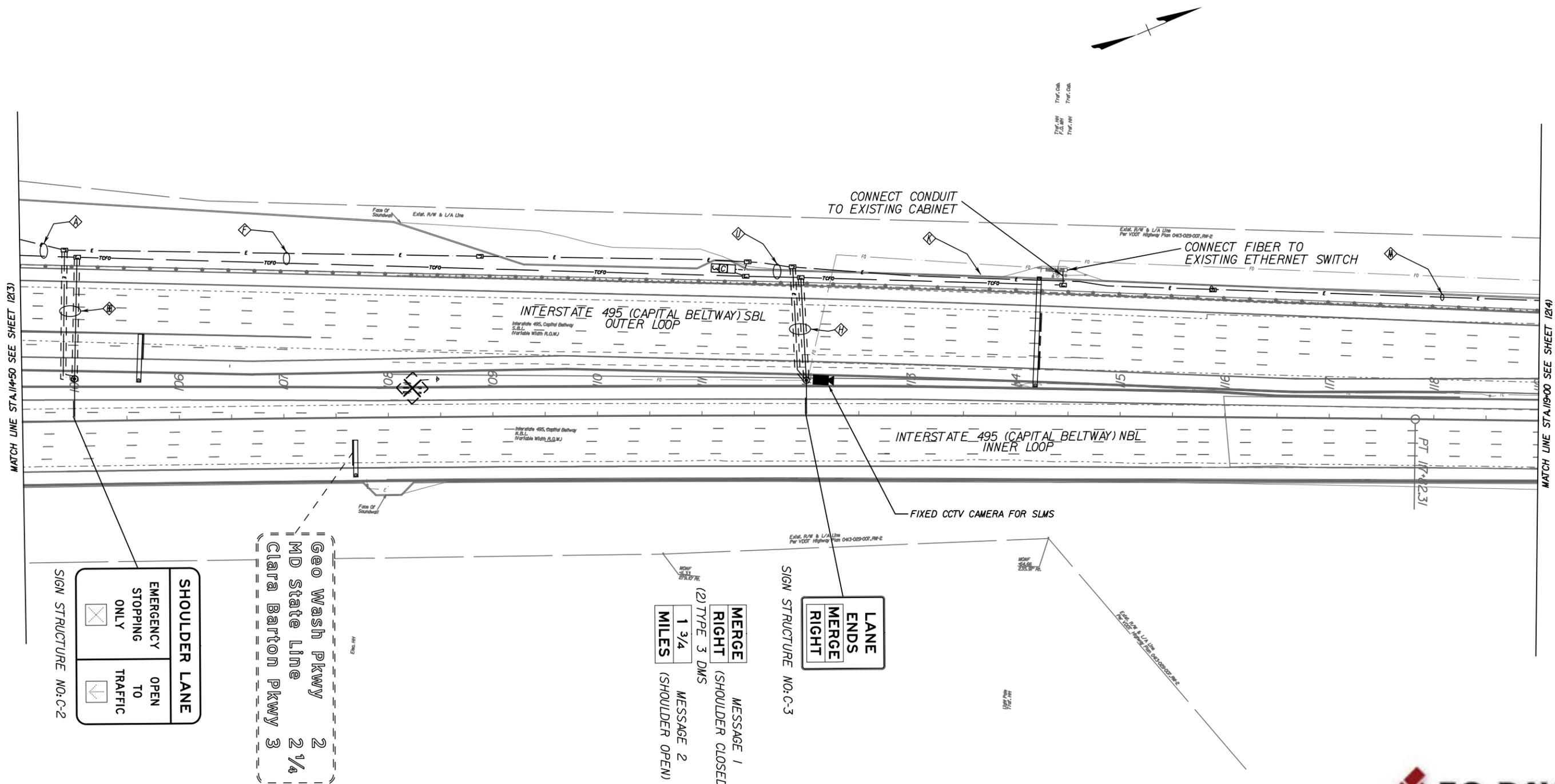
PROJECT	SHEET NO.
0495-029-123	12(2)

PROJECT MANAGER Paul M. Ishikawa (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

SIGNING AND ITS COMMUNICATION CONTROL PLANS

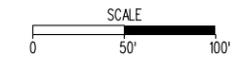
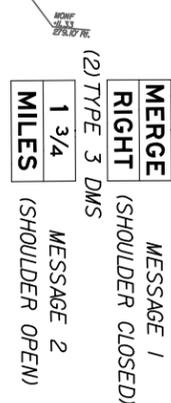
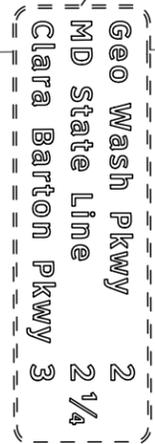
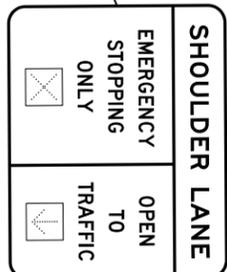
REVISED	STATE	STATE PROJECT		SHEET NO.
	ROUTE	PROJECT		
	VA.	495	0495-029-123 C501	12(3)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



MATCH LINE STA. 114+50 SEE SHEET 12(3)

MATCH LINE STA. 119+00 SEE SHEET 12(4)



CONCEPT PLANS
MARCH 12, 2014

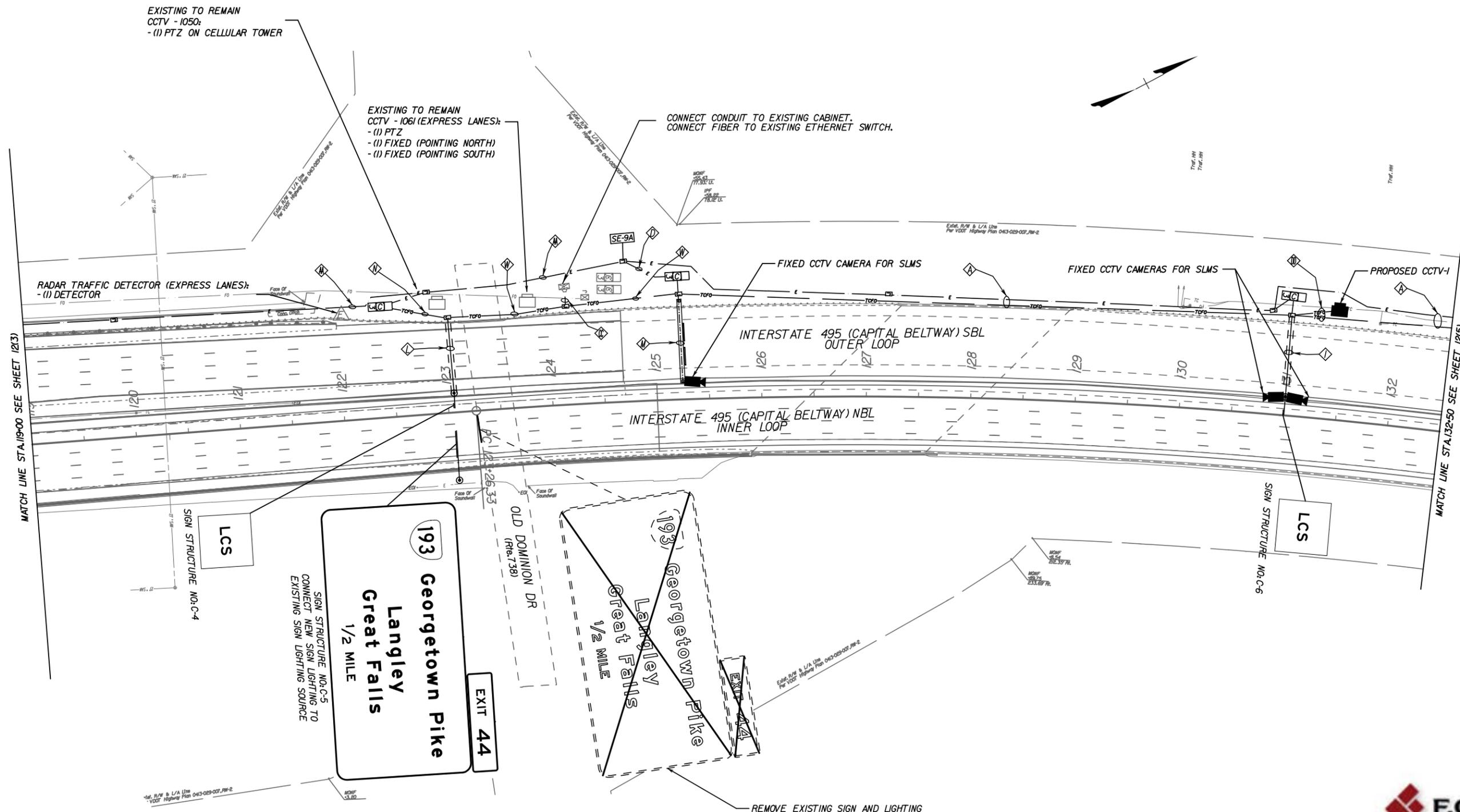
PROJECT	SHEET NO.
0495-029-123	12(3)

PROJECT MANAGER Paul M. Ishikawa (571) 483-2622
SURVEYED BY DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

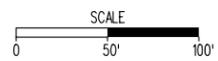
SIGNING AND ITS COMMUNICATION CONTROL PLANS

REVISED	STATE		PROJECT	SHEET NO.
	STATE	ROUTE		
	VA.	495	0495-029-123 C501	12(4)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



CONCEPT PLANS
MARCH 12, 2014



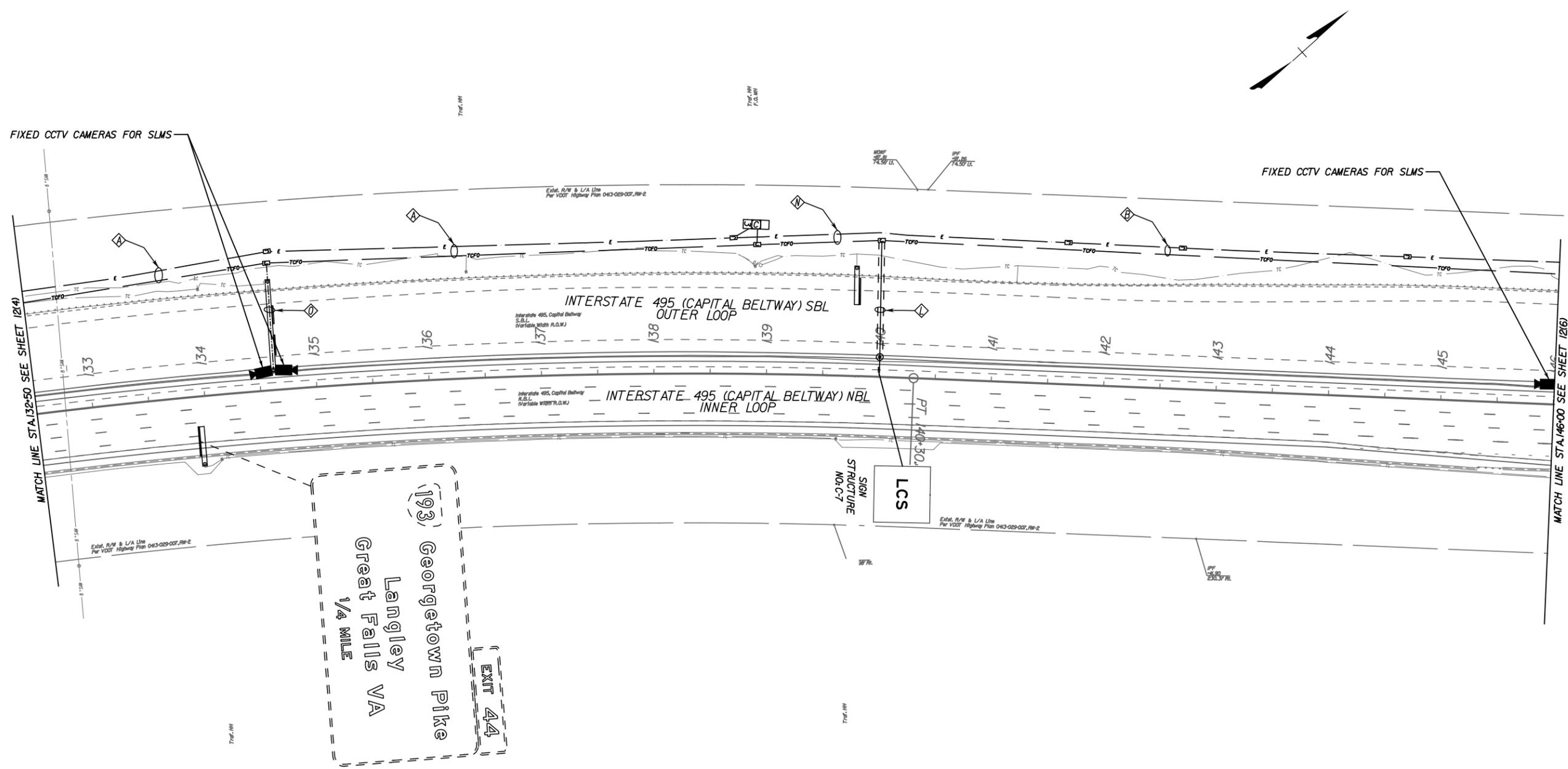
PROJECT	SHEET NO.
0495-029-123	12(4)

PROJECT MANAGER Paul M. Ishikawa (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

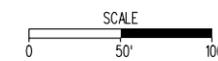
SIGNING AND ITS COMMUNICATION CONTROL PLANS

REVISED	STATE		PROJECT	SHEET NO.
	STATE	ROUTE		
	VA.	495	0495-029-123 C501	12(5)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



CONCEPT PLANS
MARCH 12, 2014

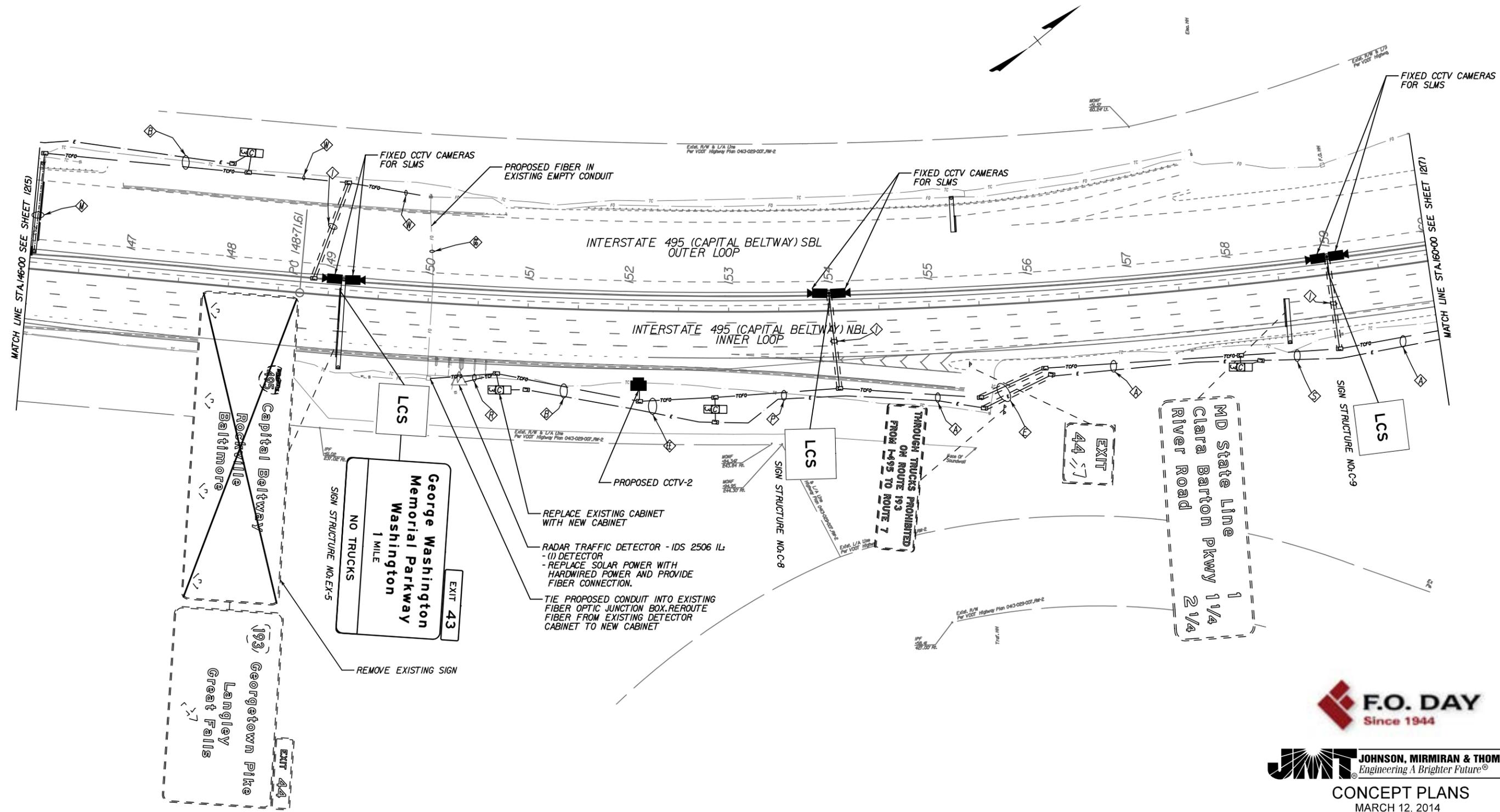


PROJECT	SHEET NO.
0495-029-123	12(5)

SIGNING AND ITS COMMUNICATION CONTROL PLANS

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	495	0495-029-123 C501	12(16)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



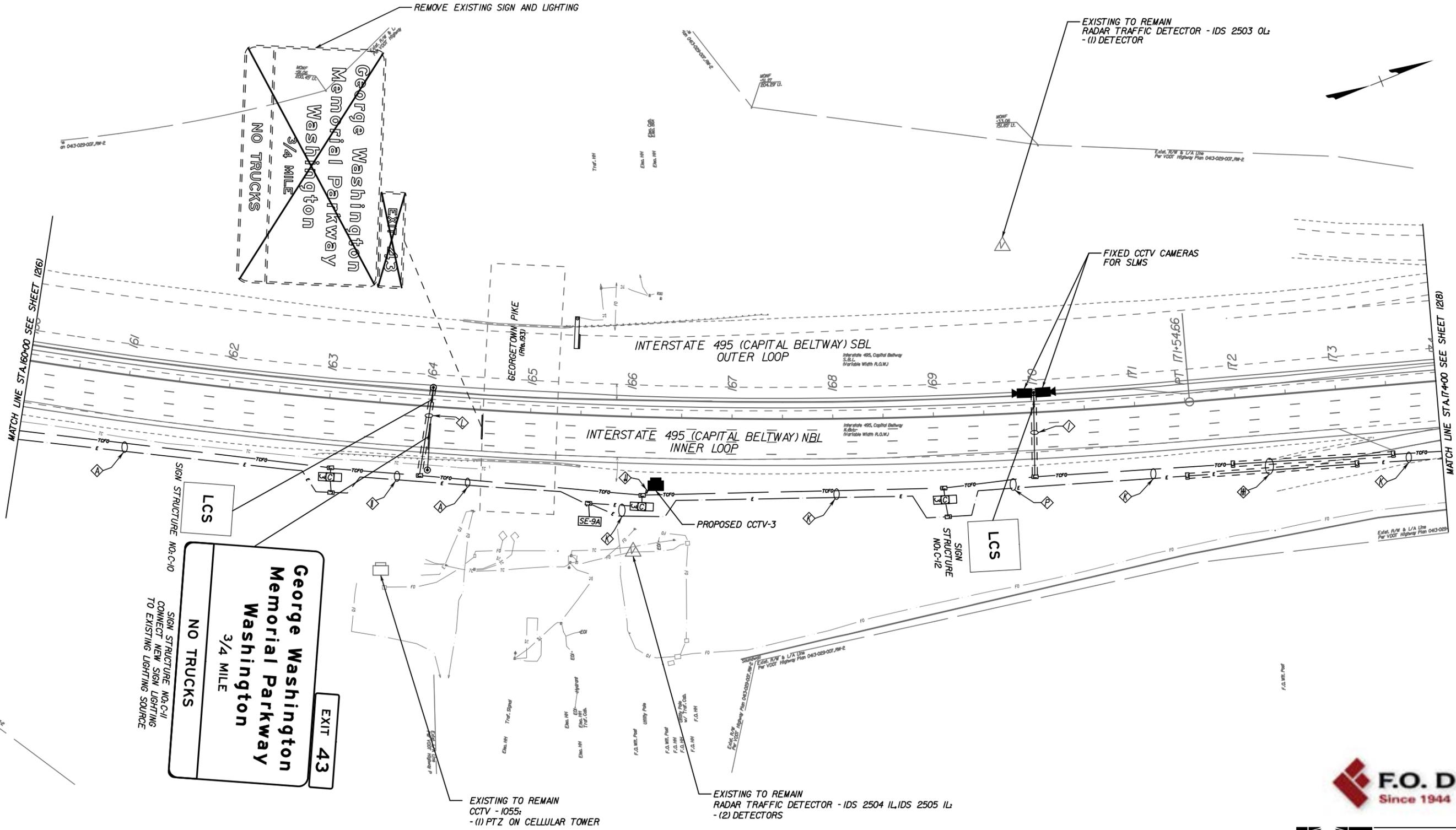
CONCEPT PLANS
MARCH 12, 2014

PROJECT	SHEET NO.
0495-029-123	12(16)

SIGNING AND ITS COMMUNICATION CONTROL PLANS

REVISED	STATE		STATE PROJECT		SHEET NO.
	STATE	ROUTE	PROJECT		
	VA.	495	0495-029-123 C501	12(7)	

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



George Washington Memorial Parkway Washington 3/4 MILE

NO TRUCKS

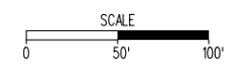
EXIT 43

SIGN STRUCTURE NO-C-11
CONNECT NEW SIGN LIGHTING TO EXISTING LIGHTING SOURCE

SIGN STRUCTURE NO-C-10



CONCEPT PLANS
MARCH 12, 2014



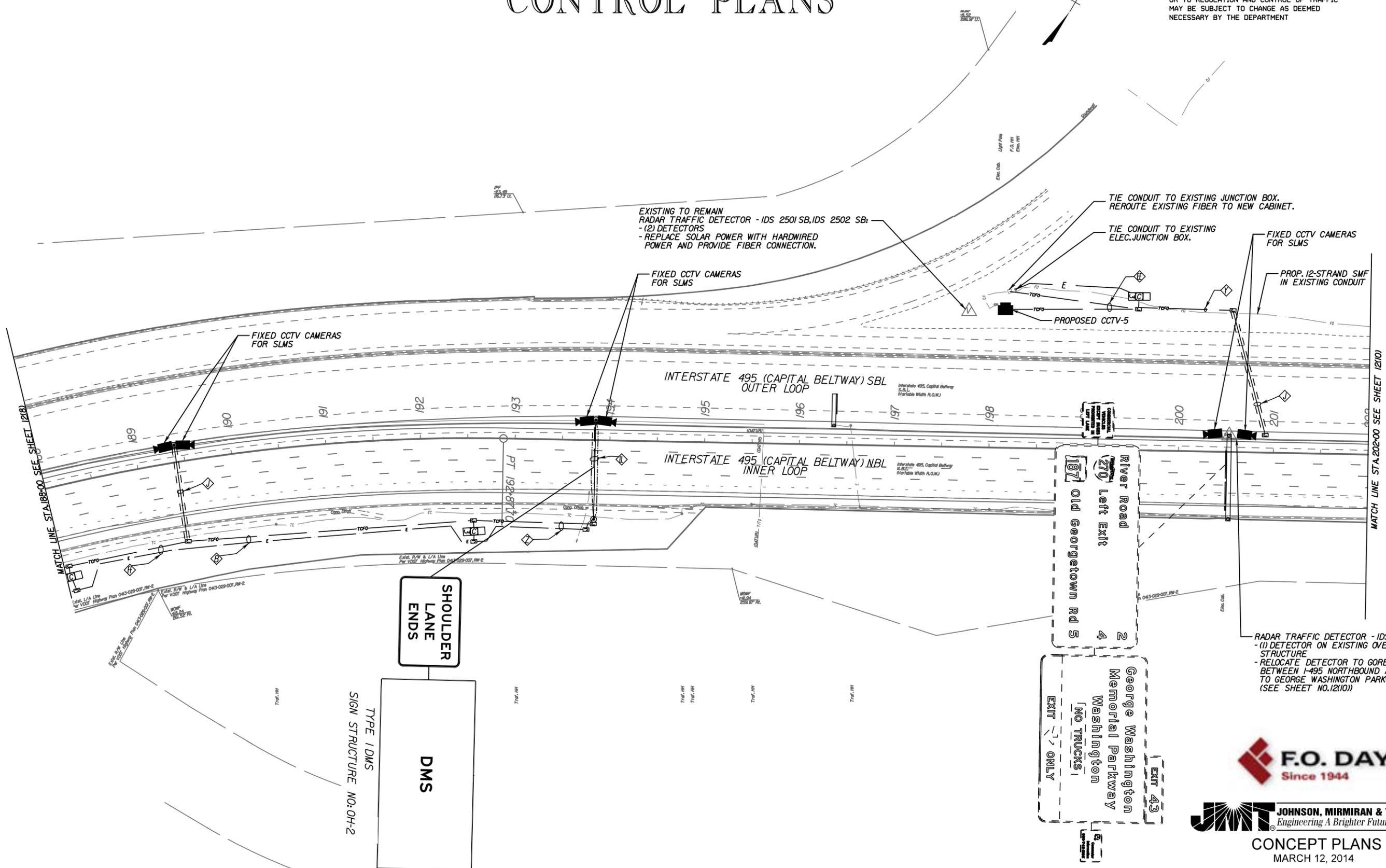
PROJECT 0495-029-123	SHEET NO. 12(7)
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PROJECT MANAGER Paul M. Ishimura (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

SIGNING AND ITS COMMUNICATION CONTROL PLANS

REVISED	STATE		SHEET NO.
	ROUTE	PROJECT	
	VA.	495	0495-029-123 C501
			12(9)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



CONCEPT PLANS
MARCH 12, 2014

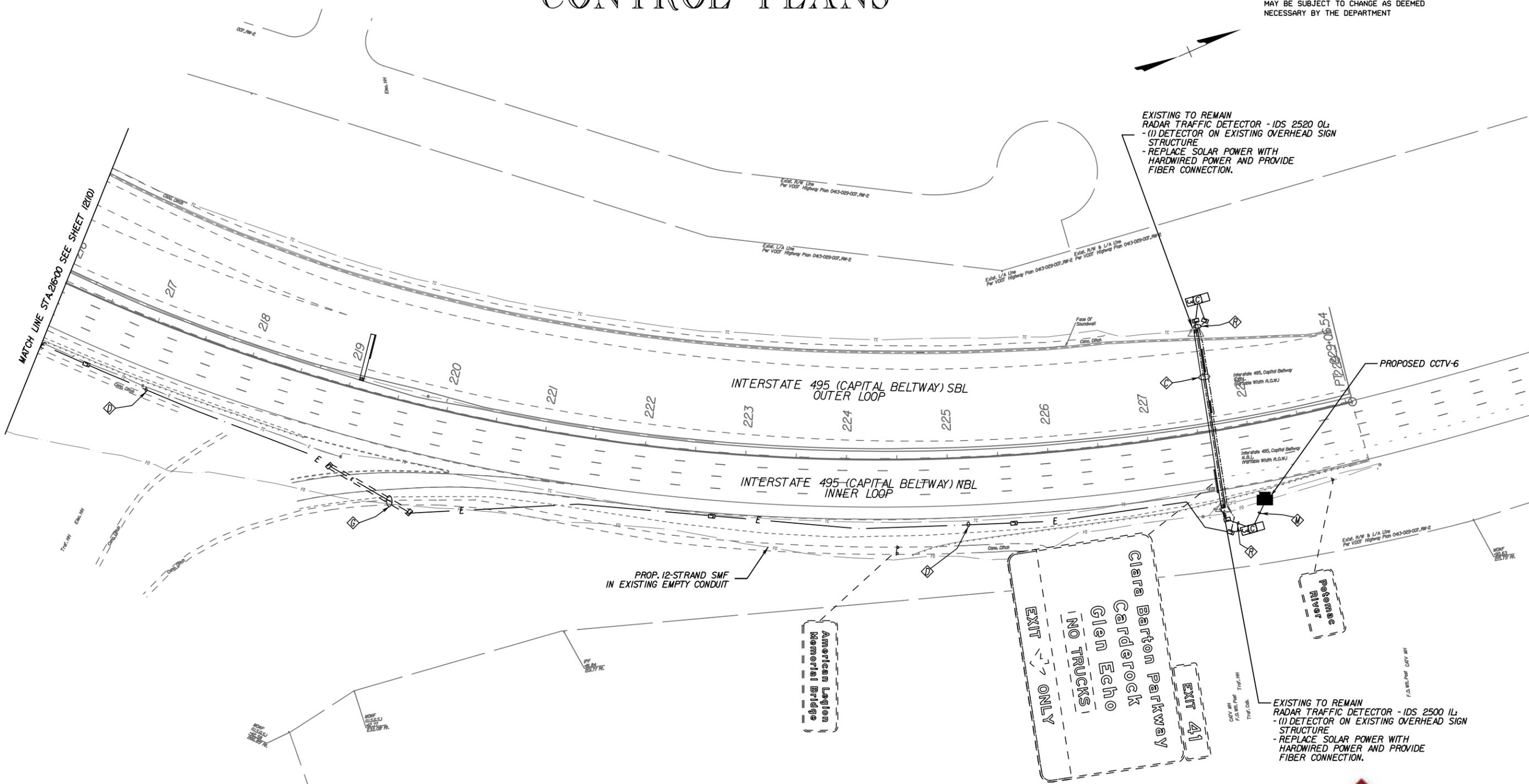
PROJECT 0495-029-123	SHEET NO. 12(9)
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PROJECT MANAGER Paul M. Ishimura (571) 483-2622
SURVEYED BY, DATE VDOT
DESIGN BY Whitman, Bequardt & Associates (703) 293-9717
Johnson, Mirmiran and Thompson 703-464-7369

SIGNING AND ITS COMMUNICATION CONTROL PLANS

REVISED	STATE		STATE PROJECT	SHEET NO.
	STATE	ROUTE		
	VA.	495	0495-029-123 C501	12(11)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



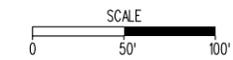
EXISTING TO REMAIN
RADAR TRAFFIC DETECTOR - IDS 2520 OL
- (1) DETECTOR ON EXISTING OVERHEAD SIGN STRUCTURE
- REPLACE SOLAR POWER WITH HARDWIRED POWER AND PROVIDE FIBER CONNECTION.

PROP. 12-STRAND SMF IN EXISTING EMPTY CONDUIT

EXISTING TO REMAIN
RADAR TRAFFIC DETECTOR - IDS 2500 IL
- (1) DETECTOR ON EXISTING OVERHEAD SIGN STRUCTURE
- REPLACE SOLAR POWER WITH HARDWIRED POWER AND PROVIDE FIBER CONNECTION.



CONCEPT PLANS
MARCH 12, 2014



PROJECT	SHEET NO.
0495-029-123	12(11)



Attachment 4.2.8

Written Statement

Fully Complaint with RFP

4.2.8 Written Statement
Fully Complaint with RFP



Design Criteria Table and Limits of Construction

The F.O. Day/JMT Team’s proposal is fully compliant with the Design Criteria Table included in the RFP Technical Requirements (Part 2) as Attachments 2.2 (shown below) and all other requirements of this RFP, Addendum #1 and Questions & Answers.

ATTACHMENT 2.2 - ROADWAY INVENTORY AND MAJOR DESIGN CRITERIA
 I-495 NORTHERN SECTION SHOULDER USE
 VDOT PROJECT NUMBER 0495-029-123, P101, C501

ROADWAY INVENTORY AND MAJOR DESIGN CRITERIA	
I-495 NORTHERN SECTION SHOULDER USE	
Roadway Classification	Urban Principal Arterial - Freeway
Geometric Design Standard	GS-5
Existing (2013) ADT	109,000 (one way)
Projected (2035) ADT	177,400 (one way)
Design Hourly Volume	11,000 (one way)
Truck Percentage (Design Hour)	4.3%
Terrain	Level
Design Speed (mph)	70
Max. Rate of Superelevation	8.00%
Lane Width (min.)	11 ft
Minimum Vertical Clearance (ft.)	16'-0"
Maximum Rollover between Shoulder Use Lane and Regular Travel Lane	5%

We also certify that the proposed limits of construction including all stormwater management facilities are located within the right-of-way limits shown on the RFP/Addendum #1 Conceptual Plans with the exception of permanent and temporary easements. The F.O. Day/JMT Team design concept does not require Design Exception and/or Design Waivers unless they are identified or included in the RFP or Addendum.

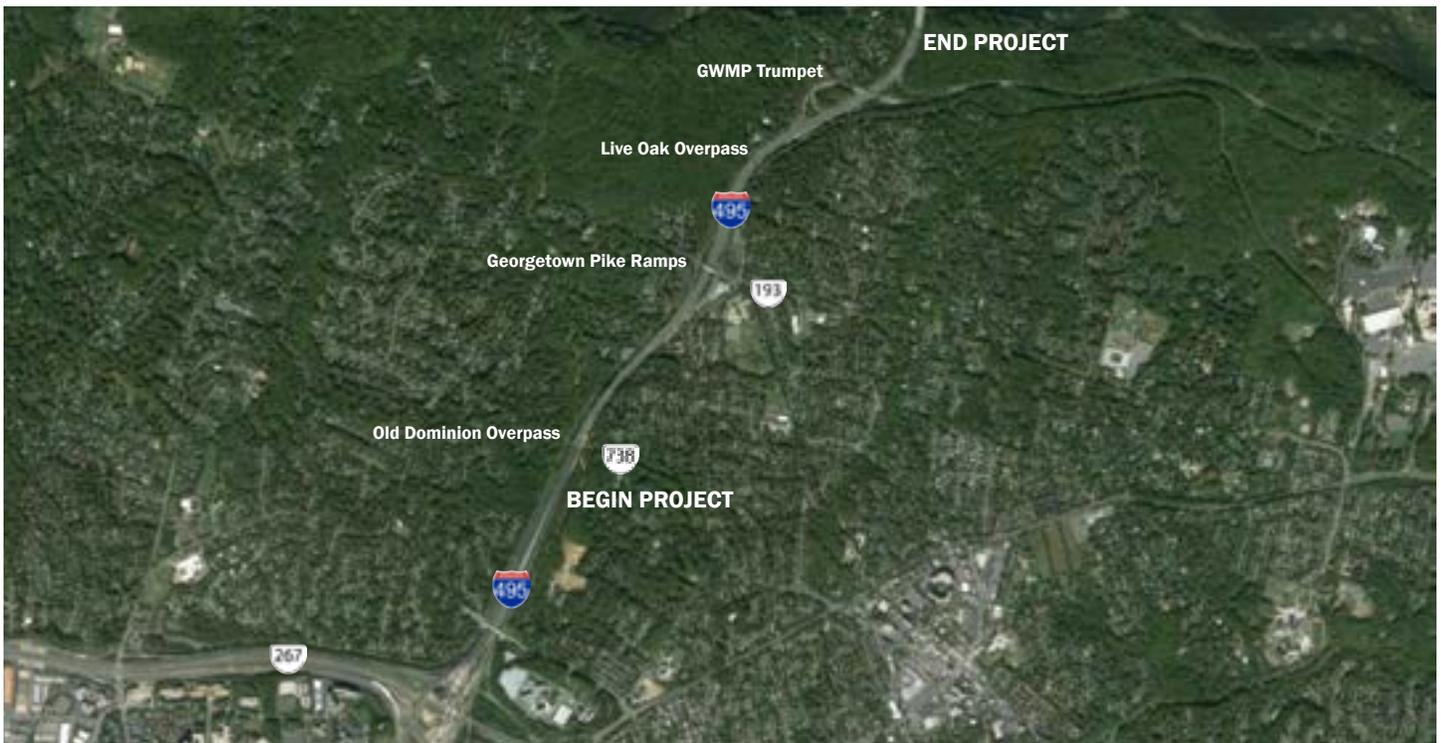


Francis O. Day Company, Inc.
850 East Gude Dr., Suite A
Rockville, MD 20850
P. 301.652.2400
F. 302.424.3697
www.foday.com

I-495 NORTHERN SECTION SHOULDER USE FAIRFAX COUNTY, VA

A Design-Build Project

From: South of Old Dominion Overpass **To:** George Washington Memorial Parkway



March 14, 2014

State Project No.: (FO) 0495-029-123, P101, C501

Federal Project No.: STP-495-5(094)

Contract ID No.: C00105130DB72

ATTACHMENT 4.0.1.2

**DESIGN-BUILD PRICE PROPOSAL
CHECKLIST**

**Project Name: I-495 Northern Section Shoulder Use
in Fairfax County, Virginia
Contract ID Number: C00105130DB72**

➤ **Contents of Price Proposal:**

- Proposal Price, in both numbers and words (Attachment 4.3.1)**
 - Schedule of Items itemized in accordance with Part 1, Section 4.4.6, including material quantities and costs of each proposed work package**
 - Proposed Monthly Payment Schedule showing the anticipated schedule on which funds will be required and associated value of work in accordance with Part 1, Section 4.4.7**
 - Price Adjustment Information and Forms for Fuel, Asphalt and Steel, including identification of pay items and associated quantities eligible for adjustment (Part 3, Section 6.3, Attachments 6.3)**
 - Proposal Guaranty (C-24) required by Section 102.07 of Part 5, Division I Amendments to the Standard Specifications**
 - Sworn Statement Forms (C-104, C-105, Attachments 4.3.4(a) and 4.3.4(b))**
 - DBE Requirements Forms (C-111, C-49 and C-112) as applicable (Attachments 4.3.5(a), 4.3.5(b) and 4.3.5(c))**
 - CD-ROM containing the entire Price Proposal in a single cohesive Adobe PDF file**
-



4.3.1

Cost Breakdown Summary



ATTACHMENT 4.3.1
PRICE PROPOSAL FORM

- 4.3.1 Offeror shall specify the pricing information for the items below, the dollars amount shall be in whole numbers:

Price Proposal Cost Breakdown Summary;

Design Services, LS	\$ 1,123,000.00
Mobilization (Construction), LS	\$ 950,000.00
Quality Assurance (QA) (Construction), LS	\$ 300,000.00
Quality Control (QC) (Construction), LS	\$ 350,000.00
Earthwork, LS	\$ 399,838.50
Roadway Incidentals, LS	\$ 715,806.70
Drainage (Structures), LS	\$ 1,245,872.50
Utilities, LS	\$ 1.00
Pavement, LS	\$ 4,652,763.40
Full Depth Transverse Joint Repair, LS	\$ 241,920.00
Permanent Traffic Control/Signage, LS	\$ 1,039,802.00
Maintenance of Traffic, LS	\$ 1,272,264.90
ITS Components, LS	\$ 2,968,550.00
Barrier Modification, LS	\$ 3,586,590.50
Bridge Mounted signs Removal and Replacement, LS	\$ 12,300.00
All Others Costs, LS	\$ 1,015,000.00

Proposal Price; (Specify the Total Lump Sum price in both numbers and words, this price shall **equal** to the total sum of the items listed above)

Lump Sum (LS): Nineteen million eight hundred seventy three thousand seven hundred nine
dollars and 50 cents ----- (\$ 19,873,709.50)

Signature:  Date: March 14, 2014

Design-Builder: Francis O. Day Company, Inc.

Vendor No.: D499

4.3.2

Adjustments to Asphalt, Fuel and Steel Prices

EXHIBIT 6.3(a)
ADJUSTMENT FOR ASPHALT

SPECIAL PROVISION FOR
ASPHALT MATERIAL PRICE ADJUSTMENT
DESIGN-BUILD PROJECTS

August 9, 2013

All asphalt material listed in the attached "Master Listing of Asphalt Material Items Eligible for Price Adjustment" will be adjusted in accordance with the provisions as set forth herein. Other items will not be adjusted, except as otherwise specified in the contract. Any item added through a Work Order which contains Asphalt Material will not be subject to Price Adjustment unless specifically designated in the Work Order to be subject to Price Adjustment.

Each month, the Department will publish an average state-wide PG 64-22 f.o.b. price per ton developed from the average terminal prices provided to the Department from suppliers of asphalt cement to contractors doing work in Virginia. The Department will collect terminal prices from approximately 12 terminals each month. These prices will be received once each month from suppliers on or about the last weekday of the month. The high and low prices will be eliminated and the remaining values averaged to establish the average statewide price for the following month. That monthly state-wide average price will be posted on the Construction Division website on or about the first weekday of the following month.

This monthly statewide average price will be the Base Index for all contracts on which Price Proposals are received during the calendar month of its posting and will be the Current Index for all asphalt placed during the calendar month of its posting. In the event an index changes radically from the apparent trend, as determined by the Engineer, the Department may establish an index which is determined to best reflect the trend.

The amount of adjustment applied will be based on the difference between the Price Proposal/Contract Base Index and the Current Index for the applicable calendar month during which the work is performed. Calculations must be done for each type of Asphalt Material put in place each month, whether the Current Index is higher or lower than the Base Index. The calculation for the adjustment shall be shown as follows:

$$A = Q \times \%AC \times IC$$

Where: A = Asphalt Adjustment Dollar Amount

Q = Quantity of Asphalt Material put in place during the month

%AC = % of Asphalt Cement in the Asphalt Material as specified in the Job Mix Formula

IC = Numeric Dollar Difference, either positive or negative, between the Base Index and Current Index

Example Calculation for Negative Price Adjustment (Credit back to VDOT):

7,500 Tons of SM-12.5A put in place during the month (Q), Job Mix is 6.1% Asphalt Cement for SM-12.5A (%AC), Base Index for the Contract is \$515/Ton, Current Index is \$500/Ton, Difference of - \$15.00/Ton (IC)

$$7,500 \text{ Tons SM-12.5A} \times 6.1\% \times -\$15.00/\text{Ton} = -\$6,862.50 \text{ Adjustment Amount}$$

Example Calculation for Positive Price Adjustment (Paid to the Design-Builder):

10,000 Tons of BM-25.0A put in place during the month (Q), Job Mix is 5.2% Asphalt Cement for BM-25.0A (%AC), Base Index for the Contract is \$515/Ton, Current Index is \$560/Ton, Difference of + \$45.00/Ton (IC)

$$10,000 \text{ Tons BM-25.0A} \times 5.2\% \times \$45.00/\text{Ton} = +\$23,400.00 \text{ Adjustment Amount}$$

Adjustment of any asphalt material item designated as a price adjustment item which does not contain PG 64-22, except PG 76-22 or PG 70-28, will be based on the indexes for PG 64-22. The quantity of asphalt cement for asphalt concrete pavement to which adjustment will be applied will be the quantity based on the percent of asphalt cement shown on the appropriate approved job mix formula.

The quantity of asphalt emulsion for surface treatments to which adjustment will be applied will be the quantity based on 65 percent residual asphalt.

Price adjustment will be shown as a separate entry on the monthly application of payment for work packages completed; however, such adjustment will not be included in the total cost of the work for progress determination or for extension of contract time. Items the Design-Builder claims in its application of payment for asphalt adjustments must include supporting calculations certified by the Quality Assurance Manager (QAM). These calculations must be completed relative to the calendar month under which the work was performed and shall be submitted for either positive or negative adjustment.

Any apparent attempt to unbalance bids in favor of items subject to price adjustment or failure to submit required cost and price data as noted hereinbefore may result in rejection of items for asphalt adjustment.

VIRGINIA DEPARTMENT OF TRANSPORTATION
MASTER LISTING OF
ASPHALT MATERIAL ITEMS ELIGIBLE FOR PRICE ADJUSTMENT
(10-27-09)

ITEM	DESCRIPTION	UNITS	SPECIFICATION
10062	Asphalt-Stab. Open-Graded Material	Ton	313
10416	Liquid Asphalt	Gal	311 312
10420	Blotted Seal Coat Ty. B	Sy	ATTD
10422	Blotted Seal Coat Ty. C	Sy	ATTD
10423	Blotted Seal Coat Ty. C-1	Sy	ATTD
10424	Blotted Seal Coat Ty. D	Sy	ATTD
10598	Ns Asphalt Concrete	Ton	315
10606	Asphalt Concrete Ty. SM-9.5	Ton	315
10607	Asphalt Concrete Ty. SM-12.5A	Ton	315
10608	Asphalt Concrete Ty. SM-12.5D	Ton	315
10609	Asphalt Concrete Ty. SM-12.5E (76-22)	Ton	315
10610	Asphalt Concrete Ty. IM-19.0A	Ton	315
10611	Asphalt Concrete Ty. IM-19.0D	Ton	315
10612	Asphalt Conc. Base Cr. Ty. BM-25.0	Ton	315
10613	Asphalt Concrete Ty. BM-37.5	Ton	315
10635	Asphalt Concrete Ty. SM-9.5A	Ton	315
10636	Asphalt Concrete Ty. SM-9.5D	Ton	315
10637	Asphalt Concrete Ty. SM-9.5E (76-22)	Ton	315
10639	Asphalt Concrete Ty. SM-19.0	Ton	315
10642	Asphalt Concrete Ty. BM-25.0A	Ton	315
10643	Asphalt Concrete Ty. BM-25.0D	Ton	315
10650	Stone Matrix Asphalt SMA-9.5(70-22)	Ton	317
10651	Stone Matrix Asphalt SMA-9.5(76-22)	Ton	317
10652	Stone Matrix Asphalt SMA-12.5(70-22)	Ton	317
10653	Stone Matrix Asphalt SMA-12.5(76-22)	Ton	317
10654	Stone Matrix Asphalt SMA-19.0(70-22)	Ton	317
10655	Stone Matrix Asphalt SMA-19.0(76-22)	Ton	317
10701	Liquid Asphalt Coating	Sy	ATTD
12505	Asphalt Concrete Curb Backup Material	Ton	315
13240	Asphalt Concrete Sidewalk	Ton	504
16110	Emul. Asph. Slurry Seal Type A	Sy	ATTD
16120	Emul. Asph. Slurry Seal Type B	Sy	ATTD
16130	Emul. Asph. Slurry Seal Type C	Sy	ATTD
16144	Latex Mod. Emul. Treat. Type B	Ton	ATTD
16145	Latex Mod. Emul. Treat. Type C	Ton	ATTD
16146	Latex Mod. Emul. Treat. Rutfilling	Ton	ATTD
16161	Modified Single Seal	Sy	ATTD
16162	Modified Double Seal	Sy	ATTD
16249	Nontracking Tack Coat	Gal.	ATTD
16250	Liquid Asphalt Matl. CMS-2 (Mod)	Gal	ATTD
16251	Liquid Asphalt Matl. CMS-2	Gal	ATTD
16252	Liquid Asphalt Matl. CRS-2	Gal	ATTD
16253	Liquid Asphalt Matl. CRS-2H	Gal.	ATTD.
16254	Liquid Asphalt Matl. RC-250	Gal	ATTD
16256	Liquid Asphalt Matl. RC-800	Gal	ATTD
16257	Ns Liquid Asphalt Matl.	Gal	ATTD
16260	Liquid Asphalt Matl. CRS-2L	Gal	ATTD
16325	NS Asphalt Concrete	Ton	N/A

16330	Asphalt Concrete Ty. SM-9.0A	Ton	315
16335	Asphalt Concrete Ty. SM-9.5A	Ton	315
16337	Asph. Conc. Ty. SM-9.5ASL (Spot Level)	Ton	315
16340	Asphalt Concrete Ty. SM-9.5D	Ton	315
16342	Asph. Conc. Ty. SM-9.5DSL (Spot Level)	Ton	315
16345	Asphalt Concrete Ty. SM-9.5E (76-22)	Ton	315
16350	Asphalt Concrete Ty. SM-12.5A	Ton	315
16352	Asph. Con. Ty. SM-12.5ASL (Spot Level)	Ton	315
16355	Asphalt Concrete Ty. SM-12.5D	Ton	315
16357	Asph. Con. Ty. SM-12.5DSL (Spot Level)	Ton	315
16360	Asphalt Concrete Ty. SM-12.5E (76-22)	Ton	315
16365	Asphalt Concrete Ty. IM-19.0A	Ton	315
16370	Asphalt Concrete Ty. IM-19.0D	Ton	315
16373	Asphalt Concrete Ty. IM-19.0A (T)	Ton	315
16374	Asphalt Concrete Ty. IM-19.0D (T)	Ton	315
16377	Asphalt Concrete Ty. BM-37.5	Ton	315
16379	Asphalt Concrete Ty. IM-19.0T	Ton	315
16390	Asphalt Concrete Ty. BM-25.0A	Ton	315
16392	Asphalt Concrete Ty. BM-25.0D	Ton	315
16395	Asphalt Concrete Ty. BM-25.0A (T)	Ton	315
16397	Asphalt Concrete Ty. BM-25.0D (T)	Ton	315
16400	Stone Matrix Asphalt SMA-9.5(70-22)	Ton	ATTD
16401	Stone Matrix Asphalt SMA-9.5(76-22)	Ton	ATTD
16402	Stone Matrix Asphalt SMA-12.5(70-22)	Ton	ATTD
16403	Stone Matrix Asphalt SMA-12.5(76-22)	Ton	ATTD
16404	Stone Matrix Asphalt SMA-19.0(70-22)	Ton	ATTD
16405	Stone Matrix Asphalt SMA-19.0(76-22)	Ton	ATTD
16490	Hot Mix Asphalt Treatment	Ton	ATTD
16500	Surf.Preparation & Restoration Type I	Ton	ATTD
16502	Surf.Preparation & Restoration Type II	Ton	ATTD
16504	Surf.Preparation & Restoration Type III	Ton	ATTD
67201	NS Asphalt Concrete Overlay	Ton	315
67210	NS Asphalt Concrete	Ton	315
68240	NS Asphalt Concrete	Ton	315

Exhibit 6.3(b)

Form C-16a
August 9, 2013

COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION
ASPHALT PRICE ADJUSTMENT (PG76-22 or PG 70-28)
DESIGN-BUILD PROJECTS

INSTRUCTIONS - This form is to be completed and returned ONLY when asphalt concrete items containing PG 76-22 or PG 70-28 is being utilized on the project.

PROJECT NUMBER: (FO) 0495-029-133, P101, C501

DISTRICT: Northern Virginia

Bid Prices in this contract for items containing PG 76-22 or PG 70-28 asphalt cement were developed using an f.o.b. price of \$ 560.00 per IMPERIAL ton for PG 76-22 or PG 70-28. This quote is project specific.

Price quotes signed by each supplier from which the Design-Builder proposes to obtain PG 76-22 or PG 70-28 shall be maintained by the Design-Builder. These quotes shall be retained on site during the life of the Contract for review by the Engineer upon request.

DATE: March 12, 2014

SIGNATURE: 

Francis O. Day Company, Inc.
(Firm or Corporation)

D499
(Vendor No.)

**EXHIBIT 6.3 (c)
ADJUSTMENT FOR FUEL**

**VIRGINIA DEPARTMENT OF TRANSPORTATION
SPECIAL PROVISION FOR
OPTIONAL ADJUSTMENT FOR FUEL
DESIGN-BUILD PROJECTS**

November 5, 2012

In the event the Design-Builder elects to seek adjustment for fuel items designated in the Price Proposal/Contract as Price Adjustment Items such items will be subject to price adjustment as set forth herein. Other items will not be adjusted, except as otherwise specified in the contract.

The Design-Builder will submit their monthly application for payment associated with eligible work packages with an adjustment up or down as appropriate for cost changes in fuel used on specific items of work identified in this provision. A master listing of standard items eligible for fuel adjustment is provided by the Department on its website at the following link <http://www.virginiadot.org/business/resources/masteroptionalfuelitems.pdf>. The listing on the web site also includes the corresponding fuel factor for each item. The fuel usage factor for each item is considered inclusive of all fuel usage.

The amount of adjustment will be computed from the change in the indexes and the on-site fuel use as shown in the Department's master listing of eligible items.

In order to be eligible for fuel adjustment under this provision, the Design-Builder shall clearly identify in within the Schedule of Values those pay items and the associated quantities it chooses to have fuel adjustment applied to in its work packages. Items the Design-Builder claims in its application of payment for fuel adjustments must be properly designated in order to be considered for adjustment. Items not properly designated or left out of the Design-Builder's Schedule of Values will automatically not be considered for adjustment.

The monthly index price to be used in the administration of this provision will be calculated by the Department from the Diesel fuel prices published by the U. S. Department of Energy, Energy Information Administration on highway diesel prices, for the Lower Atlantic region. The monthly index price will be the price for diesel fuel calculated by averaging each of the weekly posted prices for that particular month.

For the purposes of this provision, the base index price will be calculated using the data from the month preceding the receipt of bids. The base index price will be posted by the Department at the beginning of the month for all bids received during that month.

The current index price will be posted by the Department and will be calculated using the data from the month preceding the particular estimate being vouchered for payment.

The current monthly quantity for eligible items of work selected by the Design-Builder for fuel adjustment in its work packages will be multiplied by the appropriate fuel factor to determine the gallons of fuel to be cost adjusted. The amount of adjustment per gallon will be the net difference between the current index price and the base index price. Computation for adjustment will be made as follows:

$$S = (E - B) QF$$

Where; S = Monetary amount of the adjustment (plus or minus)
B = Base index price
E = Current index price
Q = Quantity of individual units of work

F = Appropriate fuel factor

Adjustments will not be made for work performed beyond the original contract time limit unless the original time limit has been changed by an executed Work Order.

If new pay items are added to this contract by Work Order and they are listed in the Department's master listing of eligible items, the Work Order must indicate which of these individual items will be fuel adjusted; otherwise, those items will not be fuel adjusted. If applicable, designating which new pay items will be added for fuel adjustment must be determined during development of the Work Order and clearly shown on the Work Order form. The Base Index price on any new eligible pay items added by Work Order will be the Base Index price posted for the month in which bids were received for that particular project. The Current Index price for any new eligible pay items added by Work Order will be the Index price posted for the month preceding the estimate on which the Work Order is paid.

When quantities differ between the last monthly application of payment prepared upon final acceptance and the final application of payment, adjustment will be made using the appropriate current index for the period in which that specific item of work was last performed.

In the event any of the base fuel prices in this contract increase more than 100 percent (i.e. fuel prices double), the Department will review each affected item of work and give the Design-Builder written notice if work is to stop on any affected item of work. The Department reserves the right to reduce, eliminate or renegotiate the price for remaining portions of affected items of work.

Any amounts resulting from fuel adjustment will not be included in the total cost of work for determination of progress or for extension of contract time.

I elect to use this provision

I elect not to use this provision

Date: March 12, 2014

Signature: 

Design-builder: Francis O. Day Co., Inc.

Vendor No.: D499

**EXHIBIT 6.3(d)
ADJUSTMENT FOR STEEL**

**VIRGINIA DEPARTMENT OF TRANSPORTATION
SPECIAL PROVISION FOR
PRICE ADJUSTMENT FOR STEEL
DESIGN-BUILD PROJECTS**

June 30, 2011

In the event the Design-Builder elects to seek adjustment for steel items designated in the Price Proposal/Contract as Price Adjustment Items such items will be subject to price adjustment as set forth herein. If new pay items which involve steel are established by Work Order, they will not be subject to Price Adjustment unless specifically designated in the Work Order to be subject to Price Adjustment.

The Design-Builder will submit their monthly application for payment associated with eligible work packages with an adjustment up or down as appropriate for cost changes in steel used on specific items of work identified in the Price Proposal/contract in accordance with this provision. Provided at the end of this provision is a master listing of standard bid items the Department has determined are eligible for steel price adjustment. Inventoried materials from the listing of eligible items are specifically excluded for consideration. In addition, concrete items where reinforcing steel is normally included in the unit bid price for the item such as (but not limited to) drop inlets, median barriers, sound barrier walls, bridge railing and parapets, are not eligible for consideration under this provision.

The requirements of this provision shall apply only to material cost changes that occur between the date of the opening of the Price Proposal and the date the material is shipped to the fabricator. To be eligible for this price adjustment, Design-Builder is required to fill out the accompanying Form for Price Adjustment for Eligible Steel Items on Design-Build Projects and submit the same with its Price Proposal for the Project. By signing the Form and submitting it with its Price Proposal Design-Builder declares its intention to participate in the price adjustment in its contract with the Department. For the purposes of this provision, the prices listed on the Form for Price Adjustment for Eligible Steel Items on Design-Build projects are fixed for cost and adjustment calculations regardless of quantities incorporated into final design. Further, in order for steel items to be eligible for adjustment, once shipped to the fabricator, the items shall be specifically stored, labeled, or tagged, recognizable by color marking, and identifiable by project for inspection and audit verification.

Design-Builder shall upon request furnish documentation supporting the price per pound for eligible steel items as shown on the Form for Price Adjustment for Eligible Steel Items on Design-Build Projects furnished with its Price Proposal. Design-Builder must use the format as shown with this Form; no other format for presenting this information will be permitted. Design-Builder shall certify that all items of documentation are original and were used in the computation of the price per pound amount for the represented eligible pay items for the month the Price Proposal was opened. This documentation shall support the base line material price ("Base Price") of the steel item only. Base price per pound shall not include the following cost components: fabrication, shipping, storage, handling, and erection.

Failure to submit all documentation required or requested supporting the per pound prices on eligible steel items will result in Design-Builder being ineligible for a price adjustment of any or all steel items.

Price adjustment of each qualifying item under consideration will be subject to the following condition:

There is an increase or decrease in the cost of eligible steel materials in excess of 10 percent up to a maximum of 60 percent from the Base Price when compared with the latest published price index ("Price Index") in effect at the time material is shipped to the fabricator.

The Price Index the Department is using is based on The U.S. Department of Labor, Bureau of Labor Statistics, Producers Price Index (PPI) which measures the average price change over time of the specific

steel eligible item from the perspective of the seller of goods. The Master List table provided at the end of this provision indicates the Producers Price Index (PPI) steel category index items and the corresponding I.D. numbers to which VDOT items will be compared. **Please note:** The Producers Price Index (PPI) is subject to revision 4 months after original publication, therefore, price adjustments and payments will not be made until the index numbers are finalized.

The price adjustment will be determined by computing the percentage of change in index value beyond 10 percent above or below the index on the date of opening of Design-Builder's Price Proposal to the index value on the date the steel material is shipped to the fabricator (Please see included sample examples). Weights and date of shipment must be documented by a bill of lading provided to the Department. The final price adjustment dollar value will be determined by multiplying this percent increase or decrease in the index (after 10%) by the represented quantity of steel shipped, by the Base Price per pound subject to the limitations herein.

Price increase/decrease will be computed as follows:

$$A = B \times P \times Q$$

- Where;
- A = Steel price adjustment in lump sum dollars
 - B = Average weighted price of steel submitted in Design-Builder's Price Proposal for project in price per pound as listed on the Form for Price Adjustment for Eligible Steel Items on Design-Build Project
 - P = Adjusted percentage change in PPI average from shipping date to date of opening of Price Proposal minus 10% (0.10) threshold
 - Q = Total quantity of steel in pounds shipped to fabricator for specific project

The need for application of the adjustments herein to extra work will be determined by the Engineer on an individual basis and, if appropriate, will be specified on the Work Order.

This price adjustment is capped at 60 percent. This means the maximum "P" value for increase or decrease that can be used in the above equation is 50% (60%-10% threshold).

Calculations for price adjustment shall be shown separate from the monthly progress payment for work packages and will not be included in the total cost of work for determination of progress or for extension of contract time.

Upon Department review and due process consideration for redress by Design-Builder, any apparent evidence to unbalance the price supplied by Design-Builder in favor of items subject to price adjustment will result in ineligibility for Department participation under this provision.

Sample Calculation of a Price Adjustment (Increase)

Project bid on April 28, 2004.

Project has 450,000 lb. of eligible structural steel.

Design Builder's *f.o.b. supplier price for structural steel submitted in the Price Proposal is \$0.2816 per pound.

*free on board

Adjusted** BLS Producers Price Index (PPI) most recently published average at time of opening of the Price Proposal is 139.6.

All eligible steel shipped to fabricator in same month, October 2004.

Adjusted BLS Producers Price Index (PPI) most recently published average for month of October is 161.1

Adjustment formula is as follows:

$$A = B \times P \times Q$$

- Where:
- A = Steel price adjustment in lump sum dollars
 - B = Average weighted price of steel submitted in the Price Proposal for Design-Build project in \$ per pound
 - P = Adjusted percentage change in PPI average from shipping date to date of submitted Price Proposal minus 10% (0.10) threshold
 - Q = Total quantity of eligible steel shipped to fabricator in October 2004 for this project in pounds

$$B = \$0.2816$$

$$P = (161.1 - 139.6) / 139.6 - 0.10 = 0.054$$

$$Q = 450,000 \text{ lb.}$$

$$A = 0.2816 \times 0.054 \times 450,000$$

$$A = \$6,842.88 \text{ pay adjustment to Design-Builder}$$

Sample Calculation of a Price Adjustment (decrease)

Project bid on April 28, 2004.

Project has 450,000 lb. of eligible structural steel.

Design-Builder's *f.o.b. supplier price for structural steel submitted in the Price Proposal is \$0.2816 per pound.

*free on board

Adjusted BLS Producers Price Index (PPI) most recently published average at time of opening of the Price Proposal is 156.6.

All eligible steel shipped to fabricator in same month, October 2004.

Adjusted BLS Producers Price Index (PPI) most recently published average for month of October is 136.3

Adjustment formula is as follows:

$$A = B \times P \times Q$$

- Where;
- A = Steel price adjustment in lump sum dollars
 - B = Average weighted price of steel submitted in the Price Proposal for Design-Build project in \$ per pound
 - P = Adjusted percentage change in PPI average from shipping date to date of submitted Price Proposal minus 10% (0.10) threshold
 - Q = Total quantity of eligible steel shipped to fabricator in October 2004 for this project in pounds

$$B = \$0.2816$$

$$P = (156.6 - 136.3) / 156.6 - 0.10 = 0.030$$

$$Q = 450,000 \text{ lb.}$$

$$A = 0.2816 \times 0.030 \times 450,000$$

$$A = \$3,801.60 \text{ credit to Department}$$

MASTER LISTING

STANDARD BID ITEMS ELIGIBLE FOR STEEL PRICE ADJUSTMENT

March 18, 2009

BLS Series I. D.

ITEM NUMBER	ITEM DESCRIPTION	UNITS	Number WPU used in \$ adjust.
00519	SHEET PILE, STEEL	SF	avg. 1017 & 101
00540	REINF. STEEL	LB	101704
00542	EPOXY COATED REINF. STEEL	LB	101704
00560	STRUCTURAL STEEL JB-1	LB	avg. 1017 & 101
11030	REINF. STEEL BRIDGE APPR. SLAB	LB	101704
11181	PATCH.HYDR.CEM.CONC. PAVE.	SY	101704
13290	GUARDRAIL GR-8 (NCHRP 350 TL-3)	LF	avg. 1017 & 101
13292	GUARDRAIL GR-8A (NCHRP 350 TL-3)	LF	avg. 1017 & 101
13294	GUARDRAIL GR-8B (NCHRP 350 TL-3)	LF	avg. 1017 & 101
13310	GUARDRAIL TERMINAL GR-6 (NCHRP 350)	LF	avg. 1017 & 101
13320	GUARDRAIL GR-2	LF	avg. 1017 & 101
13323	GUARDRAIL GR-2A	LF	avg. 1017 & 101
13331	RAD. GUARDRAIL GR-2	LF	avg. 1017 & 101
13333	RAD. GUARDRAIL GR-2A	LF	avg. 1017 & 101
13335	GUARDRAIL GR-3	LF	avg. 1017 & 101
13341	GUARDRAIL TER. GR-6(WEATHERING STEEL	LF	avg. 1017 & 101
13351	GUARDRAIL GR-8	LF	avg. 1017 & 101
13352	GUARDRAIL GR-8A	LF	avg. 1017 & 101
13353	GUARDRAIL GR-8B	LF	avg. 1017 & 101
13355	GUARDRAIL GR-10	LF	avg. 1017 & 101
13421	MEDIAN BARRIER MB-3	LF	avg. 1017 & 101
13450	MEDIAN BARRIER MB-5	LF	avg. 1017 & 101
13451	MEDIAN BARRIER MB-5A	LF	avg. 1017 & 101
13452	MEDIAN BARRIER MB-5B	LF	avg. 1017 & 101
13545	REINF. STEEL	LB	101704
14502	REINFORCING STEEL	LB	101704
15290	PATCH.CEM.CONC.PAVE.TY.CRCP-A	SY	101704
15302	PATCH.CEM.CONC.PAVE. TY. II	SY	101704
15305	PATCH.CEM.CONC.PAVE.TY. IV-A	SY	101704
17323	GUARDRAIL BEAM *	LF	avg. 1017 & 101
17325	RADIAL GUARDRAIL BEAM *	LF	avg. 1017 & 101
17327	RUB RAIL	LF	avg. 1017 & 101
17353	CABLE GR-3	LF	avg. 1017 & 101
17521	GUARDRAIL BEAM (WEATHERING STEEL)	LF	avg. 1017 & 101
17523	RADIAL GUARDRAIL BEAM (WEATHERING STEEL)	LF	avg. 1017 & 101
17525	RUB RAIL (WEATHERING STEEL)	LF	avg. 1017 & 101
22501	FENCE FE-W1	LF	avg. 1017 & 101
22643	FENCE FE-CL	LF	avg. 1017 & 101
22645	FENCE FE-CL VINYL COATED	LF	avg. 1017 & 101
23043	WATER GATE FE-4 TY.III	LF	avg. 1017 & 101
23501	FENCE FE-W1 (FABRIC ONLY)	LF	avg. 1017 & 101
45522	4" STEEL ENCASE. PIPE	LF	101706
45532	6" STEEL ENCASE. PIPE	LF	101706
45562	16" STEEL ENCASE. PIPE	LF	101706
45572	18" STEEL ENCASE. PIPE	LF	101706

45582	24" STEEL ENCASE. PIPE	LF	101706
45584	24" JACKED STEEL ENCASMENT PIPE	LF	101706
45592	30" STEEL ENCASE. PIPE	LF	101706
50402	SIGN POST STEEL 3"	LF	101706
50404	SIGN POST STEEL 4"	LF	101706
50406	SIGN POST STEEL 6"	LF	101706
50410	SIGN POST STEEL 10"	LF	101706
50412	SIGN POST STEEL 12"	LF	101706
50414	SIGN POST STEEL 14"	LF	101706
50416	SIGN POST STEEL 16"	LF	101706
50418	SIGN POST STEEL 18"	LF	101706
51317	SIG. POLE MP-1 20' ONE ARM 30'	EA	101706
51319	SIG. POLE MP-1 20' ONE ARM 32'	EA	101706
51325	SIG. POLE MP-1 20' ONE ARM 38'	EA	101706
51327	SIG. POLE MP-1 20' ONE ARM 40'	EA	101706
51329	SIG. POLE MP-1 20' ONE ARM 42'	EA	101706
51331	SIG. POLE MP-1 20' ONE ARM 44'	EA	101706
51337	SIG. POLE MP-1 20' ONE ARM 50'	EA	101706
51339	SIG. POLE MP-1 20' ONE ARM 52'	EA	101706
51341	SIG. POLE MP-1 20' ONE ARM 54'	EA	101706
51344	SIG. POLE MP-1 20' ONE ARM 56'	EA	101706
51346	SIG. POLE MP-1 20' ONE ARM 58'	EA	101706
51347	SIG. POLE MP-1 20' ONE ARM 60'	EA	101706
51348	SIG. POLE MP-1 20' ONE ARM 62'	EA	101706
51368	SIG. POLE MP-1 20'TWO ARMS 36' & 42'	EA	101706
51400	SIG. POLE MP-1 CO. LU. ONE ARM 38	EA	101706
51402	SIG. POLE MP-1 CO. LU. ONE ARM 40	EA	101706
51408	SIG. POLE MP-1 CO. LU. ONE ARM 46	EA	101706
51412	SIG. POLE MP-1 CO. LU. ONE ARM 50	EA	101706
51414	SIG. POLE MP-1 CO. LU. ONE ARM 52	EA	101706
51416	SIG. POLE MP-1 CO. LU. ONE ARM 54	EA	101706
51418	SIG. POLE MP-1 CO. LU. ONE ARM 56	EA	101706
51420	SIG. POLE MP-1 CO. LU. ONE ARM 58	EA	101706
51422	SIG. POLE MP-1 CO. LU. ONE ARM 60	EA	101706
55162	LIGHTING POLE LP-1 30'-4'	EA	101706
55163	LIGHTING POLE LP-1 30'-6'	EA	101706
55166	LIGHTING POLE LP-1 30'-12'	EA	101706
55169	LIGHTING POLE LP-1 35'-6'	EA	101706
55171	LIGHTING POLE LP-1 35'-10'	EA	101706
55176	LIGHTING POLE LP-1 40'-8'	EA	101706
55185	LIGHTING POLE LP-2 TYPE A	EA	101706
55186	LIGHTING POLE LP-2 TYPE B	EA	101706
55187	LIGHTING POLE LP-2 TYPE C	EA	101706
55188	LIGHTING POLE LP-2 TYPE D	EA	101706
55189	LIGHTING POLE LP-2 TYPE E	EA	101706
55190	LIGHTING POLE LP-2 TYPE F	EA	101706
55192	LIGHTING POLE LP-2 TYPE H	EA	101706
60452	REINF. STEEL BRIDGE APPR. SLAB	LB	101704
61700	REINF. STEEL	LB	101704
61704	CORROSION RESISTANT REINF. STEEL	LB	101704
61705	EPOXY COATED REINF. STEEL	LB	101704
61750	STRUCT. STEEL HIGH STRG. PLT. GIRDERS	LB	avg. 1017 & 101
61811	STR. STEEL PLATE GIRDER ASTM A709 GRADE50	LB	avg. 1017 & 101
61812	STR. STEEL PLATE GIRDER ASTM A709 GRADE50	LB	avg. 1017 & 101
61813	STR. STEEL PLATE GIRDER ASTM A709 GRADEHPS50W	LB	avg. 1017 & 101
61814	STR. STEEL PLATE GIRDER ASTM A709 GRADEHPS70W	LB	avg. 1017 & 101
61820	STR. STEEL ROLLED BEAM ASTM A709 GRADE 36	LB	avg. 1017 & 101

61821	STR.STEEL ROLLED BEAM ASTM A709 GRADE50	LB	avg. 1017 & 101
61822	STR.STEEL ROLLED BEAM ASTM A709 GRADE50W	LB	avg. 1017 & 101
61990	STEEL GRID FLOOR	SF	avg. 1017 & 101
64110	STEEL PILES 10"	LF	avg. 1017 & 101
64112	STEEL PILES 12"	LF	avg. 1017 & 101
64114	STEEL PILES 14"	LF	avg. 1017 & 101
64768	DRIVING TEST FOR 12" STEEL PILE	LF	avg. 1017 & 101
64778	DRIVING TEST FOR 14" STEEL PILE	LF	avg. 1017 & 101
65200	REINF. STEEL	LB	101704
65204	CORROSION RESISTANT REINF. STEEL	LB	101704
65205	EPOXY COATED REINF. STEEL	LB	101704
67086	PED. FENCE 6'	LF	avg. 1017 & 101
67088	PED. FENCE 8'	LF	avg. 1017 & 101
67089	PED. FENCE 10'	LF	avg. 1017 & 101
68100	REINF. STEEL	LB	101704
68104	CORROSION RESISTANT REINF. STEEL	LB	101704
68105	EPOXY COATED REINF. STEEL	LB	101704
68107	STR.STEEL PLATE GIRDER ASTM A709 GRADE50	LB	avg. 1017 & 101
68108	STR. STEEL PLATE GIRDER ASTM A709 GR50W	LB	avg. 1017 & 101
68109	STR. STEEL PLATE GIRDER ASTM A709 GR.HPS50W	LB	avg. 1017 & 101
68110	STR. STEEL PLATE GIRDER ASTM A709 GR.HPS70W	LB	avg. 1017 & 101
68112	STR.STEEL ROLLED BEAM ASTM A709 GR.36	LB	avg. 1017 & 101
68113	STR.STEEL ROLLED BEAM ASTM A709 GR.50	LB	avg. 1017 & 101
68114	STR.STEEL ROLLED BEAM ASTM A709 GR. 50W	LB	avg. 1017 & 101
68115	STRUCT. STEEL	LB	avg. 1017 & 101
68270	REINF. STEEL BRIDGE APPR. SLAB	LB	101704
69060	SHEET PILES, STEEL	SF	avg. 1017 & 101
69100	REINF. STEEL	LB	101704
69104	CORROSION RESISTANT REINF. STEEL	LB	101704
69105	EPOXY COATED REINF. STEEL	LB	101704
69110	STEEL PILES 10"	LF	avg. 1017 & 101
69112	STEEL PILE 12"	LF	avg. 1017 & 101
69113	DRIVING TEST FOR 12" STEEL PILE	LF	avg. 1017 & 101

I elect to use this provision

I elect not to use this provision

Date: March 12, 2014

Signature: 

Design-Builder: Francis O. Day Company, Inc.

Vendor No.: D499

4.3.3

Proposal Guaranty

COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION
PROPOSAL GUARANTY

KNOW ALL MEN BY THESE PRESENTS, THAT WE Francis O. Day Company, Inc. As principal, and Federal Insurance Company Surety, are held and firmly bound unto the Commonwealth of Virginia as obligee, in the amount of FIVE PERCENT OF THE DOLLAR VALUE OF THE BID, lawful money of the United States of America, for the payment of which, well and truly to be made, we bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally and firmly by these presents.

SIGNED, sealed and dated this 6th Day of March, 20 14

WHEREAS, the above said principal is herewith submitting its proposal for:

PROJECT NUMBER: State Project No.: (FO) 0495-029-123, P101, C501 I-495 Northern Section Shoulder Use
Federal Project No.: STP-495-5(094) Contract ID Number: C00105130DB72

NOW, THEREFORE, the condition of the above obligee is such, that if the aforesaid principal shall be awarded the contract upon said proposal and shall within the time specified in the Specifications after the notice of such award enter into a contract and give bond for the faithful performance of the contract, then this obligation shall be null and void; otherwise to remain in full force and effect and the principal and surety will pay unto the obligee the difference in money between the amount of the bid of the said principal and the amount for which the obligee may legally contract with another party to perform the said work if the latter amount be in excess of the former; but in no event shall the liability exceed the penal sum hereof.

Francis O. Day Company, Inc.
(Principal*)
By: [Signature] CFO
(Officer, Partner or Owner) (Seal)

Federal Insurance Company
(Surety Company)
By: Laurie L. Kessler
Laurie L. Kessler (Attorney-in-Fact**) (Seal)

(Principal*)
By: [Signature]
(Officer, Partner or Owner) (Seal)

15 Mountain View Rd. Warren, NJ 07059
(Address)

By: _____
(Surety Company)

(Principal*)
By: _____
(Officer, Partner or Owner) (Seal)

(Attorney-in-Fact**) (Seal)
By: _____
(Address)

*Note: If the principal is a joint venture, each party thereof must be named and execution made by same hereon. If there is more than one surety to the bid bond, each surety must be named and execution shall be made by same hereon.

Electronic Bid Only: In lieu of completing the above section of the Contract Performance Bond, the Principal shall file an Electronic Bid Bond when bidding electronically. By signing below the Principal is ensuring the identified electronic bid bond has been executed and the Principal and Surety are firmly bound unto the Commonwealth of Virginia under the same conditions of the bid bond as shown above.

Electronic Bid Bond ID# _____ Company/Bidder Name _____ Signature and Title _____

**Attach copy of Power of Attorney



Chubb
Surety

POWER
OF
ATTORNEY

Federal Insurance Company
Vigilant Insurance Company
Pacific Indemnity Company

Attn: Surety Department
15 Mountain View Road
Warren, NJ 07059

Know All by These Presents, That FEDERAL INSURANCE COMPANY, an Indiana corporation, VIGILANT INSURANCE COMPANY, a New York corporation, and PACIFIC INDEMNITY COMPANY, a Wisconsin corporation, do each hereby constitute and appoint Thomas Evans Carroll, Salvatore Joseph DiPietro, Laurie L. Kessler, Patrick W. Tracy and Jochen Wurfl of Baltimore, Maryland

each as their true and lawful Attorney-in-Fact to execute under such designation in their names and to affix their corporate seals to and deliver for and on their behalf as surety thereon or otherwise, bonds and undertakings and other writings obligatory in the nature thereof (other than bail bonds) given or executed in the course of business, and any instruments amending or altering the same, and consents to the modification or alteration of any instrument referred to in said bonds or obligations.

In Witness Whereof, said FEDERAL INSURANCE COMPANY, VIGILANT INSURANCE COMPANY, and PACIFIC INDEMNITY COMPANY have each executed and attested these presents and affixed their corporate seals on this 21st day of August, 2013.

Dawn M. Chloros, Assistant Secretary

David B. Norris, Jr., Vice President

STATE OF NEW JERSEY

ss.

County of Somerset

On this 21st day of August, 2013 before me, a Notary Public of New Jersey, personally came Dawn M. Chloros, to me known to be Assistant Secretary of FEDERAL INSURANCE COMPANY, VIGILANT INSURANCE COMPANY, and PACIFIC INDEMNITY COMPANY, the companies which executed the foregoing Power of Attorney, and the said Dawn M. Chloros, being by me duly sworn, did depose and say that she is Assistant Secretary of FEDERAL INSURANCE COMPANY, VIGILANT INSURANCE COMPANY, and PACIFIC INDEMNITY COMPANY and knows the corporate seals thereof, that the seals affixed to the foregoing Power of Attorney are such corporate seals and were thereto affixed by authority of the By-Laws of said Companies; and that she signed said Power of Attorney as Assistant Secretary of said Companies by like authority, and that she is acquainted with David B. Norris, Jr., and knows him to be Vice President of said Companies; and that the signature of David B. Norris, Jr., subscribed to said Power of Attorney is in the genuine handwriting of David B. Norris, Jr., and was thereto subscribed by authority of said By-Laws and in deponent's presence.

Notarial Seal



KATHERINE J. ADELAAR
NOTARY PUBLIC OF NEW JERSEY
No 2316685
Commission Expires July 16, 2014

Notary Public

CERTIFICATION

Extract from the By-Laws of FEDERAL INSURANCE COMPANY, VIGILANT INSURANCE COMPANY, and PACIFIC INDEMNITY COMPANY:

"All powers of attorney for and on behalf of the Company may and shall be executed in the name and on behalf of the Company, either by the Chairman or the President or a Vice President or an Assistant Vice President, jointly with the Secretary or an Assistant Secretary, under their respective designations. The signature of such officers may be engraved, printed or lithographed. The signature of each of the following officers: Chairman, President, any Vice President, any Assistant Vice President, any Secretary, any Assistant Secretary and the seal of the Company may be affixed by facsimile to any power of attorney or to any certificate relating thereto appointing Assistant Secretaries or Attorneys-in-Fact for purposes only of executing and attesting bonds and undertakings and other writings obligatory in the nature thereof, and any such power of attorney or certificate bearing such facsimile signature or facsimile seal shall be valid and binding upon the Company and any such power so executed and certified by such facsimile signature and facsimile seal shall be valid and binding upon the Company with respect to any bond or undertaking to which it is attached."

I, Dawn M. Chloros, Assistant Secretary of FEDERAL INSURANCE COMPANY, VIGILANT INSURANCE COMPANY, and PACIFIC INDEMNITY COMPANY (the "Companies") do hereby certify that

- (i) the foregoing extract of the By-Laws of the Companies is true and correct,
- (ii) the Companies are duly licensed and authorized to transact surety business in all 50 of the United States of America and the District of Columbia and are authorized by the U.S. Treasury Department; further, Federal and Vigilant are licensed in the U.S. Virgin Islands, and Federal is licensed in American Samoa, Guam, Puerto Rico, and each of the Provinces of Canada except Prince Edward Island; and
- (iii) the foregoing Power of Attorney is true, correct and in full force and effect.

Given under my hand and seals of said Companies at Warren, NJ this

06 March 2014



Dawn M. Chloros, Assistant Secretary

IN THE EVENT YOU WISH TO NOTIFY US OF A CLAIM, VERIFY THE AUTHENTICITY OF THIS BOND OR NOTIFY US OF ANY OTHER MATTER, PLEASE CONTACT US AT ADDRESS LISTED ABOVE, OR BY Telephone (908) 903-3493 Fax (908) 903-3655 e-mail: surety@chubb.com



4.3.4

Sworn Statement Forms



**COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION**

PROJECT: (FO) 0495-029-123, P101, C501

FHWA: STP-495-5(094)

This form must be completed, signed and returned with bid; and failure to do so may result in the rejection of your bid. **THE CONTRACTOR SHALL AFFIRM THE FOLLOWING STATEMENT EITHER BY SIGNING THE AFFIDAVIT AND HAVING IT NOTARIZED OR BY SIGNING THE UNSWORN DECLARATION UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE UNITED STATES. A SEPARATE FORM MUST BE SUBMITTED BY EACH PRINCIPAL OF A JOINT VENTURE BID.**

STATEMENT. In preparation and submission of this bid, I, the firm, corporation or officers, agents or employees thereof did not, either directly or indirectly, enter into any combination or arrangement with any persons, firm or corporation or enter into any agreement, participate in any collusion, or otherwise take any action in the restraint of free, competitive bidding in violation of the Sherman Act (15 U.S.C. Section 1) or Article 1.1 or Chapter 12 of Title 18.2 (Virginia Governmental Frauds Act), Sections 59.1-9.1 through 59.1-9.17 or Sections 59.1-68.6 through 59.1-68.8 of the Code of Virginia.

AFFIDAVIT

The undersigned is duly authorized by the bidder to make the foregoing statement to be filed with bids submitted on behalf of the bidder for contracts to be let by the Commonwealth Transportation Board.

Signed at Rockville Maryland, this 28 day of February, 20 14
County (City), STATE

Francis O. Day Co., Inc.
(Name of Firm)

By: [Signature] Vice President
(Signature) Title (print)

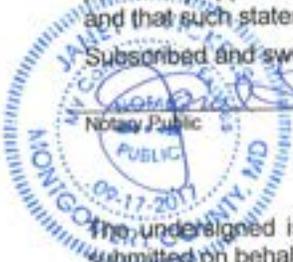
STATE of Maryland

COUNTY (CITY) of Montgomery
To-wit:

I Janet L. Vickers, a Notary Public in and for the State and County(City) aforesaid, hereby certify that this day Francis O. Day IV

personally appeared before me and made oath that he is duly authorized to make the above statements and that such statements are true and correct.

Subscribed and sworn to before me this 28 day of February, 20 14
[Signature] My Commission expires September 17, 2017



**OR
UNSWORN DECLARATION**

The undersigned is duly authorized by the bidder to make the foregoing statement to be filed with bids submitted on behalf of the bidder for contracts to be let by the Commonwealth Transportation Board.

Signed at _____, this _____ day of _____, 20 _____
County (City), STATE

By: _____
(Name of Firm) (Signature) Title (print)

3. The bidder certifies to the best of its knowledge and belief, that it and its principals:
- (a) Are not presently debarred, suspended, proposed for debarment, declared ineligible or voluntarily excluded from covered transactions by any Federal department or agency;
 - (b) Have not within a three year period preceding this proposal been convicted of or had a civil judgement rendered against them for commission of fraud or a criminal offence in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
 - (c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated above; and
 - (d) Where the bidders is unable to certify to any of the statements in this certification, the bidder shall show an explanation below.

Explanations will not necessarily result in denial of award, but will be considered in determining bidder responsibility. For any explanation noted, indicate below to whom it applies, initiating agency, and dates of action. Providing false information may result in federal criminal prosecution or administration sanctions. The bidder shall provide immediate written notice to the Department if at any time the bidder learns that its certification was erroneous when submitted or has become erroneous by reason of change circumstances.

The undersigned is duly authorized by the bidder to make the foregoing statements to be filed with bids submitted on behalf of the bidder for contracts to be let by the Commonwealth Transportation Board.

Signed at Rockville, Maryland, this 28th day of February, 20 14
County (City), STATE

Francis O. Day Co., Inc. By: [Signature] vice President
(Name of Firm) (Signature) Title (print)

STATE of Maryland COUNTY (CITY) of Montgomery
To-wit:

I Janet L. Vickers, a Notary Public in and for the State and
County(City) aforesaid, hereby certify that this day Francis O. Day IV

personally appeared before me and made oath that he is duly authorized to make the above statements and that such statements are true and correct.

Subscribed and sworn to before me this 28th day of February, 20 14

[Signature] My Commission expires September 17, 2017

Notary Public



4.3.5

DBE Requirement Forms

COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION
CERTIFICATION OF BINDING AGREEMENT
WITH
DISADVANTAGED BUSINESS ENTERPRISE FIRMS

Project No.:

Federal Project No.:

This form is to be submitted in accordance with the Department's Special Provision for Section 107.15.

It is hereby certified by the below signed Contractors that there exists a written quote, acceptable to the parties involved preliminary to a binding subcontract agreement stating the details concerning the work to be performed and the price which will be paid for the aforementioned work. This document is not intended to, nor should it be construed to, contain the entire text of the agreement between the contracting parties. This document does not take the place of, nor may it be substituted for, an official subcontracting agreement in those situations that may require such an agreement. A copy of the fully executed *subcontract agreement* shall be submitted to the Engineer within fourteen (14) business days after contract execution.

It is further certified that the aforementioned mutually acceptable quote and fully executed subcontract agreement represent the entire agreement between the parties involved and that no conversations, verbal agreements, or other forms of non-written representations shall serve to add to, delete, or modify the terms as stated.

The prime Contractor further represents that the aforementioned mutually acceptable quote and fully executed subcontract agreement shall remain on file for a period of not less than one year following completion of the prime's contract with the Department or for such longer period as provisions of governing Federal or State law or regulations may require. For purposes of this form, the term Prime Contractor shall refer to any Contractor utilizing a DBE subcontractor, regardless of tier, in which they are claiming DBE credit toward the contract goal.

Contractors further jointly and severally represent that said binding agreement is for the performance of a "commercially useful function" as that term is employed in 49 C.F.R. Part 26.55 (c), (d).

**TO BE SIGNED BY THE SUBCONTRACTOR TO THE PRIME CONTRACTOR, AND ANY LOWER TIER
SUBCONTRACTORS HAVING A CONTRACT WITH THE BELOW NAMED DBE FIRM**

Prime Contractor Francis O. Day Company, Inc.

By:  Vice President
Signature Title
Date: March 12, 2014

First Tier Subcontractor if Applicable Johnson, Mirmiran & Thompson, Inc.

By:  Vice President
Signature Title
Date: March 12, 2014

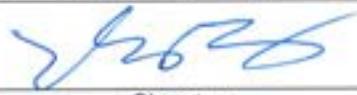
Second Tier
Subcontractor if
Applicable

By: _____ Signature _____ Title
Date: _____

Third Tier
Subcontractor if
Applicable

By: _____ Signature _____ Title
Date: _____

DBE Contractor DMY Engineering Consultants Inc.

By:  _____ Signature _____ Title
Date: 2/27/2014

COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION
CERTIFICATION OF BINDING AGREEMENT
WITH
DISADVANTAGED BUSINESS ENTERPRISE FIRMS

Project No.:

Federal Project No.:

This form is to be submitted in accordance with the Department's Special Provision for Section 107.15.

It is hereby certified by the below signed Contractors that there exists a written quote, acceptable to the parties involved preliminary to a binding subcontract agreement stating the details concerning the work to be performed and the price which will be paid for the aforementioned work. This document is not intended to, nor should it be construed to, contain the entire text of the agreement between the contracting parties. This document does not take the place of, nor may it be substituted for, an official subcontracting agreement in those situations that may require such an agreement. A copy of the fully executed *subcontract agreement* shall be submitted to the Engineer within fourteen (14) business days after contract execution.

It is further certified that the aforementioned mutually acceptable quote and fully executed subcontract agreement represent the entire agreement between the parties involved and that no conversations, verbal agreements, or other forms of non-written representations shall serve to add to, delete, or modify the terms as stated.

The prime Contractor further represents that the aforementioned mutually acceptable quote and fully executed subcontract agreement shall remain on file for a period of not less than one year following completion of the prime's contract with the Department or for such longer period as provisions of governing Federal or State law or regulations may require. For purposes of this form, the term Prime Contractor shall refer to any Contractor utilizing a DBE subcontractor, regardless of tier, in which they are claiming DBE credit toward the contract goal.

Contractors further jointly and severally represent that said binding agreement is for the performance of a "commercially useful function" as that term is employed in 49 C.F.R. Part 26.55 (c), (d).

**TO BE SIGNED BY THE SUBCONTRACTOR TO THE PRIME CONTRACTOR, AND ANY LOWER TIER
SUBCONTRACTORS HAVING A CONTRACT WITH THE BELOW NAMED DBE FIRM**

Prime Contractor Francis O. Day Company, Inc.

By:  Vice President
Signature Title
Date: March 12, 2014

First Tier Subcontractor if Applicable Johnson, Mirmiran & Thompson, Inc.

By:  Vice President
Signature Title
Date: March 12, 2014

Second Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

Third Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

DBE Contractor

Athavale, Lystad & Associates, Inc.

By: Timothy Dye
Signature President
Date: March 12, 2014
Title

COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION
CERTIFICATION OF BINDING AGREEMENT
WITH
DISADVANTAGED BUSINESS ENTERPRISE FIRMS

Project No.: I-495 Shoulder Widening Design Build Project
(FO) 0495-029-123, P101, CS01
Federal Project No.: STP-495-5(09A)

This form is to be submitted in accordance with the Department's Special Provision for Section 107.15.

It is hereby certified by the below signed Contractors that there exists a written quote, acceptable to the parties involved preliminary to a binding subcontract agreement stating the details concerning the work to be performed and the price which will be paid for the aforementioned work. This document is not intended to, nor should it be construed to, contain the entire text of the agreement between the contracting parties. This document does not take the place of, nor may it be substituted for, an official subcontracting agreement in those situations that may require such an agreement. A copy of the fully executed subcontract agreement shall be submitted to the Engineer within fourteen (14) business days after contract execution.

It is further certified that the aforementioned mutually acceptable quote and fully executed subcontract agreement represent the entire agreement between the parties involved and that no conversations, verbal agreements, or other forms of non-written representations shall serve to add to, delete, or modify the terms as stated.

The prime Contractor further represents that the aforementioned mutually acceptable quote and fully executed subcontract agreement shall remain on file for a period of not less than one year following completion of the prime's contract with the Department or for such longer period as provisions of governing Federal or State law or regulations may require. For purposes of this form, the term Prime Contractor shall refer to any Contractor utilizing a DBE subcontractor, regardless of tier, in which they are claiming DBE credit toward the contract goal.

Contractors further jointly and severally represent that said binding agreement is for the performance of a "commercially useful function" as that term is employed in 49 C.F.R. Part 26.55 (c), (d).

TO BE SIGNED BY THE SUBCONTRACTOR TO THE PRIME CONTRACTOR, AND ANY LOWER TIER
SUBCONTRACTORS HAVING A CONTRACT WITH THE BELOW NAMED DBE FIRM

Prime Contractor F.O Day

By:  Signature Vice President Title
Date: March 14, 2014

First Tier Subcontractor if Applicable _____

By: _____ Signature _____ Title _____
Date: _____

Second Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

Third Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

DBE Contractor

Quinn Consulting Services, Inc. _____

By:  _____
Signature Title
Date: 3/6/2014 _____

COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION
CERTIFICATION OF BINDING AGREEMENT
WITH

DISADVANTAGED BUSINESS ENTERPRISE FIRMS

Project No.: (F0) 0495-029-123, P101, C501

Federal Project No.: STP-495-5 (094)

This form is to be submitted in accordance with the Department's Special Provision for Section 107.15.

It is hereby certified by the below signed Contractors that there exists a written quote, acceptable to the parties involved preliminary to a binding subcontract agreement stating the details concerning the work to be performed and the price which will be paid for the aforementioned work. This document is not intended to, nor should it be construed to, contain the entire text of the agreement between the contracting parties. This document does not take the place of, nor may it be substituted for, an official subcontracting agreement in those situations that may require such an agreement. A copy of the fully executed *subcontract agreement* shall be submitted to the Engineer within fourteen (14) business days after contract execution.

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Contractors further jointly and severally represent that said binding agreement is for the performance of a "commercially useful function" as that term is employed in 49 C.F.R. Part 26.55 (c), (d).

**TO BE SIGNED BY THE SUBCONTRACTOR TO THE PRIME CONTRACTOR, AND ANY LOWER TIER
SUBCONTRACTORS HAVING A CONTRACT WITH THE BELOW NAMED DBE FIRM**

Prime Contractor Francis O. Day Company, Inc.

By:  Vice President
Signature Title
Date: March 12, 2014

First Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

Second Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

Third Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

DBE Contractor

Quarry Products "Unlimited", Inc
By: *Debra L. Speyer* *President*
Signature Title
Date: *February 28, 2014*

FO Day - Robt

COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION
CERTIFICATION OF BINDING AGREEMENT
WITH
DISADVANTAGED BUSINESS ENTERPRISE FIRMS

Project No.:

Federal Project No.:

This form is to be submitted in accordance with the Department's Special Provision for Section 107.15.

It is hereby certified by the below signed Contractors that there exists a written quote, acceptable to the parties involved preliminary to a binding subcontract agreement stating the details concerning the work to be performed and the price which will be paid for the aforementioned work. This document is not intended to, nor should it be construed to, contain the entire text of the agreement between the contracting parties. This document does not take the place of, nor may it be substituted for, an official subcontracting agreement in those situations that may require such an agreement. A copy of the fully executed *subcontract agreement* shall be submitted to the Engineer within fourteen (14) business days after contract execution.

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**TO BE SIGNED BY THE SUBCONTRACTOR TO THE PRIME CONTRACTOR, AND ANY LOWER TIER
SUBCONTRACTORS HAVING A CONTRACT WITH THE BELOW NAMED DBE FIRM**

Prime Contractor Francis O. Day Company, Inc.

By:  Vice President
Signature Title
Date: March 12, 2014

First Tier Subcontractor if Applicable _____

By: _____ Signature Title
Date: _____

Second Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

Third Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

DBE Contractor

Aggregate Relocation Specialists, LLC

By: _____
Signature Title
Date: March 12, 2014

COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION
CERTIFICATION OF BINDING AGREEMENT
WITH
DISADVANTAGED BUSINESS ENTERPRISE FIRMS

Project No.: (FO) 0495-029-123,P101, C501

Federal Project No.: STP-495-5(094)

This form is to be submitted in accordance with the Department's Special Provision for Section 107.15.

It is hereby certified by the below signed Contractors that there exists a written quote, acceptable to the parties involved preliminary to a binding subcontract agreement stating the details concerning the work to be performed and the price which will be paid for the aforementioned work. This document is not intended to, nor should it be construed to, contain the entire text of the agreement between the contracting parties. This document does not take the place of, nor may it be substituted for, an official subcontracting agreement in those situations that may require such an agreement. A copy of the fully executed *subcontract agreement* shall be submitted to the Engineer within fourteen (14) business days after contract execution.

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Contractors further jointly and severally represent that said binding agreement is for the performance of a "commercially useful function" as that term is employed in 49 C.F.R. Part 26.55 (c), (d).

**TO BE SIGNED BY THE SUBCONTRACTOR TO THE PRIME CONTRACTOR, AND ANY LOWER TIER
SUBCONTRACTORS HAVING A CONTRACT WITH THE BELOW NAMED DBE FIRM**

Prime Contractor Francis O. Day Company, Inc.

By:  Signature Vice President Title
Date: March 12, 2014

First Tier Subcontractor if Applicable Midasco, LLC

By:  Signature President Title
Date: 2/27/2014

Second Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

Third Tier
Subcontractor if
Applicable

By: _____
Signature Title
Date: _____

DBE Contractor

OLD DOMINION ELECTRICAL SUPPLY

By: [Signature] PRESIDENT
Signature Title
Date: MARCH 6, 2014

4.4.6

Schedule of Items

Attachment 4.4.6
State Project 0620-029-017, P101, R201, C501

SCHEDULE OF ITEMS

This Schedule of Items shall identify the total material quantities and costs of each proposed pay item, using item codes and units of measure that are consistent with VDOT's list of standard and non-standard item codes. Any pay items considered for price adjustments shall be identified. The values and quantities shall be clearly supported by the escrowed pricing documents.

Contractor:

Date: March 14, 2014

VDOT Item Code ¹	Item Description	Fuel (F) or Price (P) Adjustment	App Quantity	Unit ¹	Budgeted Cost (\$)
30	00104	DESIGN SERVICES	1	LS	\$ 1,123,000.00
30	00100	MOBILIZATION (CONSTRUCTION)	1	LS	\$ 950,000.00
50	25593	QUALITY ASSURANCE	1	LS	\$ 300,000.00
70	25593	QUALITY CONTROL	1	LS	\$ 350,000.00
100	51910	SAWCUT	9,607	LF	\$ 43,231.50
110	24410	DEMOLITION OF PAVEMENT	12,903	SY	\$ 245,157.00
120	00120	REGULAR EXCAVATION	3,715	CY	\$ 111,450.00
140	00110	CLEARING AND GRUBBING	1	AC	\$ 22,000.00
150	NC	STAB CONST ENTRANCE	600	TONS	\$ 27,600.00
160	27410	CHECK DAM ROCK TY I	14	EA	\$ 8,680.00
170	27451	INLET PROTECTION	47	EA	\$ 7,990.00
180	27505	TEMP SILT FENCE	4,200	LF	\$ 7,140.00
190	68572	EXPANSION DAM RECONSTRUCTION	180	LF	\$ 37,800.00
200	NC	REMOVE EXIST GLARESCREEN	9,607	LF	\$ 26,899.60
210	17391 S	GLARE SCREEN <2'	6,007	LF	\$ 126,147.00
220	17391 S	GLARE SCREEN 2-3'	1,800	LF	\$ 41,400.00
230	17391 S	GLARE SCREEN 3-4'	1,800	LF	\$ 45,000.00
240	24600	REMOVE GUARDRAIL	1,000	LF	\$ 4,200.00
250	13320	GUARDRAIL GR-2	2,175	LF	\$ 43,500.00
260	13345	GUARDRAIL GR-9	3	EA	\$ 7,800.00
270	13392	GUARDRAIL FOA-2 TY 1	5	EA	\$ 12,500.00
280	54105	ERADICATE EXIST PAV MARK	29,257	LF	\$ 87,182.70
290	NC	REMOVE EXIST PAV MARK LENSES	360	EA	\$ 1,188.00
300	54210	REMOVE EXIST RAISED PAV MARKERS	360	EA	\$ 3,960.00
310	54055	TY B CL VI PAV MARK 4"	106	LF	\$ 349.80
320	54056	TY B CL VI PAV MARK 6"	33,749	LF	\$ 155,245.40
330	54057	TY B CL VI PAV MARK 8"	1,913	LF	\$ 12,243.20
340	54059	TY B CL VI PAV MARK 12"	675	LF	\$ 6,075.00
350	54042	TY B CL I PAV MARK 24"	254	LF	\$ 4,826.00
360	54217	SNOW PLOWABLE PAV MARKERS	360	EA	\$ 10,080.00
370	27102 S	PERMANENT SEEDING	20,000	SY	\$ 16,000.00
390	51910	SAWCUT	7,539	LF	\$ 33,925.50
400	24410	DEMOLITION OF PAVEMENT	2,557	SY	\$ 53,697.00
410	01152	15" RC PIPE CL 3	880	LF	\$ 53,680.00
420	01182	18" RC PIPE CL 3	340	LF	\$ 22,440.00
430	01242	24" RC PIPE CL 3	765	LF	\$ 58,140.00
440	NC	CONVERT INLET TO JUNCTION BOX	7	EA	\$ 31,500.00
450	NC	ABANDON INLET	4	EA	\$ 5,600.00
460	NC	ABANDON PIPE	195	LF	\$ 10,920.00
470	NC	REMOVE PIPE	695	LF	\$ 12,510.00
480	NC	REMOVE INLET	6	EA	\$ 12,000.00
490	08252	INLET DI-10K L=8'	2	EA	\$ 14,600.00
500	08253	INLET DI-10K L=10'	3	EA	\$ 24,300.00
510	08254	INLET DI-10K L=12'	10	EA	\$ 89,000.00
520	08255	INLET DI-10K L=14'	2	EA	\$ 19,200.00
530	08256	INLET DI-10K L=16'	3	EA	\$ 30,000.00

Attachment 4.4.6
State Project 0620-029-017, P101, R201, C501

SCHEDULE OF ITEMS

This Schedule of Items shall identify the total material quantities and costs of each proposed pay item, using item codes and units of measure that are consistent with VDOT's list of standard and non-standard item codes. Any pay items considered for price adjustments shall be identified. The values and quantities shall be clearly supported by the escrowed pricing documents.

Contractor:

Date: March 14, 2014

VDOT Item Code ¹	Item Description	Fuel (F) or Price (P) Adjustment	App Quantity	Unit ¹	Budgeted Cost (\$)
540	08258	INLET DI-30K L=20'	4	EA	\$ 48,000.00
550	10113	AGGR BASE 21B	1,250	TON	\$ 121,250.00
560	10643	ASPHALT CONC TY BM-25	1,534	TON	\$ 306,800.00
570	0588	6" UNDERDRAIN	12,150	LF	\$ 279,450.00
580	NC	UNDERDRAIN STRUCT CONNECTIONS	41	EA	\$ 18,860.00
600	40912 S	LITILITES	1	LS	\$ 1.00
620	10128	AGGR BASE 21B	4,551	TONS	\$ 168,387.00
630	10013	CEMENT TREATED AGGR BASE 21A	1,319	TONS	\$ 61,993.00
640	10629	ASPHALT MILLING	67,140	SY	\$ 308,844.00
650	11184	TY B CRACK SEALING	105,742	LF	\$ 74,019.40
660	10643	ASPHALT CONC TY BM-25D+0.4	19,886	TONS	\$ 1,451,678.00
670	10643	ASPHALT CONC TY BM-25D+0.4	22,154	TONS	\$ 1,528,626.00
680	10651	ASPHALT CONC SMA-9.5 76-22	7,734	TONS	\$ 765,666.00
690	NC	HIGH FRICTION EPOXY SURF TREATMENT	11,742	SY	\$ 293,550.00
710	11187	FULL DEPTH JOINT REPAIR	1,728	LF	\$ 241,920.00
730	50108	SIGN PANEL	1,791	SF	\$ 64,476.00
740	50110	OVERLAY SIGN PANEL	36	SF	\$ 1,476.00
750	50575	CONCRETE FOR OVERHEAD SIGN FOUNDATION	210	CY	\$ 315,000.00
760	50902	OVERHEAD SIGN OH- 1 NO LUM	1	EA	\$ 130,000.00
770	50902	OVERHEAD SIGN C-1 W/LUM	1	EA	\$ 68,000.00
780	50902	OVERHEAD SIGN C-2 W/LUM	1	EA	\$ 62,000.00
790	50902	OVERHEAD SIGN C-3 NO /LUM	1	EA	\$ 52,000.00
800	50902	OVERHEAD SIGN C-5 W/LUM	1	EA	\$ 73,000.00
810	50902	OVERHEAD SIGN C-11 W/LUM	1	EA	\$ 83,000.00
820	50902	OVERHEAD SIGN B-2 BUTTERFLY	1	EA	\$ 28,000.00
830	50902	OVERHEAD SIGN C-14 W/LUM	1	EA	\$ 81,000.00
840	50902	OVERHEAD SIGN OH-2 W/LUM	1	EA	\$ 81,000.00
850	51963	REMOVE SIGN PANEL SP-3	1	EA	\$ 850.00
870	24160	CONSTRUCTION SIGNS	640	SF	\$ 19,840.00
880	85175	PORT VARIABLE MESSAGE SIGNS	852	DAY	\$ 93,720.00
890	24281	ELECTRONIC ARROWS	250	DAY	\$ 200,000.00
900	24278	GROUP 2 CHANNELIZING DEVICES	43,800	DAY	\$ 26,280.00
910	NC	CONST PAV MARK TY F CL 6" WHITE	237,771	LF	\$ 71,331.30
920	54466 S	CONST PAV MARK TY D REMOVABLE BLACK TAPE 6"	8,250	LF	\$ 14,850.00
930	NC	CONST PAV MARK TY D REMOVABLE WHITE TAPE 6"	8,250	LF	\$ 14,850.00
940	NC	TEMP PAV MARKERS	225	EA	\$ 495.00
950	85145	TRUCK MOUNTED ATTENUATOR	250	DAY	\$ 52,500.00
960	13611	IMPACT ATTENUATOR SERVICE	10	EA	\$ 110,000.00
970	24290	TRAFFIC BARRIER SERVICE	28,821	LF	\$ 605,241.00
980	24288	WARNING LIGHTS TY B	548	DAY	\$ 657.60
990	NC	NIGHT LIGHTING	250	DAY	\$ 62,500.00
1010	50902	LANE CONTROL SIGNALS	9	EA	\$ 216,000.00
1020	50902	OMS TYPE 1	2	EA	\$ 260,000.00
1030	50902	OMS TYPE 2	1	EA	\$ 63,000.00
1040	50902	OMS TYPE 3	2	EA	\$ 32,000.00
1050	50902	SPARE PARTS(SEE PAGE 58 2.13)	1	LS	\$ 54,000.00

Attachment 4.4.6
State Project 0620-029-017, P101, R201, C501

SCHEDULE OF ITEMS

This Schedule of Items shall identify the total material quantities and costs of each proposed pay item, using item codes and units of measure that are consistent with VDOT's list of standard and non-standard item codes. Any pay items considered for price adjustments shall be identified. The values and quantities shall be clearly supported by the escrowed pricing documents.

Contractor:

Date: March 14, 2014

VDOT Item Code ¹	Item Description	Fuel (F) or Price (P) Adjustment	App Quantity	Unit ¹	Budgeted Cost (\$)
1060	51027	TRAINING	1	LS	\$ 18,000.00
1070	51170	SE 5 SERVICE	1	LS	\$ 3,300.00
1080	51247	ITS CABINET FOUNDATION CF-3	21	EA	\$ 117,600.00
1090	51260	CONCRETE FOR ITS FOUNDATION	120	CY	\$ 144,000.00
1100	51303	SIGNAL POLE WITH MAST ARM FOR LCS	7	EA	\$ 133,000.00
1110	51425	CCTV POLE -50 FT	6	EA	\$ 57,600.00
1120	51425	DETECTOR POLE	4	EA	\$ 15,600.00
1130	51614	CCTV AND SLM VIDEO CABLE	7,500	LF	\$ 18,000.00
1140	55040	#4 CONDUCTOR CABLE	44,000	LF	\$ 74,800.00
1150	55120	TRACER WIRE	36,000	LF	\$ 25,200.00
1160	55348	SE-9 SERVICE	3	EA	\$ 69,000.00
1170	55588	JB-52 POWER	44	EA	\$ 57,200.00
1180	56024	CONDUIT (2)1 -1/4" FOR COM	13,600	LF	\$ 16,900.00
1190	56026	1-1/4" STRUCTURE MOUNTED CONDUIT	3,500	LF	\$ 49,000.00
1200	56028	1-1/2" STRUCTURE MOUNTED CONDUIT	200	LF	\$ 3,000.00
1210	56030	CONDUIT 2" FOR POWER	10,500	LF	\$ 31,500.00
1220	56042	2" HDPE CONDUIT	800	LF	\$ 2,880.00
1230	56050	DIRECTIONAL BORE	2,500	LF	\$ 235,000.00
1240	56200	TRENCH EC-1	12,000	LF	\$ 204,000.00
1250	56205	TEST BORE	23	EA	\$ 21,390.00
1260	59050	CCTV CAMERA	6	EA	\$ 51,600.00
1270	59050	SLMS CAMERA	26	EA	\$ 252,200.00
1280	59050	PRETERMINATED PATCH PANEL W/100' TAIL	21	EA	\$ 25,200.00
1290	59050	SPLICE ENCLOSURE	22	EA	\$ 13,200.00
1300	59050	FIBER OPTIC PULL BOX	48	LF	\$ 96,000.00
1310	59050	ITS CABINET	21	EA	\$ 420,000.00
1320	59050	ETHERNET FIELD SWITCH LAYER 2	21	EA	\$ 60,900.00
1330	59050	24 PORT LAYER 2 SWITCH FOR TOC	1	EA	\$ 5,000.00
1340	59071	12 STRAND FIBER OPTIC CABLE	11,000	LF	\$ 18,700.00
1350	99991	MODIFY EXISTING OVERHEAD SPAN	1	EA	\$ 2,600.00
1360	99992	RELOCATE IDS FROM OH TO NEW POLE	1	EA	\$ 480.00
1370	99993	INTEGRATION & TESTING	1	LS	\$ 48,000.00
1380	99994	BRING EXISTING ITS DEVICE ONLINE	5	EA	\$ 16,500.00
1390	99995	MODIFICATIONS TO TOC	1	EA	\$ 36,000.00
1410	NC	SAWCUT BARRIER	30	EA	\$ 3,300.00
1420	51910	SAWCUT	8,133	LF	\$ 36,598.50
1430	NC	REMOVE MEDIAN BARRIER	5,729	LF	\$ 252,070.00
1440	NC	REMOVE PIER PROTECTION	380	LF	\$ 41,800.00
1450	24410	DEMOLITION OF PAVEMENT	3,072	SY	\$ 64,512.00
1460	00120	REGULAR EXCAVATION	1,160	CY	\$ 34,800.00
1470	13410 S	BARRIER MOD DETAIL 1 H= 0" TO 3"	208	LF	\$ 43,680.00
1480	13410 S	BARRIER MOD DETAIL 2A H= 3" TO 9"	555	LF	\$ 111,000.00
1490	13410 S	BARRIER MOD DETAIL 2A H= 9" TO 18"	1,057	LF	\$ 211,400.00
1500	13410 S	BARRIER MOD DETAIL 3 H= 9" TO 18"	825	LF	\$ 132,000.00
1510	13410 S	BARRIER MOD DETAIL 3 H= 18" TO 30"	968	LF	\$ 174,240.00
1520	13410 S	MEDIAN BARRIER MB-8A MOD H= 0" TO 3"	591	LF	\$ 94,560.00

Attachment 4.4.6
State Project 0620-029-017, P101, R201, C501

SCHEDULE OF ITEMS

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Contractor:

Date: March 14, 2014

VDOT Item Code ¹	Item Description	Fuel (F) or Price (P) Adjustment	App Quantity	Unit ¹	Budgeted Cost (\$)
1530	13410 S MEDIAN BARRIER MB-SA MOD H= 3" TO 9"		520	LF	\$ 98,800.00
1540	13410 S MEDIAN BARRIER MB-SA MOD H= 9" TO 18"		555	LF	\$ 116,550.00
1550	13410 S MEDIAN BARRIER MB-SA MOD H= 18" TO 30"		1,718	LF	\$ 412,320.00
1560	13410 S MEDIAN BARRIER MB-SA MOD H= 30" TO 42"		654	LF	\$ 196,200.00
1570	NC BARRIER GATE PAD		121	LF	\$ 24,200.00
1580	13460 MEDIAN BARRIER MB-7D		220	LF	\$ 41,800.00
1590	13461 MEDIAN BARRIER MB-7E		2,740	LF	\$ 438,400.00
1600	13462 MEDIAN BARRIER MB-7F		150	LF	\$ 33,000.00
1610	NC PIER PROTECTION TL-5		645	LF	\$ 232,200.00
1620	NC MEDIAN BARRIERS @ SIGN FOUNDATIONS		232	CY	\$ 157,760.00
1630	NC MEDIAN PAVEMENT		100	SY	\$ 14,000.00
1640	10113 AGGR BASE 21B		1,302	TONS	\$ 126,294.00
1650	10643 ASPHALT CONC TY BM-25D+0.4	P	1,598	TONS	\$ 319,600.00
1660	NC MOVABLE BARRIER		1	EA	\$ 170,000.00
1670	NC BARRIER REFLECTIVE TAPE		500	LF	\$ 5,500.00
1690	31963 REMOVE EXISTING BR MT SIGN STRUCT		3	EA	\$ 12,300.00
1710	85130 BOND		1	LS	\$ 55,000.00
1730	25590 CONTINGENCY		1	LS	\$ 600,000.00
1740	00101 CONSTRUCTION SURVEYING		1	LS	\$ 360,000.00
	Total				\$ 19,873,709.50

¹ Use five-digit item codes and units of measure that are consistent with VDOT's list of standard and non-standard item codes (i.e. 00100-Mobilization; 00120-Regular Excavation, etc...).

S Similar VDOT Code
 NC No VDOT Code



4.4.7

Proposed Monthly Payment Schedule

**I-495 Northern Section Shoulder Use
Project No. (FO) 0495-029-133, P101, C501**

Francis O. Day Company, Inc.

Monthly Payment Schedule

Month	Projected Monthly Total	Projected Cummulative Total
May 2014	\$ 1,213,800	\$ 1,213,800
June 2014	\$ 2,369,682	\$ 3,583,482
July 2014	\$ 2,744,030	\$ 6,327,513
August 2014	\$ 3,715,340	\$ 10,042,853
September 2014	\$ 3,891,235	\$ 13,934,088
October 2014	\$ 3,897,385	\$ 17,831,473
November 2014	\$ 1,212,573	\$ 19,044,046
December 2014	\$ 612,551	\$ 19,656,597
January 2015	\$ 36,186	\$ 19,692,782
February 2015	\$ 36,186	\$ 19,728,968
March 2015	\$ 36,186	\$ 19,765,153
April 2015	\$ 36,186	\$ 19,801,339
May 2015	\$ 36,186	\$ 19,837,524
June 2015	\$ 36,186	\$ 19,873,710



Francis O. Day Company, Inc.
850 East Gude Dr., Suite A
Rockville, MD 20850
P. 301.652.2400
F. 302.424.3697
www.foday.com
